

# AUGUSTA REGIONAL AIRPORT

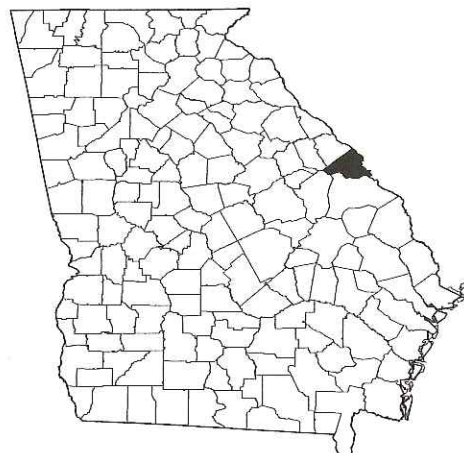
## CONSTRUCT TAXIWAY G

1501 AVIATION WAY, AUGUSTA, GA 30906-9620

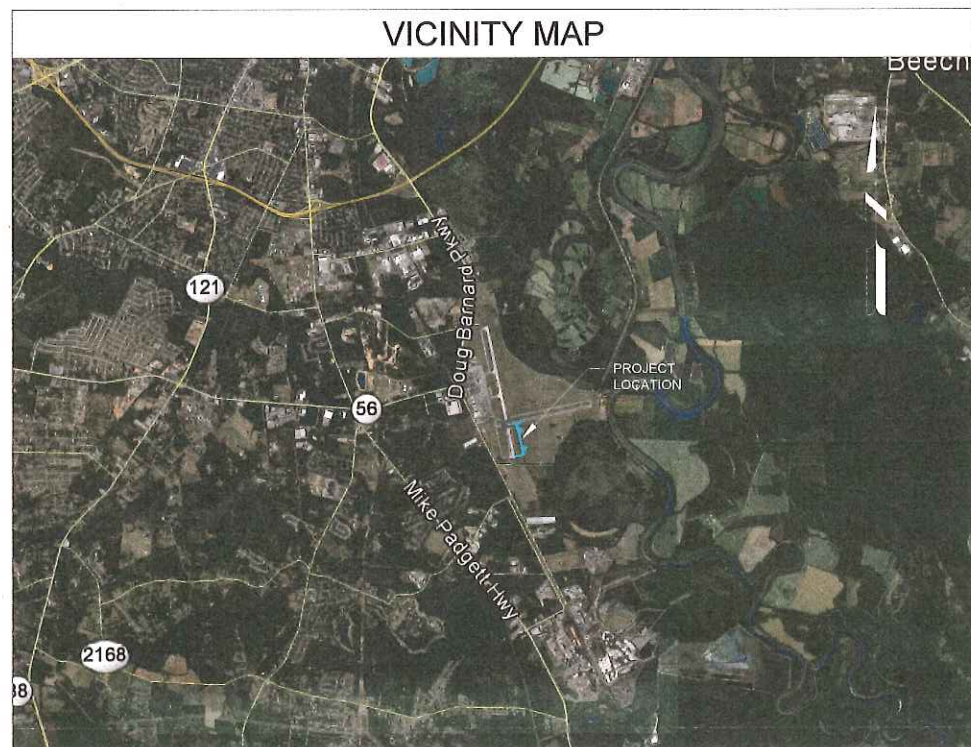
PROJECT NO: 0119700-170264.01

DECEMBER 6, 2022

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RICHMOND COUNTY



VICINITY MAP



LOCATION MAP

**Mead & Hunt**  
Mead and Hunt, Inc.  
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Lexington, SC 29072  
phone: 803-996-2900  
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AUGUSTA REGIONAL AIRPORT  
CONSTRUCT TAXIWAY G

1501 AVIATION WAY  
AUGUSTA, GA 30906-9620

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MSH NO: 0119700-170869.02  
DATE: DECEMBER 6, 2022  
DESIGNED BY: EJS  
DRAWN BY: NJH  
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SHEET CONTENTS  
COVER SHEET

SHEET NO:

G-001



X:\01197001\70866-02\TECH\CAD\DRAWINGS\G-003 LEGEND AND ABBREVIATIONS.DWG  
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**Mead & Hunt**

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*Augusta*  
Regional Airport

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CHECKED BY: DES

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SHEET CONTENTS  
SHEET INDEX

SHEET NO

**G-002**

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# LEGENDS AND ABBREVIATIONS

## LEGEND:

	BENCHMARK
	CONTROL POINT
	BOLLARD
	CLEANOUT, SANITARY OR STORM
	ELECTRIC MANHOLE
	ELECTRIC HANDHOLE (PULLBOX)
	GATE
	MARKER, CABLE
	MARKER, DUCT
	SIGN (SINGLE POST)
	SOIL BORING
	STORM INLET, ROUND
	STORM FLARED END SECTION
	STORM SEWER MANHOLE
	STORM WATER QUALITY MANHOLE
	UNDERDRAIN MANHOLE
	CONDUIT, GENERIC
	ELECTRIC, UNDERGROUND
	EXISTING CONTOUR LINES
	PROPOSED CONTOUR LINES
	FENCE
	FIBER OPTIC CABLE
	GRADING LIMITS
	PROPERTY LINE
	RIPRAP
	ASPHALT, TAXIWAY SHOULDER
	CONCRETE, TAXIWAY
	GROUND, EXISTING
	GROUND, PROPOSED
	ASPHALT REMOVAL
	SECTION REFERENCE
	ELEVATION REFERENCE
	SHEET REFERENCE (SEE X-XXX)
	DETAIL REFERENCE (SEE XX-XXX)
	EXISTING SPOT ELEVATION 734.97
	FINISHED / PROPOSED SPOT ELEVATION 738.76
	PROPOSED ELEVATION 737.15
	EXISTING ELEVATION 737.55
	FINISHED / PROPOSED SLOPE 3:1 2%
	HAUL or TRAFFIC ROUTE
	REMOVE
	REMOVE
	SAW CUT PAVEMENT

## EXISTING

	BASE CAN L-867
	BASE CAN
	C/L BASE CAN
	TDZ BASE CAN
	RLG (ELEVATED BASE MOUNTED)
	RGL (ELEVATED STAKE MOUNTED)
	RGL (INSET)
	MIRL (ELEVATED BASE MOUNTED)
	MIRL (ELEVATED STAKE MOUNTED)
	MIRL (INSET)
	HIRL (ELEVATED BASE MOUNTED)
	HIRL (ELEVATED STAKE MOUNTED)
	HIRL (INSET)
	MITL (ELEVATED BASE MOUNTED)
	MITL (ELEVATED STAKE MOUNTED)
	MITL (INSET)
	PAPI
	REIL
	TDZ
	CABLE MARKER
	DUCT MARKER
	DISTANCE REMAINING SIGN
	GUIDANCE SIGN
	CONDUIT LABEL (CABLE QTY & CIRCUIT NAME)
	AIRFIELD RATED ELECTRIC MANHOLE

## LINES

	AOA AIRPORT OPERATION AREA
	BRL BUILDING RESTRICTION LINE
	FAA FAA
	ROFA RUNWAY OBJECT FREE AREA
	RPZ RUNWAY PROTECTION ZONE
	RRA RUNWAY RESTRICTED AREA
	RSA RUNWAY SAFETY AREA
	TAXIWAY EDGE
	TOFA TAXIWAY OBJECT FREE AREA
	TSA TAXIWAY SAFETY AREA

## PROPOSED

	BASE CAN L-867
	BASE CAN
	C/L BASE CAN
	TDZ BASE CAN
	RLG (ELEVATED BASE MOUNTED)
	RGL (ELEVATED STAKE MOUNTED)
	RGL (INSET)
	MIRL (ELEVATED BASE MOUNTED)
	MIRL (ELEVATED STAKE MOUNTED)
	MIRL (INSET)
	HIRL (ELEVATED BASE MOUNTED)
	HIRL (ELEVATED STAKE MOUNTED)
	HIRL (INSET)
	MITL (ELEVATED BASE MOUNTED)
	MITL (ELEVATED STAKE MOUNTED)
	MITL (INSET)
	PAPI
	REIL
	TDZ
	CABLE MARKER
	DUCT MARKER
	DISTANCE REMAINING SIGN
	GUIDANCE SIGN
	CONDUIT LABEL (CABLE QTY & CIRCUIT NAME)
	AIRFIELD RATED ELECTRIC MANHOLE

	AOA AIRPORT OPERATION AREA
	BRL BUILDING RESTRICTION LINE
	FAA FAA
	ROFA RUNWAY OBJECT FREE AREA
	RPZ RUNWAY PROTECTION ZONE
	RRA RUNWAY RESTRICTED AREA
	RSA RUNWAY SAFETY AREA
	TAXIWAY EDGE
	TOFA TAXIWAY OBJECT FREE AREA
	TSA TAXIWAY SAFETY AREA

## ABBREVIATIONS:

A	ABANDON
AB	AGGREGATE BASE
A/C	AIRCRAFT
ABAND	ABANDON
AC	ASPHALT CONCRETE
ALCMS	AIRFIELD LIGHTING CONTROL AND MONITORING SYSTEM
ALT	ALTERNATE
AMSL	ABOVE MEAN SEA LEVEL
AOA	AIRCRAFT OPERATIONS AREA
APCH	APPROACH
APPROX	APPROXIMATE
ASB	AGGREGATE SUB-BASE
AR	ACCESS ROAD
ARFF	AIRCRAFT RESCUE AND FIRE FIGHTING
ATCT	AIR TRAFFIC CONTROL TOWER
AWG	AMERICAN WIRE GAUGE
BC	BEGINNING OF CURVE
BIT	BITUMINOUS
BLDG	BUILDING
BM	BENCHMARK
BOT	BOTTOM
BVC	BEGINNING OF VERTICAL CURVE
CA TEAM	CONSTRUCTION ADMINISTRATION TEAM
C-C	CENTER TO CENTER
CB	CATCH BASIN
CIPCP	CAST IN-PLACE CONCRETE PIPE
CJ	CONSTRUCTION JOINT
CFS	CUBIC FEET PER SECOND
CL	CENTERLINE
CLF	CHAINLINK FENCE
CLR	CLEAR
CMP	CORRUGATED METAL PIPE
CO	CLEANOUT
CONC	CONCRETE
CE	CONCRETE ENCASED
CONT	CONTINUOUS
CP	CONTROL POINT
CTB	CEMENT TREATED BASE
CKT	CIRCUIT
CSPP	CONSTRUCTION SAFETY PHASING PLAN
DB	DIRECT BURIAL
DEG	DEGREE
DI	DROP INLET
DIA	DIAMETER
DIM	DIMENSION
DIP	DUCTILE IRON PIPE
DP	DEPTH
(E)	EXISTING
EC	END OF CURVE
EG	EXISTING GRADE
ELEV	ELEVATION
EOP	EDGE OF PAVEMENT
EQ	EQUAL
EVC	END OF VERTICAL CURVE
ETR	EXISTING TO REMAIN
FAA	FEDERAL AVIATION ADMINISTRATION
FBO	FIXED BASE OPERATOR
FES	FLARED END SECTION
FF	FINISHED FLOOR
FG	FINISHED GRADE
FH	FIRE HYDRANT
FL	FLOW LINE
FOD	FOREIGN OBJECT DEBRIS
FPS	FEET PER SECOND
FT	FEET

G	GAS LINE
GAL	GALLON
GALV	GALVANIZED
GA MUTCD	GEORGIA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES
GB	GRADE BREAK
GND	GROUND
GPM	GALLONS PER MINUTE
GPR	GROUND PENETRATING RADAR
GPSP	GENERAL PERRY SMITH PARKWAY
GS	GLIDE SLOPE
HH	HANDHOLE
H	HEIGHT
HDPE	HIGH DENSITY POLYETHYLENE
HIRL	HIGH INTENSITY RUNWAY LIGHT
HIR,THL	HIGH INTENSITY THRESHOLD LIGHT
HORIZ	HORIZONTAL
HMA	HOT MIX ASPHALT
HP	HIGH POINT
HW	HEADWALL
HWL	HIGH WATER LEVEL
HWY	HIGHWAY
IE	INVERT ELEVATION
IFR	INSTRUMENT FLIGHT RULES
ILS	INSTRUMENT LANDING SYSTEM
IN	INCHES
IP	IN-PAVEMENT
L	LENGTH
LBS	POUNDS
LF	LINEAL FEET
LOC	LOCALIZER
LWL	LOW WATER LEVEL
MH	MANHOLE
MALS	MEDIUM INTENSITY APPROACH LIGHTING SYSTEM
MALSF	MALS W/ SEQUENCED FLASHERS
MALSRL	MALS W/ RUNWAY ALIGNMENT INDICATOR LIGHTS
MAX	MAXIMUM
ME	MATCH EXISTING
MID	MID POINT
MIN	MINIMUM
MIRL	MEDIUM INTENSITY RUNWAY LIGHT
MITL	MEDIUM INTENSITY TAXIWAY LIGHT
MPH	MILES PER HOUR
N	NO
(N)	NEW
NIC	NOT IN CONTRACT
NO. OR #	NUMBER
NOTAM	NOTICE TO AIRMAN
NTS	NOT TO SCALE
OFF	OFFSET
OFZ	OBJECT FREE ZONE
O/S	OFFSET
OC	ON CENTER
OH	OVERHEAD
OWS	OIL WATER SEPARATOR
PAPI	PRECISION APPROACH PATH INDICATOR
PR	PAIR
PB	PULL BOX
PC	POINT OF CURVATURE
PCC	PORTLAND CEMENT CONCRETE
PCF	POUNDS PER CUBIC FOOT
PERF	PERFORATED
PI	POINT OF INTERSECTION
POB	POINT OF BEGINNING
POC	POINT OF CURVE
POE	POINT OF ENDING
PSI	POUNDS PER SQUARE INCH
PSF	POUNDS PER SQUARE FOOT

PT	POINT OF TANGENCY
PVC	POINT OF VERTICAL CURVE
PVC	POLYVINYL CHLORIDE
PVI	POINT OF VERTICAL INTERSECTION
PVT	POINT OF VERTICAL TANGENCY
Q	RATE OF FLOW
QTY	QUANTITY
R	RADIUS
(R)	REMOVE
R&R	REMOVE AND REPLACE
RC	RELATIVE COMPACTION
REL	RELOCATE EXISTING
RCP	REINFORCED CONCRETE PIPE
REQ	REQUIRED
ROFA	RUNWAY OBJECT FREE AREA
ROW	RIGHT OF WAY
RGB	RUNWAY GUARD LIGHT
RSA	RUNWAY SAFETY AREA
RWA	RUNWAY WORK RESTRICTED AREA
RWAPP	RUNWAY APPROACH LIGHT
RWY OR RW	RUNWAY
S	SANITARY LINE
SF	SQUARE FOOT
SG	STRAIGHT GRADE
SH	SHOULDER
SIDA	SECURITY IDENTIFICATION DISPLAY AREA
SMGS	SURFACE MOVEMENT GUIDANCE AND CONTROL SYSTEM
SPCD	SAFETY PLAN COMPLIANCE DOCUMENT
SS	STAINLESS STEEL
ST	STORM LINE
STA	STATION
STD	STANDARD
STL	STEEL
T	TELEPHONE LINE
TC	TOP OF CURB
TG	TOP OF GRATE
TL	TAXILINE
TOE	TOE OF BANK
TOP	TOP OF BANK
TDZ	TOUCHDOWN ZONE
TWY	TAXIWAY
TOFA	TAXIWAY OBJECT FREE AREA
TSA	TAXIWAY SAFETY AREA
TYP	TYPICAL
UD	UNDERDRAIN
OFA	OBJECT FREE AREA
UFN	UNTIL FURTHER NOTICE
UG	UNDERGROUND
UON	UNLESS OTHERWISE NOTED
V	VELOCITY
VC	VERTICAL CURVE
VERT	VERTICAL
VFR	VISUAL FLIGHT RULES
VG	VALLEY GUTTER
VIF	VERIFY IN FIELD
VASI	VISUAL APPROACH SLOPE INDICATOR
W	WATER LINE
WA	WORK AREA
W	WITH
W/O	WITHOUT
WSE	WATER SURFACE ELEVATION
WSP	WELDED STEEL PIPE
WV	WATER VALVE
WWM	WELDED WIRE MESH

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**AUGUSTA REGIONAL AIRPORT  
 CONSTRUCT TAXIWAY G**

1501 AVIATION WAY  
 AUGUSTA, GA 30906-9620

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MAH NO: 0119700-170868.02  
 DATE: DECEMBER 6, 2022  
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 SHEET COMMENTS  
**LEGEND AND ABBREVIATIONS**

SHEET NO.

**G-003**



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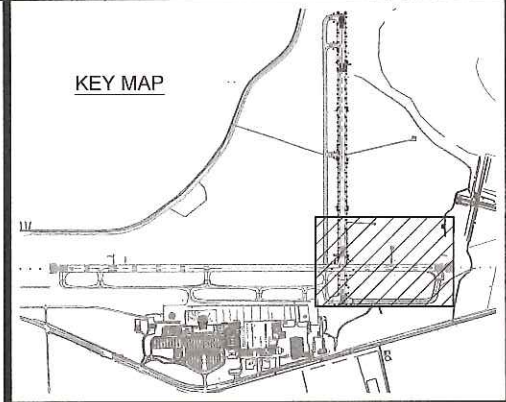
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SHEET CONTENTS  
 PROJECT LAYOUT  
 PLAN

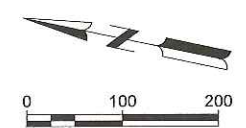
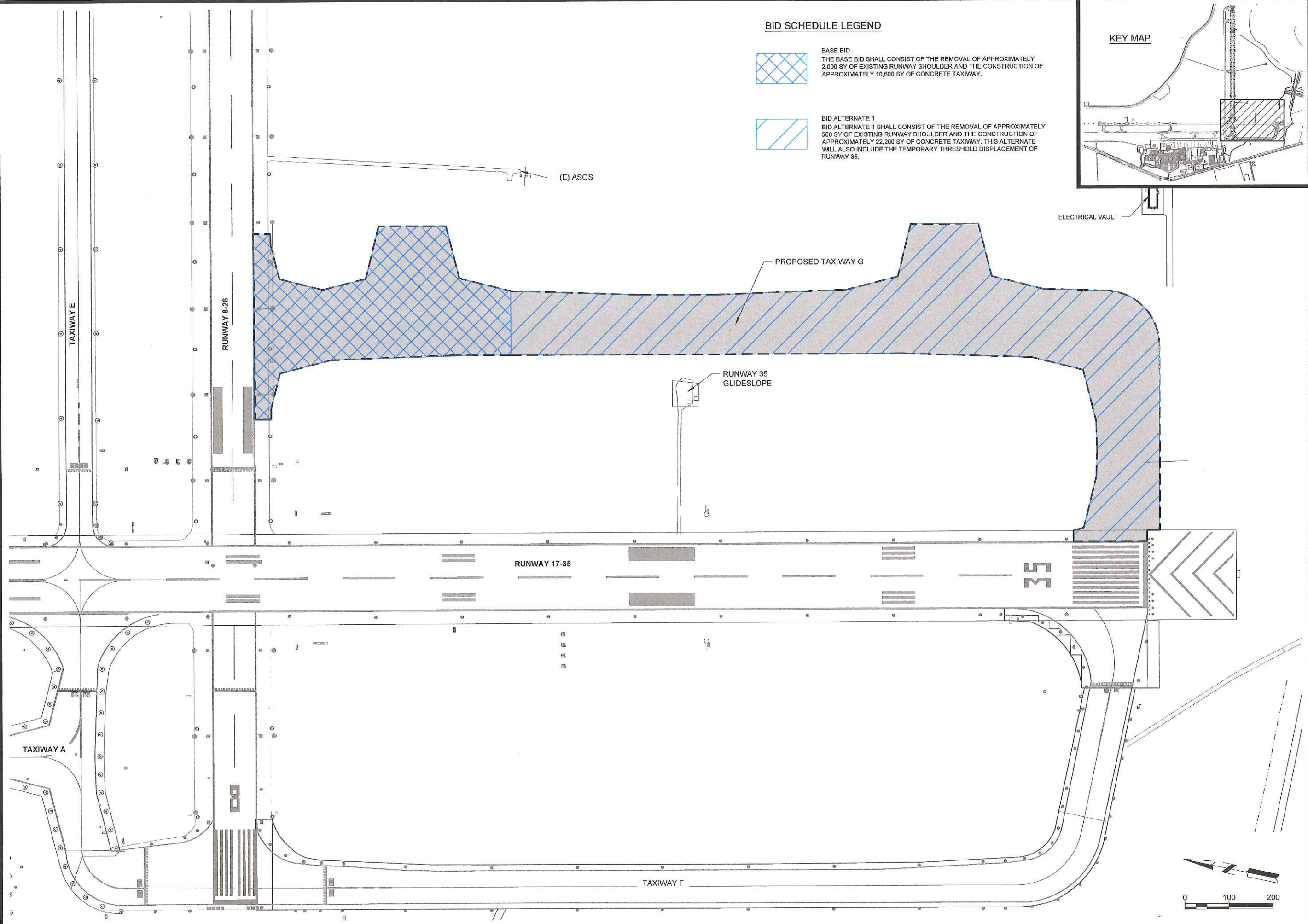
SHEET NO.

**G-021**



**BID SCHEDULE LEGEND**

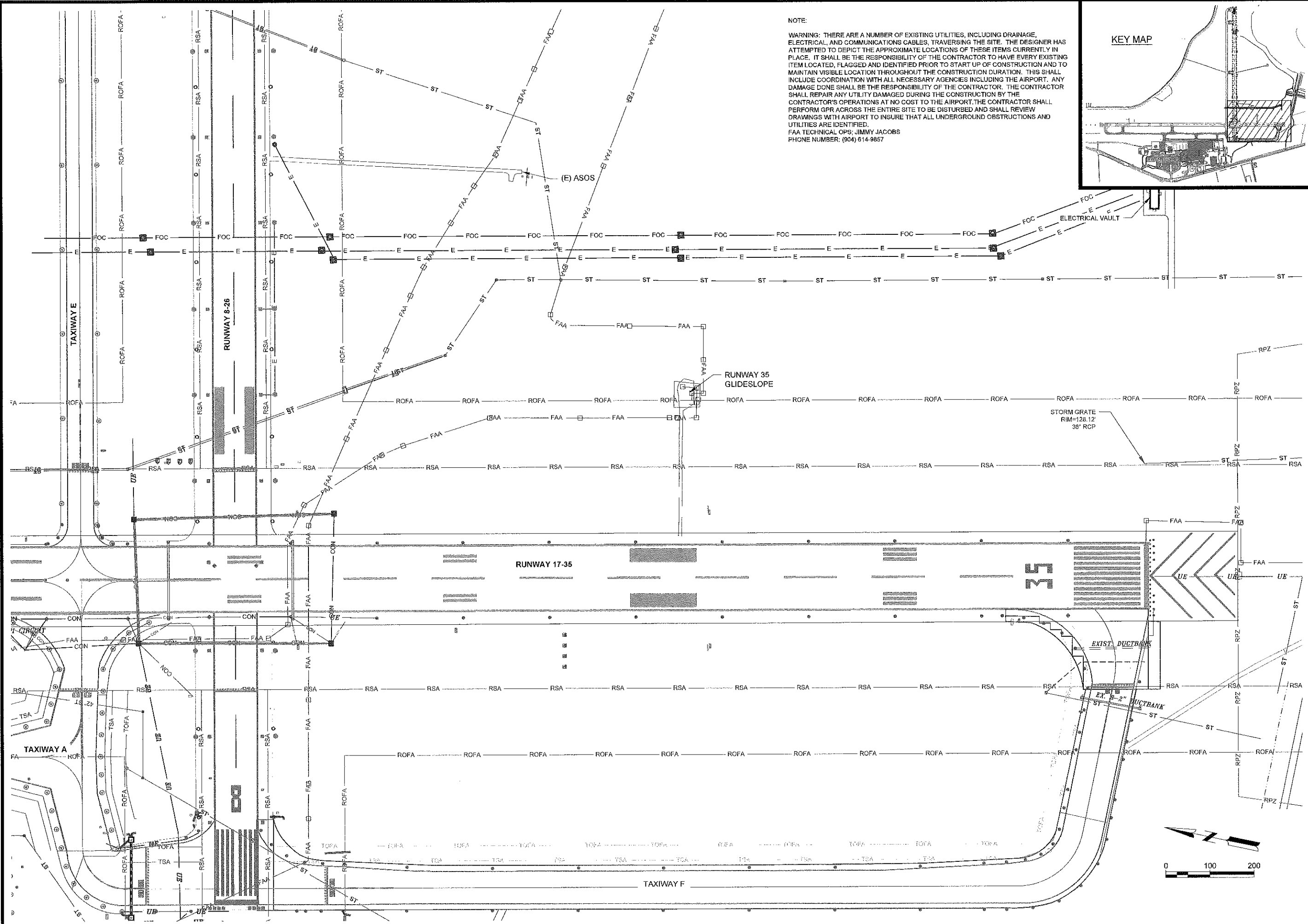
-  **BASE BID**  
 THE BASE BID SHALL CONSIST OF THE REMOVAL OF APPROXIMATELY 2,000 SY OF EXISTING RUNWAY SHOULDER AND THE CONSTRUCTION OF APPROXIMATELY 10,600 SY OF CONCRETE TAXIWAY.
-  **BID ALTERNATE 1**  
 BID ALTERNATE 1 SHALL CONSIST OF THE REMOVAL OF APPROXIMATELY 500 SY OF EXISTING RUNWAY SHOULDER AND THE CONSTRUCTION OF APPROXIMATELY 22,200 SY OF CONCRETE TAXIWAY. THIS ALTERNATE WILL ALSO INCLUDE THE TEMPORARY THRESHOLD DISPLACEMENT OF RUNWAY 35.



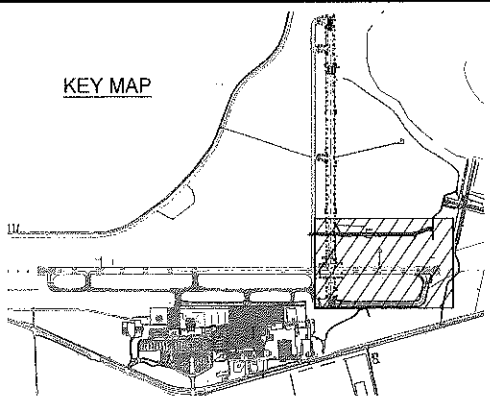
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NOTE:  
WARNING: THERE ARE A NUMBER OF EXISTING UTILITIES, INCLUDING DRAINAGE, ELECTRICAL AND COMMUNICATIONS CABLES, TRAVERSING THE SITE. THE DESIGNER HAS ATTEMPTED TO DEPICT THE APPROXIMATE LOCATIONS OF THESE ITEMS CURRENTLY IN PLACE. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO HAVE EVERY EXISTING ITEM LOCATED, FLAGGED AND IDENTIFIED PRIOR TO START UP OF CONSTRUCTION AND TO MAINTAIN VISIBLE LOGGING THROUGHOUT THE CONSTRUCTION DURATION. THIS SHALL INCLUDE COORDINATION WITH ALL NECESSARY AGENCIES INCLUDING THE AIRPORT. ANY DAMAGE DONE SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR SHALL REPAIR ANY UTILITY DAMAGED DURING THE CONSTRUCTION BY THE CONTRACTOR'S OPERATIONS AT NO COST TO THE AIRPORT. THE CONTRACTOR SHALL PERFORM GPR ACROSS THE ENTIRE SITE TO BE DISTURBED AND SHALL REVIEW DRAWINGS WITH AIRPORT TO INSURE THAT ALL UNDERGROUND OBSTRUCTIONS AND UTILITIES ARE IDENTIFIED.  
FAA TECHNICAL OPS; JIMMY JACOBS  
PHONE NUMBER: (904) 614-9857



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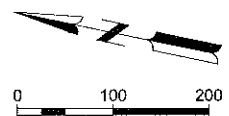
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EXISTING  
CONDITIONS

SHEET NO.

**G-031**



**LEGEND:**

- BENCHMARK
- IRON PIN
- PK or MAG NAIL
- REBAR
- SECTION
- SECTION QUARTER CORNER
- SECTION CORNER HALF
- PROPERTY LINE
- ROW RIGHT OF WAY

**SURVEY DATA:**

SURVEY DATE(S) 5/16/2016, 11/27/2018  
 COORDINATE SYSTEM USGS NAD 83  
 VERTICAL DATUM USGS NAVD 88  
 CONTROL SOURCE SEE TABLE  
 SURVEY UNITS US SURVEY FOOT

**PRIMARY SURVEY CONTROL TABLE**

POINT	LOCAL PROJECTION		NAVD88 ELEVATION	DESCRIPTION
	NORTHING	EASTING		
20002	1225860.21	717712.10	134.55	SACS (FAA AGS ARP 2)
1	1222624.55	717885.01	129.99	GEO FAA

**ALIGNMENT LINE DATA**

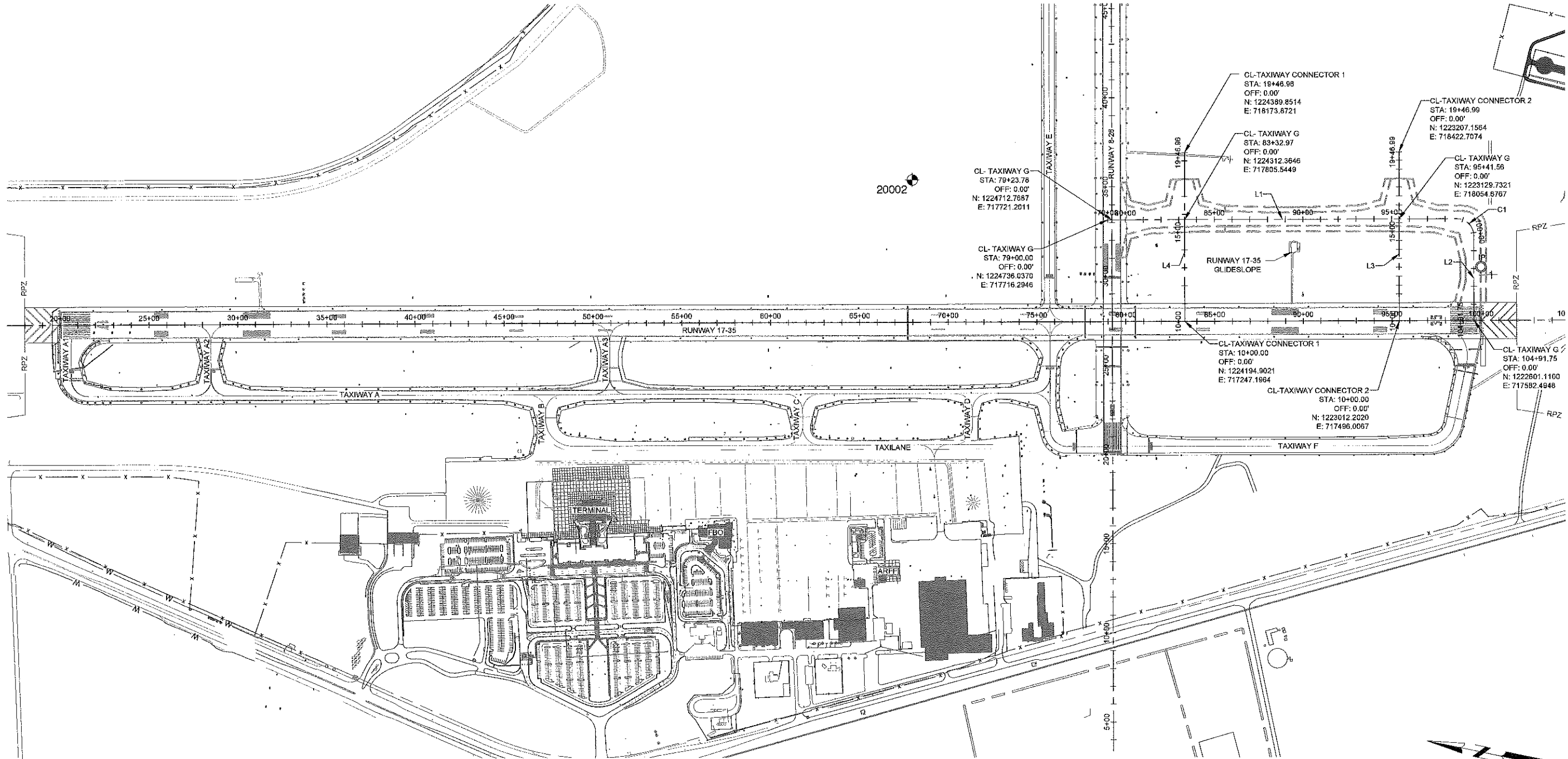
LINE #	LENGTH	BEARING
L1	1966.51	S11° 53' 45.32"E
L2	476.01	S78° 06' 22.87"W
L3	946.99	N78° 07' 10.75"E
L4	946.96	N78° 07' 10.75"E

**ALIGNMENT CURVE DATA**

#	RADIUS	Δ	TANGENT	LENGTH	CENTER POINT NORTHING	CENTER POINT EASTING	PC NORTHING	PC EASTING	PT NORTHING	PT EASTING	#
C1	95.00'	90° 00' 00"	95.00	149.23'	1222792.173	718028.701	1222811.756	718121.661	1222699.213	718048.284	

**SITE SURVEY PLAN NOTES:**

- PRIVATE SUBSURFACE UTILITY LOCATIONS SHOWN HEREON ARE BASED UPON GROUND MARKINGS PLACED BY CLIENT REPRESENTATIVE. MARKINGS MAY NOT BE BY BENEFIT OF SUBSURFACE DETECTING INSTRUMENTS AS SOME WERE MARKED PER PERSONNEL BEST RECOLLECTION.
- PUBLIC SUBSURFACE UTILITY LOCATIONS SHOWN HEREON ARE BASED UPON GROUND MARKINGS PLACED BY GEORGIA 811. GEORGIA 811 DOES NOT GUARANTEE THE PRECISION OF THEIR MARKINGS. IN ACCORDANCE WITH GEORGIA LAW, SUBSURFACE UTILITIES MUST BE EXPOSED VIA HAND DIGGING BEFORE MACHINE DIGGING IS PERMISSIBLE. UTILITY LOCATION MARKINGS ARE VALID FOR ONLY 10 DAYS. CONTRACTOR MUST ORDER NEW UTILITY LOCATE PRIOR TO ANY EXCAVATION.
- SANITARY SEWER AND STORM SEWER LOCATIONS HAVE BEEN DETERMINED BY OBSERVABLE SURFACE STRUCTURES AND RESPECTIVE FEATURES. INTERMEDIATE PIPE LOCATIONS ARE APPROXIMATE AS ACCURATE LOCATIONS WERE NOT AVAILABLE AT TIME OF SURVEY.
- CONTROL POINTS AND BENCHMARKS SHOWN HEREON ARE FOR REFERENCE PURPOSES ONLY. PRIOR TO STAKING, THE CONTROL MUST BE INDEPENDENTLY VERIFIED AS UNDISTURBED. NO WARRANTY IS MADE WITH RESPECT TO THE ACCURACY OF CONTROL SHOWN HEREON AS THEY ARE SUBJECT TO POTENTIAL DISTURBANCE.



**SURVEY CONTROL PLAN**  
 SCALE: 1" = 300'

1  
 G-041

**Mead & Hunt**  
 Mead and Hunt, Inc.  
 878 South Lake Drive  
 Lexington, SC 29072  
 phone: 803-996-2900  
 meadhunt.com

**Augusta**  
 Regional Airport

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**AUGUSTA REGIONAL AIRPORT  
 CONSTRUCT TAXIWAY G**

1501 AVIATION WAY  
 AUGUSTA, GA 30906-9620

REVISION  
 ISSUED FOR BID

NOT FOR CONSTRUCTION

REV# NO: 0119700-170889.02  
 DATE: DECEMBER 6, 2022  
 DESIGNED BY: N/A  
 DRAWN BY: N/LH  
 CHECKED BY: DES  
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SHEET CONTENTS  
 SURVEY CONTROL

SHEET NO

**G-041**



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**AUGUSTA REGIONAL AIRPORT  
CONSTRUCT TAXIWAY G**

1501 AVIATION WAY  
AUGUSTA, GA 30906-9620

REVISION  
ISSUED FOR BID

NOT FOR CONSTRUCTION

MSH NO: 0119700-170869.02  
DATE: DECEMBER 6, 2022  
DESIGNED BY: EJS  
DRAWN BY: NJH  
CHECKED BY: DES  
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SHEET CONTENTS  
PROJECT QUANTITY  
TABLES

SHEET NO.

**G-061**

SUMMARY OF QUANTITIES			
SPEC.	ITEM	UNIT	QUANTITY
<b>AGS TAXIWAY G (BASE BID - ASPHALT SHOULDERS)</b>			
C-100.1	Contractor Quality Control Program	LS	1
C-102.1a	Installation, Maintenance, and Removal of Silt Fence or Silt Sock	LF	11754
C-102.1b	Construct, Maintain, and Remove Inlet Sediment Trap	EA	48
C-102.1c	Construct, Maintain, and Remove Construction Exit	EA	1
C-102.1d	Water Quality Monitoring and Sampling	EA	36
C-102.1e	Water Quality Inspections	EA	18
C-102.1f	Erosion Control Mobilization	LS	1
C-102.1g	Emergency Erosion Control Mobilization	LS	1
C-102.1h	Permanent Water Quality Inserts	EA	14
C-105.1	Mobilization, Cleanup, and Demobilization	LS	1
C-105.2	Airfield Safety and Traffic Control	LS	1
P-101.1	Asphaltic Concrete Pavement Removal, Full Depth, Off Site	SY	2032
P-101.2	Cold Milling	SY	233
P-152.1	Unclassified Excavation, Dispose Off-Site	CY	3290
P-152.3	Subgrade Preparation	SY	16190
P-152.4	Unsuitable/Over Excavation	CY	5000
P-154.1	Uncrushed Aggregate Base Course (6")	CY	1607
P-154.2	Uncrushed Aggregate Base Course (12")	CY	1895
P-306.1	Lean Concrete Base Course (5")	SY	11170
P-401.1	Asphalt Concrete Surface Course (4")	TON	1085
P-403.1	Asphalt Concrete Base Course (4")	TON	1085
P-501.1	Portland Cement Concrete Pavement (14")	SY	10581
X-501.1	Portland Cement Concrete Curing Facility	LS	1
P-602.1	Emulsified Asphalt Prime Coat	Gal	482
P-603.1	Emulsified Asphalt Tack Coat	Gal	241
P-605.1	Joint Sealing Filler	LF	17577
P-620.1	Permanent Pavement Markings	SF	9393
P-620.2	Temporary Pavement Markings	SF	9393
P-620.3	Reflective Media	LBS	280
P-620.4	Thermoplastic Preformed Surface Sign	EA	2
D-701.1	Concrete Sewer Pipe, 18-Inch, Class V	LF	70
D-705.1	6-Inch Perforated Polyethylene Underdrain Pipe, Schedule 40, Complete	LF	1165
D-705.2	Underdrain Clean-out	EA	8
D-751.2	Airfield Inlet with Aircraft Rated Grate	EA	1
D-751.3	Adjust Storm Manhole/Inlet to Grade	EA	1
D-751.4	Connect Storm Sewer Pipe to Existing Storm Structure	EA	1
T-901.1	Temporary Seeding	AC	22.25
T-901.2	Permanent Seeding	AC	13.45
T-901.3	Seeding, Staging Area	AC	1
T-905.1	Topsolling (Obtain on Site or Removed from Stockpile)	CY	390
T-905.2	Topsolling, Staging Area	CY	2762
L-108.1	No. 6 AWG Counterpoise, Including Grounding Rods, Installed	LF	1696
L-108.2	No. 8 AWG, 5kV, L-824 Type C Cable (Runway)	LF	453
L-108.3	Remove Abandoned Communication Line	LF	1957
L-110.1	Concrete Encased Electrical Duct Bank, 2W-2"	LF	371
L-110.2	Concrete Encased Electrical Duct Bank, 4W-4"	LF	445
L-110.3	Concrete Encased Conduit, 1W-2" Type II PVC	LF	1675
L-110.4	Non-Encased Conduit, 1W-2" Type II PVC	LF	353
L-115.1	Aircraft Rated Manhole, 4'x4' with Spring Assisted Cover	EA	2
L-115.2	Electrical Junction Structure with Aircraft rated Blank Cover, without Drainage	EA	1
L-115.3	Electrical Junction Structure with Blank Cover, without Drainage	EA	1
L-125.1	In-Pavement LED Medium Intensity Runway Edge Light, L-852D (L)	EA	2
L-125.2	Base Mounted, LED Medium Intensity Taxiway Edge Light With Drainage	EA	25
L-125.3	Base Mounted, LED Medium Intensity Taxiway Edge Light Without Drainage	EA	11
L-125.4	Miscellaneous Lighting Equipment	LS	1
L-125.5	Remove Edge Light Fixture and/or Base Can	EA	5
L-125.6	Taxiway Guidance Sign, 1 Module, Size 2, Style 3, Mode 2	EA	2
L-125.7	Taxiway Guidance Sign, 2 Module, Size 2, Style 3, Mode 3	EA	2
L-125.9	Taxiway Guidance Sign, 4 Module, Size 2, Style 3, Mode 3	EA	1
L-125.10	Non-Lighted Taxiway End Sign	EA	1
L-125.11	Edge Light Number Tags	EA	36

SUMMARY OF QUANTITIES			
SPEC.	ITEM	UNIT	QUANTITY
<b>AGS TAXIWAY G (BASE BID-CONCRETE SHOULDERS)</b>			
C-100.1	Contractor Quality Control Program	LS	1
C-102.1a	Installation, Maintenance, and Removal of Silt Fence or Silt Sock	LF	11754
C-102.1b	Construct, Maintain, and Remove Inlet Sediment Trap	EA	48
C-102.1c	Construct, Maintain, and Remove Construction Exit	EA	1
C-102.1d	Water Quality Monitoring and Sampling	EA	36
C-102.1e	Water Quality Inspections	EA	18
C-102.1f	Erosion Control Mobilization	LS	1
C-102.1g	Emergency Erosion Control Mobilization	LS	1
C-102.1h	Permanent Water Quality Inserts	EA	14
C-105.1	Mobilization, Cleanup, and Demobilization	LS	1
C-105.2	Airfield Safety and Traffic Control	LS	1
P-101.1	Asphaltic Concrete Pavement Removal, Full Depth, Off Site	SY	1972
P-101.2	Cold Milling	SY	233
P-152.1	Unclassified Excavation, Remove Off-Site	CY	3290
P-152.3	Subgrade Preparation	SY	16190
P-152.4	Unsuitable/Over excavation	CY	5000
P-154.1	Uncrushed Aggregate Base Course (6")	CY	2698
P-306.1	Lean Concrete Base Course (5")	SY	11170
P-501.1	Portland Cement Concrete Pavement (14")	SY	15402
X-501.1	Portland Cement Concrete Curing Facility	LS	1
P-605.1	Joint Sealing Filler	LF	17577
P-620.1	Permanent Pavement Markings	SF	9393
P-620.2	Temporary Pavement Markings	SF	9393
P-620.3	Reflective Media	LBS	280
P-620.4	Thermoplastic Preformed Surface Sign	EA	2
D-701.1	Concrete Sewer Pipe, 18-Inch, Class V	LF	70
D-705.1	6-Inch Perforated Polyethylene Underdrain Pipe, Schedule 40, Complete	LF	1165
D-705.2	Underdrain Clean-out	EA	8
D-751.2	Airfield Inlet with Aircraft Rated Grate	EA	1
D-751.3	Adjust Storm Manhole/Inlet to Grade	EA	1
D-751.4	Connect Storm Sewer Pipe to Existing Storm Structure	EA	1
T-901.1	Temporary Seeding	AC	22.25
T-901.2	Permanent Seeding	AC	13.45
T-901.3	Seeding, Staging Area	AC	1
T-905.1	Topsolling (Obtain on Site or Removed from Stockpile)	CY	390
T-905.2	Topsolling, Staging Area	CY	2762
L-108.1	No. 6 AWG Counterpoise, Including Grounding Rods, Installed	LF	1696
L-108.2	No. 8 AWG, 5kV, L-824 Type C Cable (Runway)	LF	453
L-108.3	Remove Abandoned Communication Line	LF	1957
L-110.1	Concrete Encased Electrical Duct Bank, 2W-2"	LF	371
L-110.2	Concrete Encased Electrical Duct Bank, 4W-4"	LF	445
L-110.3	Concrete Encased Conduit, 1W-2" Type II PVC	LF	1675
L-110.4	Non-Encased Conduit, 1W-2" Type II PVC	LF	353
L-115.1	Aircraft Rated Manhole, 4'x4' with Spring Assisted Cover	EA	2
L-115.2	Electrical Junction Structure with Aircraft rated Blank Cover, without Drainage	EA	25
L-115.3	Electrical Junction Structure with Blank Cover, without Drainage -- per each	EA	11
L-125.1	In-Pavement LED Medium Intensity Runway Edge Light, L-852D (L)	EA	2
L-125.2	Base Mounted, LED Medium Intensity Taxiway Edge Light With Drainage	EA	25
L-125.3	Base Mounted, LED Medium Intensity Taxiway Edge Light Without Drainage	EA	11
L-125.4	Miscellaneous Lighting Equipment	LS	1
L-125.5	Remove Edge Light Fixture and/or Base Can	EA	5
L-125.6	Taxiway Guidance Sign, 1 Module, Size 2, Style 3, Mode 2	EA	2
L-125.7	Taxiway Guidance Sign, 2 Module, Size 2, Style 3, Mode 2	EA	2
L-125.9	Taxiway Guidance Sign, 4 Module, Size 2, Style 3, Mode 2	EA	1
L-125.10	Non-Lighted Taxiway End Sign	EA	1
L-125.11	Edge Light Number Tags	EA	36

NOTE: SUMMARY OF QUANTITIES TABLES ARE DESIGNED TO BE USED AS A REFERENCE IN THE FIELD. VALUES OUTLINED IN THE BID FORM TAKE SUPERIORITY OVER ANY VALUES SHOWN IN THE QUANTITY TABLES

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SUMMARY OF QUANTITIES			
SPEC.	ITEM	UNIT	QUANTITY
<b>AGS TAXIWAY G (ALTERNATE 1 - ASPHALT SHOULDERS)</b>			
C-100.1	Contractor Quality Control Program	LS	1
C-102.1a	Installation, Maintenance, and Removal of Silt Fence or Silt Sock	LF	11754
C-102.1b	Construct, Maintain, and Remove Inlet Sediment Trap	EA	48
C-102.1c	Construct, Maintain, and Remove Construction Exit	EA	1
C-102.1d	Water Quality Monitoring and Sampling	EA	36
C-102.1e	Water Quality Inspections	EA	18
C-102.1f	Erosion Control Mobilization	LS	1
C-102.1g	Emergency Erosion Control Mobilization	LS	1
C-105.1	Mobilization, Cleanup, and Demobilization	LS	1
C-105.2	Airfield Safety and Traffic Control	LS	1
P-101.1	Asphaltic Concrete Pavement Removal, Full Depth, Off Site	SY	488
P-101.3	Remove Existing 36" RCP	LF	102
P-152.2	Unclassified Excavation, Select Fill from On-Site Material	CY	2844
P-152.3	Subgrade Preparation	SY	37137
P-152.4	Unsuitable/Over excavation	CY	10000
P-154.1	Uncrushed Aggregate Base Course (6")	CY	3998
P-154.2	Uncrushed Aggregate Base Course (12")	CY	4383
P-306.1	Lean Concrete Base Course (5")	SY	23536
P-401.1	Asphalt Concrete Surface Course (4")	TON	2959
P-403.1	Asphalt Concrete Base Course (4")	TON	2959
P-501.1	Portland Cement Concrete Pavement (14")	SY	22182
X-501.1	Portland Cement Concrete Curing Facility	LS	1
P-602.1	Emulsified Asphalt Prime Coat	Gal	1315
P-603.1	Emulsified Asphalt Tack Coat	Gal	657
P-605.1	Joint Sealing Filler	LF	32861
P-620.1	Permanent Pavement Markings	SF	81652
P-620.2	Temporary Pavement Markings	SF	81652
P-620.3	Reflective Media	LBS	5532
P-620.4	Thermoplastic Preformed Surface Sign	EA	2
P-620.5	Temporary Displaced Threshold	LS	1
D-701.1	Concrete Sewer Pipe, 18-inch, Class V	LF	231
D-701.2	Concrete Sewer Pipe, 24-inch, Class V	LF	89
D-705.1	6-Inch Perforated Polyethylene Underdrain Pipe, Schedule 40, Complete	LF	3281
D-705.2	Underdrain Clean-out	EA	14
D-751.1	Aircraft Ranted Manhole with Aircraft Rated Lid	EA	2
D-751.2	Airfield Inlet with Aircraft Rated Grate	EA	1
D-751.3	Adjust Storm Manhole/Inlet to Grade	EA	1
D-751.4	Connect Storm Sewer Pipe to Existing Storm Structure	EA	1
T-901.1	Temporary Seeding	AC	22.25
T-901.2	Permanent Seeding	AC	13.45
T-901.3	Seeding, Staging Area	AC	1
T-905.1	Topsolling (Obtain on Site or Removed from Stockpile)	CY	871
T-905.2	Topsolling, Staging Area	CY	2762
L-108.1	No. 6 AWG Counterpoise, Including Grounding Rods, Installed	LF	4463
L-108.2	No. 8 AWG, 5kV, L-824 Type C Cable (Runway)	LF	6543
L-110.1	Concrete Encased Electrical Duct Bank, 2W-2"	LF	362
L-110.2	Concrete Encased Electrical Duct Bank, 4W-4"	LF	372
L-110.3	Concrete Encased Conduit, 1W-2" Type II PVC	LF	4102
L-110.4	Non-Encased Conduit, 1W-2" Type II PVC	LF	555
L-115.1	Aircraft Rated Manhole, 4'x4' with Spring Assisted Cover	EA	2
L-115.3	Electrical Junction Structure with Blank Cover, without Drainage	EA	1
L-125.2	L-861T(L) Elevated, 14" HT, LED MITL on New L-867B Base Can with Drainage	EA	22
L-125.3	L-861T(L) Elevated, 14" HT, LED MITL on New L-867B Base Can without Drainage	EA	28
L-125.4	Miscellaneous Lighting Equipment	LS	1
L-125.6	Taxiway Guidance Sign, 1 Module, Size 2, Style 3, Mode 2	EA	2
L-125.7	Taxiway Guidance Sign, 2 Module, Size 2, Style 3, Mode 2	EA	1
L-125.8	Taxiway Guidance Sign, 3 Module, Size 2, Style 3, Mode 2	EA	1
L-125.9	Taxiway Guidance Sign, 4 Module, Size 2, Style 3, Mode 2	EA	4
L-125.10	Edge Light Number Tags	EA	50

SUMMARY OF QUANTITIES			
SPEC.	ITEM	UNIT	QUANTITY
<b>AGS TAXIWAY G (ALTERNATE 1 - CONCRETE SHOULDERS)</b>			
C-100.1	Contractor Quality Control Program	LS	1
C-102.1a	Installation, Maintenance, and Removal of Silt Fence or Silt Sock	LF	11754
C-102.1b	Construct, Maintain, and Remove Inlet Sediment Trap	EA	48
C-102.1c	Construct, Maintain, and Remove Construction Exit	EA	1
C-102.1d	Water Quality Monitoring and Sampling	EA	36
C-102.1e	Water Quality Inspections	EA	18
C-102.1f	Erosion Control Mobilization	LS	1
C-102.1g	Emergency Erosion Control Mobilization	LS	1
C-105.1	Mobilization, Cleanup, and Demobilization	LS	1
C-105.2	Airfield Safety and Traffic Control	LS	1
P-101.1	Asphaltic Concrete Pavement Removal, Full Depth, Off Site	SY	474
P-101.3	Remove Existing 36" RCP	LF	102
P-152.2	Unclassified Excavation, Select Fill from On-Site Material	CY	2844
P-152.3	Subgrade Preparation	SY	37137
P-152.4	Unsuitable/Over excavation	CY	10000
P-154.1	Uncrushed Aggregate Base Course (6")	CY	6189
P-306.1	Lean Concrete Base Course (5")	SY	23536
P-501.1	Portland Cement Concrete Pavement (14")	SY	35331
X-501.1	Portland Cement Concrete Curing Facility	LS	1
P-605.1	Joint Sealing Filler	LF	32861
P-620.1	Permanent Pavement Markings	SF	81652
P-620.2	Temporary Pavement Markings	SF	81652
P-620.3	Reflective Media	LBS	5532
P-620.4	Thermoplastic Preformed Surface Sign	EA	2
P-620.5	Temporary Displaced Threshold	LS	1
D-701.1	Concrete Sewer Pipe, 18-inch, Class V	LF	231
D-701.2	Concrete Sewer Pipe, 24-inch, Class V	LF	89
D-705.1	6-Inch Perforated Polyethylene Underdrain Pipe, Schedule 40, Complete	LF	3281
D-705.2	Underdrain Clean-out	EA	14
D-751.1	Aircraft Ranted Manhole with Aircraft Rated Lid	EA	2
D-751.2	Airfield Inlet with Aircraft Rated Grate	EA	1
D-751.3	Adjust Storm Manhole/Inlet to Grade	EA	1
D-751.4	Connect Storm Sewer Pipe to Existing Storm Structure	EA	1
T-901.1	Temporary Seeding	AC	22.25
T-901.2	Permanent Seeding	AC	13.45
T-901.3	Seeding, Staging Area	AC	1
T-905.1	Topsolling (Obtain on Site or Removed from Stockpile)	CY	871
T-905.2	Topsolling, Staging Area	CY	2762
L-108.1	No. 6 AWG Counterpoise, Including Grounding Rods, Installed	LF	4463
L-108.2	No. 8 AWG, 5kV, L-824 Type C Cable (Runway)	LF	6543
L-110.1	Concrete Encased Electrical Duct Bank, 2W-2"	LF	362
L-110.2	Concrete Encased Electrical Duct Bank, 4W-4"	LF	372
L-110.3	Concrete Encased Conduit, 1W-2" Type II PVC	LF	4102
L-110.4	Non-Encased Conduit, 1W-2" Type II PVC	LF	555
L-115.1	Aircraft Rated Manhole, 4'x4' with Spring Assisted Cover	EA	2
L-115.3	Electrical Junction Structure with Blank Cover, without Drainage	EA	1
L-125.2	L-861T(L) Elevated, 14" HT, LED MITL on New L-867B Base Can with Drainage	EA	22
L-125.3	L-861T(L) Elevated, 14" HT, LED MITL on New L-867B Base Can without Drainage	EA	28
L-125.4	Miscellaneous Lighting Equipment	LS	1
L-125.6	Taxiway Guidance Sign, 1 Module, Size 2, Style 3, Mode 2	EA	2
L-125.7	Taxiway Guidance Sign, 2 Module, Size 2, Style 3, Mode 2	EA	1
L-125.8	Taxiway Guidance Sign, 3 Module, Size 2, Style 3, Mode 2	EA	1
L-125.9	Taxiway Guidance Sign, 4 Module, Size 2, Style 3, Mode 2	EA	4
L-125.10	Edge Light Number Tags	EA	50

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**AUGUSTA REGIONAL AIRPORT  
CONSTRUCT TAXIWAY G**

1501 AVIATION WAY  
AUGUSTA, GA 30906-9620

REVISION  
ISSUED FOR BID

NOT FOR CONSTRUCTION

MR#140: 0119700-170869.02  
DATE: DECEMBER 6, 2022  
DESIGNED BY: EJS  
DRAWN BY: NJH  
CHECKED BY: DES

SHEET CONTENTS  
PROJECT QUANTITY  
TABLES

SHEET NO.

**G-062**

NOTE: SUMMARY OF QUANTITIES TABLES ARE DESIGNED TO BE USED AS A REFERENCE IN THE FIELD. VALUES OUTLINED IN THE BID FORM TAKE SUPERIORITY OVER ANY VALUES SHOWN IN THE QUANTITY TABLES



**COORDINATION**

**1. PRECONSTRUCTION CONFERENCE.**

- A. A PRECONSTRUCTION CONFERENCE WILL BE CONVENED AND CONDUCTED BY THE AIRPORT AUTHORITY AND CONSTRUCTION ADMINISTRATION TEAM (CA TEAM). THIS CONFERENCE WILL BE USED TO DISCUSS OPERATIONAL SAFETY, TESTING, QUALITY CONTROL, QUALITY ACCEPTANCE, SECURITY, SAFETY, LABOR REQUIREMENTS, ENVIRONMENTAL FACTORS, AND OTHER FACTORS THAT WILL PERTAIN TO THIS CONSTRUCTION PROJECT.
B. THE PRECONSTRUCTION CONFERENCE WILL BE CONDUCTED AS SOON AS PRACTICABLE AFTER THE CONTRACT HAS BEEN AWARDED AND HELD BEFORE THE NOTICE TO PROCEED IS GIVEN TO THE CONTRACTOR.
C. PARTICIPANTS IN THE PRECONSTRUCTION CONFERENCE SHALL INCLUDE: AIRPORT STAFF, AIRPORT OPERATIONS, DESIGN TEAM, CONSTRUCTION ADMINISTRATION TEAM, FAA AIR TRAFFIC CONTROL, CONTRACTOR, AND SUBCONTRACTORS.
D. THE INFORMATION COVERED IN THIS MEETING WILL FOLLOW THE GUIDELINES OUTLINED IN AC 150/5300-9B, "PREDESIGN, PREBID, AND PRECONSTRUCTION CONFERENCES FOR AIRPORT GRANT PROJECTS."

**2. CONTRACTOR PROGRESS MEETINGS**

- A. DURING THE DURATION OF THE PROJECT, WEEKLY CONTRACTOR PROGRESS MEETINGS WILL BE HELD AND CONDUCTED BY THE CONSTRUCTION ADMINISTRATION TEAM.
B. THE PROGRESS MEETINGS WILL COVER PROJECT SCHEDULE, CONSTRUCTION SAFETY, ISSUES, ETC.

**3. FAA AIR TRAFFIC CONTROL ORGANIZATION COORDINATION**

- A. COMMUNICATION WITH THE FAA AIR TRAFFIC CONTROL TOWER WILL BE COORDINATED BY THE AIRPORT STAFF, CONSTRUCTION ADMINISTRATION TEAM AND/OR THE AIRPORT OPERATIONS DIVISION.
B. THE FAA AIR TRAFFIC CONTROL TOWER PERSONNEL WILL BE INVITED TO ATTEND THE PRECONSTRUCTION CONFERENCE AT WHICH TIME THE OVERALL CONSTRUCTION SCHEDULE WILL BE PRESENTED.
C. A MEETING WILL BE SCHEDULED WITH THE FAA AIR TRAFFIC CONTROL PERSONNEL PRIOR TO THE START OF EACH MAJOR CONSTRUCTION PHASE WHICH SIGNIFICANTLY IMPACTS/MODIFIES AIRFIELD CLOSURES THROUGHOUT THE DURATION OF THE CONSTRUCTION PROJECT. PARTICIPANTS IN THESE MEETINGS SHALL INCLUDE: AIRPORT STAFF, AIRPORT OPERATIONS, DESIGN TEAM, CONSTRUCTION ADMINISTRATION TEAM, FAA AIR TRAFFIC CONTROL, CONTRACTOR, AND SUBCONTRACTORS.

**ARFF VEHICLE ACCESS ROUTE**

- 1. CROSS AIRPORT ACCESS FOR ARFF VEHICLES SHALL NOT BE IMPACTED DURING THIS PROJECT.

**PROTECTION OF NAVIGATION AIDS (NAVAIDS)**

- 1. THERE ARE VARIOUS FAA AND AIRPORT OWNED NAVIGATIONAL AIDS LOCATED WITHIN THE PROJECT LIMITS. CONTRACTOR WILL TAKE ALL PRECAUTIONS NECESSARY TO PROTECT EQUIPMENT, INCLUDING, BUT NOT LIMITED TO: COORDINATION WITH FAA TECH OPS PERSONNEL TO IDENTIFY ABOVE GROUND EQUIPMENT AND BELOW GROUND CABLING, AND BARRICADING AROUND EQUIPMENT AS NECESSARY TO MAINTAIN SEPARATION BETWEEN CONTRACTORS' EQUIPMENT AND NAVAID EQUIPMENT. ANY DAMAGE TO NAVAIDS WILL BE REPAIRED OR REPLACED AT CONTRACTORS' EXPENSE TO THE SATISFACTION OF THE OWNER. A NOTAM WILL BE ISSUED TO CLOSE RUNWAYS AFFECTED BY UNANTICIPATED POWER OUTAGES OR DAMAGING OF NAVAIDS, WITH THE CONTRACTOR IMMEDIATELY RESTORING POWER THROUGH COORDINATION OF THE OWNER.

**CONTRACTOR ACCESS**

**CONSTRUCTION SITE ACCESS AND HAUL ROAD**

- 1. HAUL ROADS TO BE USED ON THIS PROJECT ARE INDICATED ON THE DRAWINGS OR OTHERWISE SPECIFICALLY AUTHORIZED BY THE CA TEAM. THE CONTRACTOR SHALL CONFINE ALL VEHICLES AND EQUIPMENT TO THE DESIGNATED CONSTRUCTION AREAS, STAGING AREAS AND HAUL ROUTES.
2. ACCESS POINTS TO THE PROJECT SITE ARE SHOWN ON THE DRAWINGS. THE SPECIFIED GATES SHALL BE MONITORED BY A CONTRACTOR SUPPLIED GUARD DURING ALL CONTRACTOR OPERATIONS WHILE THE GATE IS OPEN OR UNLOCKED.
3. THE CONTRACTOR SHALL RESTORE ALL TURFED AND PAVED AREAS USED FOR HAUL ROADS TO THEIR ORIGINAL CONDITION, INCLUDING ESTABLISHMENT OF NEW TURF. ALL COSTS FOR CONSTRUCTING, REMOVING, AND RESTORING OF HAUL ROADS REQUIRED FOR THE COMPLETION OF THE WORK SHALL BE BY THE CONTRACTOR UNDER MOBILIZATION. THE EXISTING CONDITION OF ALL ANTICIPATED HAUL ROUTES SHALL BE DOCUMENTED BY THE CONTRACTOR PRIOR TO HAULING.
4. THE CONTRACTOR SHALL NOT PERMIT ANY UNAUTHORIZED CONSTRUCTION PERSONNEL OR TRAFFIC ON THE PROJECT SITE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TRAFFIC CONTROL TO AND FROM THE PROJECT SITE. CONTRACTOR PROVIDED DIRECTIONAL SIGNAGE AT THE ACCESS GATES AND ALONG THE DELIVERY ROUTE TO THE STAGING AREA AND PROJECT SITE SHALL BE SUBMITTED AND REVIEWED BY THE DESIGNER AND AIRPORT OPERATIONS PRIOR TO INSTALLATION.
5. ALL CONTRACTOR MATERIAL ORDERS FOR DELIVERY TO THE SITE SHALL BE DIRECTED TO THE ACCESS POINT IDENTIFIED OR CONTRACTOR STAGING AREA.
6. THE CONTRACTOR, THROUGH AIRPORT OPERATIONS/SECURITY, SHALL ESTABLISH AND MAINTAIN A LIST OF CONTRACTOR AND SUB-CONTRACTOR VEHICLES AUTHORIZED TO OPERATE ON THE PROJECT SITE. VEHICLE USE PERMITS SHALL BE OBTAINED BY THE CONTRACTOR IN ACCORDANCE WITH AIRPORT PROCEDURES.
7. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE THE USE OF OFF-SITE ROUTES (STATE HIGHWAYS, COUNTY ROADS OR CITY STREETS) WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE.
8. ALL VEHICLES USING HAUL ROUTES INCLUDING OFF-SITE ROUTES, SHALL BE COVERED TO PREVENT BLOWING AWAY OR SPILLAGE OF LOOSE MATERIAL. ALL SPILLAGES ON PUBLIC ROADWAYS AND SITE ROADS SHALL BE PROMPTLY CLEANED UP AND LEGALLY DISPOSED OF AT NO ADDITIONAL COST TO THE SPONSOR.
9. THE CONTRACTOR WILL NOT BE PERMITTED TO USE ANY ACCESS OR HAUL ROADS OTHER THAN THOSE DESIGNATED ON THE CONTRACT DRAWINGS. EMERGENCY ACCESS BY AIRCRAFT RESCUE AND FIRE FIGHTING (ARFF) RIGHT-OF-WAY ON ACCESS ROADS, HAUL ROADS, TAXIWAYS, AND RUNWAYS SHALL NOT BE IMPEDED AT ANY TIME.

**CONTRACTOR STAGING AREA**

- 1. THE LIMITS OF CONSTRUCTION, CONTRACTOR'S STAGING AREA AND STOCKPILING AREAS REQUIRED FOR THE CONTRACTOR'S EXCLUSIVE USE DURING CONSTRUCTION ARE SHOWN ON THE PLANS. ADDITIONAL AREAS MAY BE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE DESIGNER AND OWNER. THE CONTRACTOR SHALL PROVIDE DEVICES VISIBLE FOR BOTH DAY AND NIGHT USE TO DELINEATE THE PERIMETER OF ALL SUCH AREAS.
2. CONTRACTOR SHALL INSTALL A MINIMUM 3" THICK GRADED AND COMPACTED GRAVEL BED OR RECYCLED ASPHALT PAVEMENT TO THE LIMITS OF THE CONTRACTOR STAGING AREA THAT IS NOT IMPERVIOUS. THE CONTRACTOR SHALL PROTECT ALL EXISTING UTILITIES AND DRAINAGE STRUCTURES FROM ANY DAMAGE CAUSED WHILE THE AREA IS BEING USED AS A CONSTRUCTION STAGING AREA. ALL DAMAGE SHALL BE REPAIRED TO THE SATISFACTION OF THE AIRPORT AUTHORITY AND AT NO ADDITIONAL COST TO THE AIRPORT AUTHORITY. A STAGING AREA LAYOUT PLAN SHALL BE SUBMITTED TO AIRPORT AND THE DESIGNER FOR REVIEW AND APPROVAL PRIOR TO CONSTRUCTION.
3. THE CONTRACTOR SHALL NOT PARK EQUIPMENT OR STORE MATERIALS WITHIN 10 FEET OF AOA FENCE AND/OR PERIMETER FENCE.
4. THE CONTRACTOR SHALL MAINTAIN EXISTING DRAINAGE PATTERNS AT THE STAGING AND STOCKPILE AREAS AND PROVIDE TEMPORARY ROUTING OF STORMWATER AROUND THE AREAS.
5. IN ORDER TO PREVENT SEDIMENT FROM LEAVING THE CONTRACTOR STAGING AREA, THE CONTRACTOR SHALL INSTALL TEMPORARY SILT FENCE AROUND THE STAGING AREA AND PROVIDE INLET PROTECTION DEVICES FOR ALL EXISTING DRAINAGE STRUCTURES IN ACCORDANCE WITH THE MANUAL FOR SEDIMENT AND EROSION CONTROL IN GEORGIA (LATEST EDITION) AND GDOT STANDARD DETAILS.
6. ALL EROSION CONTROL MEASURES WITHIN THE CONTRACTOR STAGING AREA SHALL BE INCIDENTAL TO SPECIFICATION ITEM C-102 EROSION CONTROL MOBILIZATION.
7. CONTRACTOR SHALL INFORM THE CA TEAM ON A DAILY BASIS OF THE DAILY CONSTRUCTION ACTIVITIES, AS WELL AS, UPCOMING ACTIVITIES WITH THE INTENT TO LIMIT AIRPORT OPERATION CONFLICTS.
8. THE CONTRACTOR SHALL PROVIDE TEMPORARY UTILITIES TO THE SITE, INCLUDING WATER AND ELECTRIC FOR THE BATCH PLANT. ALL COSTS ASSOCIATED WITH TEMPORARY UTILITIES SHALL BE INCIDENTAL TO X-100 MOBILIZATION.
9. ALL STAGING AREAS SHALL BE INSPECTED AND APPROVED BY AIRPORT FIRE MARSHALL. THE CONTRACTOR SHALL SUPPLY ANY AND ALL FIRE FIGHTING EQUIPMENT, PROTECTION AND SAFETY EQUIPMENT/SUPPLIES AS REQUESTED BY THE AIRPORT AUTHORITY'S FIRE MARSHALL WITHIN 24 HOURS AFTER REQUESTED.

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- 10. CONTRACTOR SHALL SUPPLY COVERED TRASH AND RUBBISH DUMPSTERS AND ALL OTHER CONTAINERS FOR REMOVAL OF TRASH, RUBBISH, AND DEBRIS RESULTING FROM THE WORK OF THE CONTRACT. THE CONTRACTOR SHALL NOT ALLOW DUMPSTERS TO OVERFLOW.
11. THE CONTRACTOR SHALL COMPLETELY CLEAN UP AND RESTORE THE ENTIRE STAGING AND STORAGE AREAS, AS APPROVED BY THE DESIGNER PRIOR TO FINAL COMPLETION. ALL UNUSED MATERIALS SHALL BE REMOVED FROM THE PROJECT SITE AT THE CONTRACTORS EXPENSE, UNLESS PRIOR APPROVAL HAS BEEN GIVEN FROM THE AIRPORT AND THE STAGING AREA GRADED SMOOTH, SLOPED TO DRAIN AND SEEDED. INCIDENTAL TO C-105 MOBILIZATION, CLEANUP, AND DEMOBILIZATION.

**CONTRACTOR EMPLOYEE AND EQUIPMENT PARKING**

- 1. ALL VEHICLES SHALL BE PARKED AND SERVICED IN THE DESIGNATED STAGING AND EMPLOYEE PARKING AREAS SHOWN ON THE PLANS. THE CONTRACTOR IS RESPONSIBLE FOR TRANSPORTING EMPLOYEES FROM THESE AREAS TO THE JOBSITE. ALL SERVICING SHALL BE IN ACCORDANCE WITH APPLICABLE FEDERAL, STATE, AND LOCAL REGULATIONS.
2. ALL MATERIALS AND EQUIPMENT WHEN NOT IN USE SHALL BE PLACED IN APPROVED AREAS WHERE THEY WILL NOT CONSTITUTE A HAZARD TO AIRCRAFT OPERATIONS AND NOT PENETRATE CLEARANCE HEIGHT RESTRICTIONS AS SHOWN ON THE CONSTRUCTION PLAN. ALL EQUIPMENT SHALL BE PARKED IN THE APPROPRIATE AREA WHEN NOT IN USE.

**VEHICLE CONDITION**

- 1. VEHICLES AND EQUIPMENT THAT ARE DEEMED A POTENTIAL HAZARD BY THE CA TEAM OR AIRPORT SHALL BE REMOVED FROM THE JOB SITE AND STAGED PROPERLY AT THE REQUEST OF THE CA TEAM. VEHICLES AND EQUIPMENT THAT LEAK ANY AUTOMOTIVE FLUID INCLUDING, BUT NOT LIMITED TO, OIL, HYDRAULIC FLUID, TRANSMISSION FLUID, GEAR OIL, GASOLINE, AND DIESEL WILL BE REMOVED TO THE STAGING AREA AND NOT ALLOWED TO OPERATE ON ANY PAVED SURFACE. IF THE VEHICLE CANNOT BE REPAIRED WITHIN A FEW DAYS THE VEHICLE SHALL BE REMOVED FROM THE AIRPORT. LEAKING FLUIDS ON PAVEMENTS DAMAGE THE PAVEMENT.
2. THE CONTRACTOR SHALL CLEANUP AT CONTRACTOR'S EXPENSE. ANY AND ALL LEAKS OR SPILLS. LEAKS ON PAVED SURFACES SHALL BE CLEANED UP IMMEDIATELY. SIGNIFICANT LEAK SPOTS ON PAVEMENT, AS DETERMINED BY THE CA TEAM, SHALL BE REPLACED WITH NEW PAVEMENT. ASPHALT WILL REQUIRE MILLING AND PLACEMENT OF NEW BITUMINOUS MATERIAL. PCC WILL REQUIRE SAW, REMOVAL AND REPAIR AS DIRECTED BY THE CA TEAM. DIRT OR GRAVEL AREAS WILL REQUIRE REMOVAL, LEGAL DISPOSAL AND REPLACEMENT OF THE AREA WITH SIMILAR APPROVED MATERIALS.

**LOCATION OF STOCKPILED MATERIALS**

- 1. THERE SHALL NOT BE ANY STOCKPILED MATERIALS IN THE ACTIVE RUNWAY OFA, TAXIWAY OFA, OR NAVAID CRITICAL AREAS. STOCKPILED MATERIAL OR EQUIPMENT SHALL NOT BE STORED NEAR AIRCRAFT TURNING AREAS OR OPERATIONAL MOVEMENT AREAS. APRONS, OR EXCAVATIONS AND TRENCHES. STOCKPILED MATERIALS SHALL NOT BE STORED NEAR NAVAIDS, VISUAL OR APPROACH AIDS, NOR SHALL THEY OBSTRUCT THE ATCTS LINE OF SIGHT TO ANY RUNWAY OR TAXIWAY. THE CONTRACTOR SHALL ENSURE THAT STOCKPILED MATERIALS DO NOT CAUSE DEGRADED OR HAZARDOUS CONDITIONS TO AIRPORT OPERATIONS SAFETY. THIS INCLUDES DETERMINING AND VERIFYING THAT STOCKPILED MATERIALS ARE STORED AT AN APPROVED LOCATION, THAT THEY ARE PROPERLY STOMED TO PREVENT FOREIGN OBJECT DEBRIS (FOD), ATTRACTION BY WILDLIFE, OR OBSTRUCTION OF AIR OPERATIONS EITHER BY THEIR PROXIMITY TO NAVAIDS OR TO AIRCRAFT MOVEMENT AREAS.
2. ALL STOCKPILED MATERIAL(S)/SUPPLIES SHALL BE CONSTRAINED IN A MANNER TO PREVENT MOVEMENT RESULTING FROM AIRCRAFT BLAST OR WIND CONDITIONS. MATERIAL(S)/SUPPLIES SHALL NOT BE STORED WITHIN 500 FEET OF AIRCRAFT TURNING AREAS OR MOVEMENT AREAS. STOCKPILED MATERIAL(S)/SUPPLIES SHALL NOT EXCEED 16 FEET IN HEIGHT UNLESS THE CONTRACTOR HAS COMPLIED WITH ALL REQUIREMENTS FOR AIRSPACING AND SECURED APPROVAL FROM AIRPORT OPERATIONS. ALL MATERIAL(S)/SUPPLIES SHALL BE POSITIONED SO IT WILL NOT OBSTRUCT THE LINE OF SIGHT FROM THE CONTROL TOWER TO THE MOVEMENT AREA. MARKING AND LIGHTING SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS CONTAINED IN THESE CONSTRUCTION PLANS. LOOSE MATERIALS, SUCH AS STRAW, SHALL BE COVERED AS DIRECTED BY CA TEAM.

**VEHICLE AND PEDESTRIAN OPERATIONS**

- 1. VEHICLE AND ACCESS ROUTES FOR AIRPORT CONSTRUCTION SHALL BE CONTROLLED AS NECESSARY TO PREVENT INADVERTENT OR UNAUTHORIZED ENTRY OF PERSONS, VEHICLES OR ANIMALS ONTO AIR OPERATION AREAS. NO VEHICLE SHALL ENTER THE AIR OPERATIONS AREA EXCEPT AT PREDETERMINED LOCATIONS. THE AMOUNT OF CONSTRUCTION TRAFFIC WILL REQUIRE THE CONTRACTOR TO USE A GUARD AT ACCESS GATES AND A FLAG PERSON TO CONTROL TRAFFIC CROSSING TAXIWAYS AND OTHER AIRCRAFT MOVEMENT AREAS. CONTRACTOR PERSONNEL WHO OPERATE VEHICLES IN THE AOA SHALL COMPLY WITH AC 150/5210-5 (LATEST VERSION), PAINTING, MARKING AND LIGHTING OF VEHICLES USED ON AN AIRPORT.
2. ALL CONSTRUCTION VEHICLES/MECHANIZED EQUIPMENT SHALL HAVE A VEHICLE PASS AS DETERMINED BY AIRPORT OPERATIONS DISPLAYED.
3. ALL CONSTRUCTION VEHICLES/MECHANIZED EQUIPMENT AUTHORIZED WITHIN THE MOVEMENT AREA OR RELATED SAFETY AREAS SHALL BE MARKED WITH A CLEAN 3' X 3' ORANGE AND WHITE CHECKERED FLAG WITH EACH BOX BEING 1' SQUARE, LOCATED ON THE UPPERMOST PORTION OF THE VEHICLE/MOTORIZED EQUIPMENT, OR BE ESCORTED BY A VEHICLE 90 EQUIPPED.
4. DURING NIGHTTIME HOURS, ALL EQUIPMENT OPERATING ON THE AIRPORT EXCEEDING 15 FEET IN HEIGHT SHALL BE LIT WITH A RED OBSTRUCTION LIGHT LOCATED ON THE UPPERMOST PORTION OF THE EQUIPMENT.
5. VEHICLES/MECHANIZED EQUIPMENT AUTHORIZED ON THE MOVEMENT AREA (RUNWAYS, TAXIWAYS, AND RAMPS) AND/OR ASSOCIATED SAFETY AREAS SHALL BE EQUIPPED WITH AN ELECTRICALLY POWERED, AMBER COLOR, 360-DEGREE OMNI-DIRECTION LIGHT, MOUNTED ON THE VEHICLE SUCH THAT IT IS CONSPICUOUS FROM ANY DIRECTION.

**REQUIRED ESCORTS**

- 1. THE CONTRACTOR MUST PROVIDE AN ADEQUATE NUMBER OF ESCORTS FOR MATERIAL DELIVERIES ALONG HAUL ROUTES AND THE MOVEMENTS OF THE CONTRACTOR'S VEHICLES/MECHANIZED EQUIPMENT AND PERSONNEL WITHIN THE MOVEMENT AREA AND NON-MOVEMENT AREAS AS AUTHORIZED BY THE AIRPORT OPERATIONS. EACH ESCORT MAY ACCOMPANY A MAXIMUM OF 5 VEHICLES AT A TIME ACROSS MOVEMENT AREAS AND MUST MAINTAIN VISUAL ACCESS AT ALL TIMES.
2. DURING ANY ABSENCE OF THE APPROVED ESCORT(S) OR FOR PERIODS THAT THEY ARE UNABLE TO PERFORM THEIR SPECIFIED DUTIES, ALL WORK WITHIN THE MOVEMENT AREA AND ASSOCIATED SAFETY AREAS FOR PROJECTS SHALL STOP. ADDITIONALLY, ALL PERSONNEL AND EQUIPMENT SHALL BE ESCORTED TO APPROVED LOCATIONS OUTSIDE THE MOVEMENT AREA AND RELATED SAFETY AREAS. NO CONTRACT TIME EXTENSION WILL BE GRANTED FOR TIME LOST DUE TO THE ABSENCE OF ESCORT(S). WORK SHALL RESUME ONLY WITH THE RETURN OF THE APPROVED ESCORT(S).
3. THE ESCORT SHALL ASSURE THAT ALL EQUIPMENT MAINTAINS PROPER CLEARANCES FROM MOVING AIRCRAFT.

**TRAINING REQUIREMENTS FOR VEHICLE DRIVERS**

- 1. CONTRACTOR EMPLOYEES DESIGNATED AS ESCORTS, ARE REQUIRED TO BE BADGED BY AUGUSTA REGIONAL AIRPORT OPERATIONS / SECURITY, GO THROUGH SECURITY/DRIVER'S SAFETY TRAINING PROGRAM, AND PASS THE ASSOCIATED TEST. TRAINING IS BY APPOINTMENT ONLY. FURTHER ADDITIONAL AIRFIELD AND SITE TRAINING WILL BE PROVIDED WITH BADGED INDIVIDUALS UPON SUCCESSFUL COMPLETION OF CLASSROOM TRAINING AND BEFORE THE START OF CONSTRUCTION. ALL COSTS ASSOCIATED WITH BADGING SHALL BE AT THE CONTRACTOR'S EXPENSE.
2. MOVEMENT AREA ESCORT EMPLOYEES ARE REQUIRED TO COMPLETE AND PASS AN ADDITIONAL CLASS ON GROUND VEHICLE OPERATIONS.

• OPERATIONS MANAGER : MAIN LINE: (706) 798-3236 DIRECT: (706) 796-4004

**TWO-WAY RADIO COMMUNICATIONS PROCEDURES**

- 1. THE ONSITE PROJECT SUPERINTENDENT AND ANY FLAGMEN ASSIGNED TO THE PROJECT WILL BE REQUIRED TO MONITOR AIRPORT TWO-WAY RADIO COMMUNICATIONS BETWEEN THE AIR TRAFFIC CONTROL TOWER AND PILOTS. THE CONTRACTOR WILL NOT CONTACT THE TOWER OR PILOTS, BUT USE RADIO MONITORING TO STAY INFORMED ABOUT ONGOING AIRPORT OPERATIONS AND AIRCRAFT MOVEMENTS. AUTHORIZED MOVEMENT AREA ROUTES WILL BE DETERMINED BY THE CA TEAM AND AIRPORT OPERATIONS. THE CONTRACTOR SHALL NOT ENTER OR CROSS ANY OPEN RUNWAY OR TAXIWAY WITHOUT AN AUTHORIZED ESCORT FROM THE CA TEAM OR AIRPORT OPERATIONS. NON-COMPLIANCE WILL RESULT IN REMOVAL OF THE VIOLATOR FROM THE JOB SITE AND THE VIOLATOR'S AIRPORT IDENTIFICATION BADGE

- WILL BE CONFISCATED. IN ADDITION, CONTRACTOR SHALL BE RESPONSIBLE FOR PAYING ALL FINES ASSOCIATED WITH THE VIOLATION. THE FAA FINE IS TYPICALLY \$11,000 MINIMUM PER OCCURRENCE. EMERGENCIES AND OPERATING CONDITIONS MAY NECESSITATE SUDDEN CHANGES, BOTH IN AIRPORT OPERATIONS AND IN THE OPERATIONS OF THE CONTRACTOR. AIRCRAFT OPERATIONS SHALL ALWAYS HAVE PRIORITY OVER ANY AND ALL OF THE CONTRACTOR'S OPERATIONS. SHOULD RUNWAYS OR TAXIWAYS BE REQUIRED FOR THE USE OF AIRCRAFT AND SHOULD AIRPORT OPERATIONS, THE CONTROL TOWER, OR THE CA TEAM DEEM THE CONTRACTOR TO BE TOO CLOSE TO ACTIVE RUNWAYS OR TAXIWAYS THE CONTRACTOR SHALL SUSPEND HIS OPERATIONS, REMOVE HIS PERSONNEL, PLANT, EQUIPMENT, AND MATERIALS TO A SAFE DISTANCE AND STAND BY UNTIL THE RUNWAYS AND TAXIWAYS ARE NO LONGER REQUIRED FOR USE BY AIRCRAFT. THERE WILL BE NO COMPENSATION FOR DELAYS OR INEFFICIENCIES DUE TO THESE CHANGES.
2. FOR SCHEDULING CONTACT AIRFIELD OPERATIONS TRAINING AT (706) 798-4004.
3. CONTRACTOR SHALL PROVIDE RADIOS CAPABLE OF MONITORING AIRPORT FREQUENCY 121.60 MHZ.

**MAINTENANCE OF THE SECURED AREA OF THE AIRPORT**

- 1. SPECIAL ACCESS REQUIREMENTS AND OPERATING LIMITATIONS ARE REQUIRED INSIDE THE SECURITY FENCE. THE CONTRACTOR SHALL DELINEATE WORK LIMITS WITHIN THESE AREAS AS PER THE PHASING PLAN. CONFINEMENT, EQUIPMENT AND MATERIALS OUTSIDE OF THE RUNWAY OBJECT FREE AREA (ROFA) WHEN RUNWAY IS ACTIVE. CONFINEMENT, EQUIPMENT AND MATERIALS OUTSIDE OF THE TAXIWAY TOFA WHEN THE TAXIWAY IS ACTIVE. WORK SITE WILL GENERALLY BE ENCLOSED WITH CONSTRUCTION AREA MARKERS AS SHOWN ON THE SAFETY/PHASING PLAN. SEE THE SPECIFICATIONS FOR SPECIAL CONDITIONS AND FOR OTHER CONDITIONS RELATING TO SAFETY.
2. THE CONTRACTOR SHALL HAVE ACCESS TO THE AIRPORT ONLY AT THOSE LOCATIONS DESIGNATED ON THE PLANS. ALL OTHER ACCESS SHALL BE BY SPECIAL REQUEST AND SUBJECT TO APPROVAL BY AIRPORT OPERATIONS. THE CONTRACTOR WILL PROVIDE SECURITY PERSONNEL TO CONTROL MOVEMENTS THROUGH THE CONTRACTOR'S ACCESS GATE UNLESS THE GATE REMAINS LOCKED. THE CONTRACTOR SHALL REFER TO SPECIAL PROVISION SECTION SP-20 OF THE SPECIFICATIONS FOR THE REQUIREMENTS OF THE SECURITY PERSONNEL.

**WILDLIFE MANAGEMENT**

- 1. CONTRACTOR SHALL INSTRUCT EMPLOYEES NOT TO DISCARD FOOD OR OTHER TRASH ON OR AROUND WORK SITES THAT COULD ATTRACT WILDLIFE. CONTRACTOR EMPLOYEES SHALL NOT INTENTIONALLY FEED ANY WILDLIFE WHILE WORKING AT THE AIRPORT.
2. CONTRACTOR SHALL PROPERLY SEAL ALL TRASH CONTAINERS AT WORK SITES SUCH THAT WILDLIFE CANNOT GAIN ACCESS TO CONTAINERS DURING NON-CONSTRUCTION PERIODS.
3. CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS STAFF IF LARGE NUMBERS OF BIRDS ARE OBSERVED AT WORK SITES. CONTRACTOR SHALL IMMEDIATELY NOTIFY OPERATIONS STAFF IF DEER ARE SIGHTED WITHIN THE AIRFIELD FENCE.

**FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT**

- 1. THE CONTRACTOR SHALL HAVE AVAILABLE AT ALL TIMES A VACUUM TYPE MECHANICAL SWEEPER AND WATER TRUCK TO CLEAN ALL TAXIWAY AND APRON PAVEMENT OF DIRT, STONES, AND LOOSE DEBRIS WHERE CONSTRUCTION TRAFFIC CROSSES AT ALL ACTIVE AIRPORT PAVED SURFACES. PAYMENT FOR VACUUM AND WATER TRUCKS OR FOR PAVEMENT CLEANING SHALL BE INCIDENTAL TO PAY ITEM M-2 SAFETY AND SECURITY.
2. NO DEBRIS SHALL BE ALLOWED TO REMAIN ON THE ROADWAYS OR AIRPORT PAVED SURFACES. ACTIVE TAXIWAYS AND APRONS SHALL BE KEPT FREE OF DEBRIS AT ALL TIMES. USING POWER VACUUM SWEEPERS TO KEEP ALL ACCESS AND CONSTRUCTION AREAS CLEAR OF SOILS, CLODS, OR OTHER DEBRIS. PAYMENT FOR VACUUM SWEEPING AND CLEANING OF RUNWAY, TAXIWAYS AND/OR APRONS IS INCIDENTAL TO ITEM M-2 SAFETY AND SECURITY.
3. THE CONTRACTOR SHALL HAVE AVAILABLE ON-SITE AT ALL TIMES A METHOD OF PERIODIC SPRAYING OF ANY STOCKPILE, HAUL ROADS, OR EXPOSED AREAS TO LIMIT DUST.

**NOTIFICATION OF CONSTRUCTION ACTIVITIES**

- 1. PRIOR TO COMMENCEMENT OF CONSTRUCTION ACTIVITY, THE CONTRACTOR SHALL NOTIFY IN WRITING, AT LEAST 5 DAYS IN ADVANCE, AIRPORT STAFF AND THE CA TEAM OF ITS INTENTIONS TO BEGIN CONSTRUCTION, STATING THE PROPOSED TIME, DATE, AND AREA OF WHICH CONSTRUCTION IS TO OCCUR IN ORDER FOR THE APPROPRIATE NOTICE-TO-AIRMEN (NOTAM) TO BE ISSUED. DURING THE PERFORMANCE OF THIS CONTRACT, THE AIRPORT FACILITY SHALL REMAIN IN USE TO THE MAXIMUM EXTENT POSSIBLE. THE CONTRACTOR SHALL NOT ALLOW EMPLOYEES, SUBCONTRACTORS, SUPPLIERS, OR ANY OTHER UNAUTHORIZED PERSONS TO ENTER IN ANY AIRPORT AREA WHICH MAY BE OPEN FOR AIRCRAFT USE, EXCEPT AS NOTED ON THE CONSTRUCTION SAFETY PHASING PLAN.
2. CONTRACTOR SHALL INFORM THE CA TEAM ON A DAILY BASIS OF THE DAILY CONSTRUCTION ACTIVITIES.
3. NOTAMS
A. IN ORDER FOR THE CONTRACTOR TO OPERATE WITHIN AIRPORT PROPERTY, APPROPRIATE NOTICES TO AIRMEN (NOTAM) MUST BE ISSUED BY THE AIRPORT THROUGH THE FAA FLIGHT SERVICE STATION. THESE NOTICES PROVIDE INFORMATION ON CLOSED, LIMITED, OR HAZARDOUS CONDITIONS TO AIRMEN AND USERS OF THE AIRPORT. A 72 HOUR NOTICE IS REQUIRED FOR ISSUANCE OF THE PROPER NOTAM. ALL CONSTRUCTION OPERATIONS MUST BE CLOSELY COORDINATED WITH THE DESIGNER FOR NOTAM ISSUANCE.
4. EMERGENCY NOTIFICATION PROCEDURES
A. THE CONTRACTOR SHALL IMMEDIATELY CALL 911 IF AN ACCIDENT OCCURS WITH INJURIES ON AIRPORT PROPERTY ADVISING THE LOCATION IS ON AUGUSTA REGIONAL AIRPORT FOR THEM TO COORDINATE WITH THE AIRPORT AUTHORITY.
B. THE CONTRACTOR SHALL ALSO IMMEDIATELY NOTIFY AIRPORT OPERATIONS TO COORDINATE ALL EMERGENCY EFFORTS, (706) 799-5372.
C. WITHIN 24 HOURS, THE CONTRACTOR SHALL PROVIDE A WRITTEN REPORT OF ALL ACCIDENTS TO AIRPORT OPERATIONS AND CA TEAM.
5. COORDINATION WITH ARFF
A. IF A FIRE OCCURS ON AIRPORT PROPERTY THE CONTRACTOR SHALL NOT ATTEMPT TO FIGHT THE FIRE BEYOND WHAT MAY BE DOUSED BY USE OF A FIRE EXTINGUISHER. THE CONTRACTOR SHALL IMMEDIATELY CALL 911 ADVISING THE LOCATION IS ON AUGUSTA REGIONAL AIRPORT FOR THEM TO COORDINATE WITH THE AIRPORT AUTHORITY.
B. NON-EMERGENCY COMMUNICATION WITH AIRPORT ARFF WILL BE COORDINATED BY THE CA TEAM.
C. AN AIRPORT ARFF REPRESENTATIVE WILL BE INVITED TO ATTEND THE PRECONSTRUCTION CONFERENCE AT WHICH TIME THE OVERALL CONSTRUCTION SCHEDULE WILL BE PRESENTED.
D. A MEETING WILL BE SCHEDULED WITH THE AIRPORT ARFF REPRESENTATIVE PRIOR TO THE START OF EACH MAJOR CONSTRUCTION PHASE WHICH SIGNIFICANTLY IMPACTS/MODIFIES AIRFIELD CLOSURES THROUGHOUT THE DURATION OF THE CONSTRUCTION PROJECT. PARTICIPANTS IN THESE MEETINGS SHALL INCLUDE: AIRPORT STAFF, AIRPORT OPERATIONS, DESIGN TEAM, CONSTRUCTION ADMINISTRATION TEAM, CONTRACTOR, AND SUBCONTRACTORS.
6. NOTIFICATION TO THE FAA
A. THE CONTRACTOR'S USE OF CRANES, BOOM TRUCKS, CONCRETE PUMP TRUCKS, DRILL RIGS AND OTHER TALL OBJECTS WILL REQUIRE SUBMITTAL AND APPROVAL BY THE AIRPORT AND DESIGNER. IF ON AIRPORT PROPERTY, THE EQUIPMENT SHALL REQUIRE FAA AIRSPACE REVIEW AS SUBMITTAL ON FAA FORM 7460-1 NOTICE OF CONSTRUCTION.

- IF CONTRACTOR REQUIRES EQUIPMENT IN EXCESS OF MAXIMUM ALLOWABLE HEIGHT, THEN SUBMIT A 7460-1 45 DAYS IN ADVANCE OF CRANE ERECTION. ALL CONSTRUCTION INVOLVING CRANES SHALL FURTHER BE COORDINATED AT LEAST 5 DAYS IN ADVANCE, EXCLUDING WEEKENDS, WITH THE AIRPORT OPERATIONS. THIS DOES NOT INCLUDE THE TIME REQUIRED FOR AIRSPACE REVIEW. THE FOLLOWING INFORMATION AND ACTIONS ARE REQUIRED LOCATION OF THE CRANE.
• MAXIMUM EXTENDABLE HEIGHT.
• THE TOP OF EACH CRANE BOOM SHALL BE MARKED BY A 3' X 3' ORANGE AND WHITE CHECKERED FLAG -- EACH BOX BEING 1' SQUARE.
• EACH CRANE SHALL BE LOWERED AT NIGHT AND DURING PERIODS OF POOR VISIBILITY AS DIRECTED BY AIRPORT OPERATION. IN THE EVENT THE CRANE IS APPROVED TO REMAIN EXTENDED DURING THE HOURS FROM SUNSET TO SUNRISE, THE HIGHEST POINT OF THE CRANE BOOM WILL BE LIT WITH A RED OBSTRUCTION LIGHT IN ACCORDANCE WITH AC 707460-1.
• SET CRANE LIMITERS DURING ALL CONSTRUCTION.



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DATE: DECEMBER 6, 2022
DESIGNED BY: EJS
DRAWN BY: NJH
CHECKED BY: DES

DO NOT SCALE DRAWINGS

SHEET CONTENTS
PHASING NOTES

SHEET NO.

G-071

## INSPECTION REQUIREMENTS

- CONSTRUCTION EQUIPMENT: THE CONTRACTOR SHALL INSPECT ALL CONSTRUCTION EQUIPMENT ON A DAILY BASIS TO ENSURE THAT THE EQUIPMENT IS IN GOOD WORKING ORDER AND THAT ORANGE AND WHITE CONSTRUCTION FLAGS AND BEACONS ARE PRESENT, CLEAN, AND IN GOOD CONDITION.
- CONSTRUCTION BARRICADES: THE CONTRACTOR SHALL INSPECT ALL CONSTRUCTION BARRICADES ON A DAILY BASIS TO ENSURE THAT BARRICADES ARE IN GOOD CONDITION AND THAT FLASHING BEACONS ARE IN WORKING ORDER. IF BARRICADES ARE DAMAGED THEY SHALL BE REMOVED FROM THE CONSTRUCTION SITE AND REPLACED IMMEDIATELY. ANY INOPERABLE FLASHING LIGHTS SHALL BE REMOVED AND REPLACED AT THE END OF EVERY DAY.
- CONSTRUCTION EQUIPMENT FUELING AREA: THE CONTRACTOR SHALL INSPECT THE CONSTRUCTION EQUIPMENT FUELING AREA DAILY. ANY FUEL SPILLS WILL BE REPORTED TO AIRPORT OPERATIONS AS SOON AS SPILL HAS BEEN IDENTIFIED. IF TEMPORARY FUEL TANKS ARE SUPPLIED BY THE CONTRACTOR THEY MUST BE SURROUNDED BY CONCRETE JERSEY BARRIERS. ALSO, TANKS MUST BE MARKED FLAMMABLE ON ALL SIDES AND LABELED WITH THE TYPE OF FUEL THEY CONTAIN. THE CONTRACTOR IS RESPONSIBLE FOR PREPARING AN SPCC FOR THE FUEL TANKS, IF REQUIRED, IN ACCORDANCE WITH FEDERAL REGULATIONS.
- ACTIVE AIRPORT PAVEMENTS: THE CONTRACTOR SHALL INSPECT ALL ACTIVE AIRPORT PAVEMENTS CONTINUOUSLY DURING CONSTRUCTION ACTIVITIES. MATERIALS TRACKED ONTO ACTIVE AIRPORT PAVEMENTS MUST BE CONTINUOUSLY REMOVED DURING THE PROJECT. PRIOR TO LEAVING THE CONSTRUCTION SITE AT THE END OF EACH DAY, THE CONTRACTOR MUST CONTACT AIRPORT OPERATIONS FOR AN INSPECTION OF THE CLEANLINESS OF AIRPORT PAVEMENTS.
- A FINAL SAFETY INSPECTION MAY BE REQUIRED PRIOR TO ALLOWING AIR CARRIER SERVICE. COORDINATION WITH THE FAA AIRPORT CERTIFICATION SAFETY INSPECTOR WILL DETERMINE IF A FINAL INSPECTION WILL BE NECESSARY.

## UNDERGROUND UTILITIES

- THE CONTRACTOR SHALL IDENTIFY ANY KNOWN UNDERGROUND INTERFERENCES OR DISCREPANCIES ON ALL AVAILABLE DRAWINGS THAT CAN BE PROVIDED BY CONTACTING THE DESIGNER AND THE CA TEAM AT:  
  
MEAD & HUNT, INC.  
5955 CORE ROAD, SUITE 515  
NORTH CHARLESTON, SC 29046  
PHONE: (843) 520-2986
- PRIOR TO COMMENCING ANY EXCAVATION (ON OR OFF AOA), DRILLING (ON OR OFF THE AOA), DRIVING FENCE POSTS (ALONG THE AOA), TRENCHING (ON OR OFF THE AOA), SAW CUTTING (AOA ONLY), THE CONTRACTOR SHALL PERFORM GPR ACROSS THE ENTIRE SITE TO BE DISTURBED AND SHALL REVIEW DRAWINGS WITH AIRPORT TO INSURE THAT ALL UNDERGROUND OBSTRUCTIONS AND UTILITIES ARE IDENTIFIED. IN ADDITION THE CONTRACTOR SHALL CONTACT GEORGIA 811 AND FAA AND COORDINATE WITH THE AIRPORT PROJECT SUPERVISOR TO ASSIGN THE VERIFICATION OF UTILITIES BY AIRPORT MAINTENANCE. GEORGIA 811 FAA, MAINTENANCE, AND THE CONTRACTOR SHALL ATTEMPT TO LOCATE UTILITIES. THE CONTRACTOR WILL BE COMPLETELY RESPONSIBLE FOR ALL DAMAGE TO UNDERGROUND UTILITIES. THE CONTRACTOR SHALL COORDINATE REQUEST FOR SWEEPS OF UTILITIES BY COMPLETING THE CONTRACTOR REQUEST FOR SWEEP FORM AT LEAST 72 HOURS PRIOR TO ANY EXCAVATIONS. AIRPORT AUTHORITY WILL NOTIFY THE CONTRACTOR A MINIMUM WITH 24 HOURS AFTER RECEIVING NOTICE. AT THAT TIME THE AIRPORT AUTHORITY WILL INDICATE IF IT CAN COMPLETE THE SWEEP. IF IT CANT, THE CONTRACTOR WILL BE RESPONSIBLE TO COMPLETE THE SWEEP AND/OR USE GEORGIA 811.
- EACH UTILITY SHALL BE SWEEP IN THE FOLLOWING MANNER: FLAGS CAN BE USED BUT SHALL BE COLOR COORDINATED AS SUGGESTED BELOW. IN ADDITION THE "ACRONYM" FOR THAT UTILITY SHALL BE WRITTEN ON ONE SIDE OF THE FLAG WITH A PERMANENT MARKER.
- STAKES CAN BE USED. THE TOP TWO INCHES OF THE STAKE SHALL BE PAINTED IN COLOR AS SUGGESTED BELOW. IN ADDITION THE "ACRONYM" FOR THAT UTILITY SHALL BE WRITTEN ON ONE SIDE OF THE STAKE WITH A PERMANENT MARKER. STAKES SHOULD NOT BE USED IN RSA OR TSA IF THEY CAN BE AVOIDED.
- PAINTING IS ONLY AUTHORIZED ON ASPHALT, CONCRETE, AND METAL SURFACES. MARKINGS SHALL BE COLOR COORDINATED AS SUGGESTED BELOW. THE ACRONYM FOR THE UTILITY SHALL BE USED FOR EACH UTILITY. A LINE THAT SHOWS THE DIRECTION OF THE UTILITY SHALL EMANATE FROM THE ACRONYM IN EACH DIRECTION.
- ALL MARKING OF UTILITIES SHALL BE EVERY 50 FEET.

	ACRONYM	COLOR
ELECTRICAL LOOPS (NON AOA)	USE "ELEC"	RED
AIRFIELD ELECTRICAL	USE "ELEC"	RED
NATURAL GAS	USE "NAT GAS"	YELLOW
SANITARY	USE "SANIT"	BROWN
STORM	USE "STORM"	BROWN
WATER (POTABLE AND FIRE)	USE "WATER"	BLUE
FAA COPPER	USE "FAA COP"	RED
FAA FIBER	USE "FAA FIB"	ORANGE
FIBER	USE "FIBER"	ORANGE
TELEPHONE	USE "TELE"	ORANGE

- IF UNDERGROUND UTILITY IS ABANDONED, CONTRACTOR SHALL STILL STAKE, MARK, OR FLAG BUT WRITE DOWN "ABAND" BEFORE THE ABBREVIATED PREFIX INDICATED ABOVE.
- THE INDIVIDUAL MARKING, STAKING, OR FLAGGING SHALL MARK THE UTILITIES IN A WAY THAT COINCIDES WITH THE DRAWINGS THAT ARE REFERENCED ON THE REQUEST FOR SWEEP FORM.
- IF A UTILITY OR ANY UNDERGROUND OBSTRUCTION IS FOUND IT SHALL BE REPORTED IMMEDIATELY TO THE DESIGNER OR THE AIRPORT PROJECT SUPERVISOR.
- CONTRACTOR EMPLOYEES IN AN EXCAVATION SHALL BE PROTECTED FROM CAVE-INS BY AN ADEQUATE PROTECTIVE SYSTEM UNLESS THE EXCAVATION IS:  
A. MADE ENTIRELY OF STABLE ROCK, OR  
B. LESS THAN 5 FEET DEEP AND DETERMINATION HAS BEEN MADE THAT THERE IS NO POTENTIAL FOR A CAVE-IN.
- EXCAVATION SHALL BE PROTECTED USING PROPER BARRICADING MATERIALS WHICH SHALL BE INSTALLED A MINIMUM OF 6 FEET BACK FROM EXCAVATION (UNLESS IN CONFLICT WITH AIRFIELD REQUIREMENTS). BARRICADE MATERIAL CAN BE WOOD, STEEL CABLES, OR CHAIN SUPPORTED AT INTERVALS SO THAT THE BARRICADE DOES NOT SAG OR DROP BELOW THE REQUIRED HEIGHT. CAUTION TAPE IS NOT AN APPROVED BARRICADE MATERIAL. GUARDRAIL/JERSEY BARRIERS MAY BE REQUIRED AND SHALL PROVIDE A TOP RAIL, MID RAIL, AND TOE BOARD AT PROPER ELEVATIONS AND BE ABLE TO WITHSTAND A MINIMUM 200 POUND FORCE WITHOUT COLLAPSING. THE CONTRACTOR IS RESPONSIBLE FOR PREPARING ANY TRENCHING PLANS IN ACCORDANCE WITH OSHA REGULATIONS.
- THE CONTRACTOR IS RESPONSIBLE FOR DOCUMENTING UTILITY INFORMATION FOR USE DURING CONSTRUCTION AND PREPARATION OF AS-BUILTS.

## PENALTIES

- ENTERING THE MOVEMENT AREA (I.E. RUNWAYS, TAXIWAYS, ETC.) WITHOUT AUTHORIZATION FROM THE FAA AIR TRAFFIC CONTROL TOWER AND THE AIRPORT OPERATIONS WILL RESULT IN THE SUSPENSION OF AN ASSIGNED ID BADGE AND/OR RAMP DRIVING PRIVILEGES AND COULD SUBJECT THE CONTRACTOR'S KEY PERSONNEL TO PERMANENT REVOCATION OF THEIR AIRFIELD DRIVING PRIVILEGES. FURTHERMORE, RUNWAY INCURSIONS MAY RESULT IN FINES AND/OR TERMINATION OF THIS CONTRACT. CONTRACTOR IS SOLELY RESPONSIBLE FOR THESE FINES FOR THEIR FORGES AS WELL AS ANY SUBCONTRACTORS, SUPPLIERS, OR ANY OTHERS EMPLOYED BY THE CONTRACTOR ON THIS PROJECT.

## RUNWAY AND TAXIWAY VISUAL AIDS

- TAXIWAY VISUAL AIDS, INCLUDING ANY TEMPORARY TAXIWAY PAVEMENT MARKING WILL BE AS SHOWN ON THE PLANS AND PROVIDED BY THE CONTRACTOR. MARKINGS SHALL BE IN COMPLIANCE WITH AC 150/5340-1, STANDARDS FOR AIRPORT MARKINGS. LIGHTING SHALL CONFORM TO AC 150/5340-30, DESIGN AND INSTALLATION DETAILS FOR AIRPORT VISUAL AIDS, AC 150/5345-50, SPECIFICATIONS FOR PORTABLE RUNWAY TAXIWAY LIGHTS, AND AC 150/5345-53 AIRPORT LIGHTING

CERTIFICATION PROGRAM. SIGNS SHALL CONFORM TO AC 150/5345-44, SPECIFICATION FOR RUNWAY AND TAXIWAY SIGNS, AC 150/5340-18, STANDARDS FOR AIRPORT SIGN SYSTEMS, AND AC 150/5345-53, AIRPORT LIGHTING CERTIFICATION PROGRAM.

- IF AIRCRAFT OPERATION AREAS MUST BE CLOSED, THE CONTRACTOR SHALL FURNISH AND PLACE PORTABLE BARRICADES ACROSS TAXIWAYS TO KEEP VEHICLES FROM ENTERING ACTIVE OPERATION AREAS AND TO KEEP AIRCRAFT FROM TAXIING INTO CONSTRUCTION AREAS. EXCAVATION AND OPEN TRENCHES MAY BE PERMITTED UP TO THE EDGE OF AN APRON, PROVIDED THE DROP OFF IS APPROPRIATELY MARKED AND LIGHTED. BARRICADES SHALL BE MARKED WITH DIAGONAL, ALTERNATING ORANGE AND WHITE STRIPES AND SUPPLEMENTED WITH EITHER FLASHING LIGHTS DURING HOURS OF RESTRICTED VISIBILITY OR DARKNESS. LIGHTS SHALL BE BARRICADE TYPE TYPICAL FOR CONSTRUCTION ZONES, AND RED IN COLOR. ALL LIGHTS MUST BE CHECKED NIGHTLY TO ENSURE THAT THEY ARE OPERATING. ANY LIGHTS NOT FUNCTIONING SHALL BE IMMEDIATELY REPLACED.
- BARRICADES LOCATED WITHIN AIRCRAFT OPERATION AREAS SHALL BE LOW LEVEL AVIATION BARRICADES SPECIFICALLY MANUFACTURED AND DESIGNED FOR SUCH PURPOSE. THEY SHALL BE ALTERNATING ORANGE AND WHITE IN COLOR 10" HIGH AND 86" LONG, MADE OF UV-RESISTANT POLYETHYLENE AS MANUFACTURED BY MULTI-BARRIER (MODEL AR 10X96 HDPE) OR APPROVED EQUAL.

## MARKING AND SIGNS FOR ACCESS ROUTES

- MARKINGS AND SIGNS USED ON ACCESS ROUTES SHALL CONFORM TO AC 150/5340-18 AND, TO THE EXTENT PRACTICABLE, WITH THE MOST CURRENT VERSION OF THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).

## HAZARD MARKING AND LIGHTING

### HAZARD MARKING

- HAZARD-MARKING BARRICADES, TRAFFIC CONES, FLASHERS, ETC. SHOULD BE USED: TO IDENTIFY AND DEFINE THE LIMITS OF CONSTRUCTION MAKING THEM VISIBLE TO AIRCRAFT, PERSONNEL, OR VEHICLES; TO IDENTIFY HAZARDS SUCH AS OPEN MANHOLES, SMALL AREAS UNDER REPAIR, STOCKPILED MATERIAL, WASTE AREAS, ETC.; TO PREVENT AIRCRAFT FROM TAXIING ONTO A CLOSED TAXIWAY; AND TO IDENTIFY FAA, AIRPORT, AND NATIONAL WEATHER SERVICE FACILITIES, CABLES, POWER LINES, INSTRUMENT LANDING SYSTEM (ILS) CRITICAL AREAS, AND OTHER SENSITIVE AREAS TO PREVENT DAMAGE, INTERFERENCE, AND FACILITY SHUTDOWNS. HAZARDOUS AREAS, IN WHICH NO PART OF AN AIRCRAFT MAY ENTER, SHOULD BE INDICATED BY THE USE OF BARRICADES MARKED WITH DIAGONAL, ALTERNATING ORANGE AND WHITE STRIPES. THE BARRICADES SHOULD BE SUPPLEMENTED WITH ALTERNATING ORANGE AND WHITE FLAGS, AND INSTALLED SO THAT THEY ARE ALWAYS IN THE EXTENDED POSITION AND PROPERLY ORIENTED. DURING REDUCED VISIBILITY OR NIGHT HOURS, THE BARRICADES SHOULD BE SUPPLEMENTED WITH FLASHING RED LIGHTS. THE INTENSITY OF THE LIGHTS AND SPACING FOR BARRICADES, FLAGS, AND LIGHTS SHOULD BE ADEQUATE TO DELINEATE THE HAZARDOUS AREA WITHOUT AMBIGUITY. THE CONTRACTOR SHALL HAVE A DESIGNATED PERSON ON CALL 24-HOURS A DAY FOR EMERGENCY MAINTENANCE OF AIRPORT HAZARD LIGHTING AND BARRICADES.

### MARKING AND LIGHTING

- LOW PROFILE LIGHTS, RETROREFLECTIVE TAXIWAY EDGE MARKERS, AND LOW LEVEL BARRICADES SHALL BE PROVIDED AND ERRECTED BY THE CONTRACTOR AS SHOWN ON THE PLANS OR AS DIRECTED BY THE CA TEAM. ALL CONSTRUCTION AREAS, INCLUDING CLOSED TAXIWAYS, SHOULD BE CLEARLY AND VISIBLY SEPARATED FROM ACTIVE AIR OPERATION AREAS. HAZARD AREAS, FACILITIES, CABLES, AND POWER LINES SHOULD ALSO BE CLEARLY IDENTIFIED BY THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING THE CONDITION AND VISIBILITY OF ALL MARKERS IDENTIFYING ABOVE-MENTIONED AREAS AND THAT MARKING AND LIGHTING AIDS REMAIN IN PLACE. ALTERNATING ORANGE AND WHITE FLAGLINES, TRAFFIC CONES, OMNIDIRECTIONAL YELLOW FLASHERS, AND/OR SIGNS SHOULD BE USED AS NECESSARY TO CLEARLY SEPARATE ALL CONSTRUCTION/MAINTENANCE AREAS FROM OTHER PARTS OF THE AOA. ALL BARRICADES, TEMPORARY MARKERS, FLAGLINES SUPPORTS, AND OTHER OBJECTS PLACED AND LEFT IN SAFETY AREAS ON ANY OPEN TAXIWAY, OR TAXILANE SHOULD BE AS LOW AS POSSIBLE TO THE GROUND; OF LOW MASS; EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF ITS COMPONENTS; WEIGHTED DOWN OR STURDILY ATTACHED TO THE SURFACE TO PREVENT DISPLACEMENT FROM PROPWASH, JET BLAST, WING VORTEX, OR OTHER SURFACE WIND CURRENTS; AND IF AFFIXED TO THE SURFACE, FRANGIBLE AT GROUND LEVEL.

## EQUIPMENT

### LOW PROFILE BARRICADES

- THE CONTRACTOR SHALL PROVIDE LOW PROFILE BARRICADES AS DESCRIBED IN THE PLANS ALONG RUNWAY OR TAXIWAY EDGES WHEREVER OPEN EXCAVATIONS OR IRREGULAR GRADES ARE LEFT WITHIN THE SAFETY AREA OF AN ACTIVE RUNWAY OR TAXIWAY OR WHERE TEMPORARY PAVEMENT CLOSURES OR AIRCRAFT LIMITATIONS ARE REQUIRED. BARRICADES ALONG ACTIVE APRON OR TAXIWAY PAVEMENT SHALL BE PLACED APPROXIMATELY 10 FEET FROM THE EDGE OF THE FULL STRENGTH PAVEMENT. WHERE POSSIBLE, OR AS SHOWN ON THE OPERATIONAL AND PHASING PLANS OR AS DETERMINED BY THE DESIGNER AND AIRPORT OPERATIONS TO DELINEATE THE CONTRACTORS WORK AREAS, GAP BETWEEN BARRICADES SHALL BE NO MORE THAN 5 FEET END TO END. NO GAPS ARE ALLOWED BETWEEN BARRICADES LOCATED ADJACENT TO RUNWAY SAFETY AREAS.
- THE CONTRACTOR SHALL MAINTAIN THE LIGHTS AND BARRICADES IN AN OPERABLE CONDITION FOR THE DURATION OF THE PROJECT.
- ALL BARRICADES SHALL BE CHECKED VISUALLY FOR SIGNS OF WEAR AND TEAR ON A WEEKLY BASIS AND SHALL BE REPAINTED AND/OR REPLACED WHEN DEEMED APPROPRIATE BY THE CA TEAM. THE CONDITION OF LIGHTING UNITS SHALL BE CHECKED DAILY. ALL LIGHT FIXTURES SHALL BE VERIFIED OPERATING BY THE CONTRACTOR ON A DAILY BASIS BEFORE THE CONTRACTOR CEASES OPERATION FOR THE DAY. THE AREAS AROUND ALL BARRICADES SHALL BE CLEANED AT LEAST ONCE EACH WEEK AND THE CONTRACTOR SHALL SWEEP UP ACCUMULATED DEBRIS AND REMOVE IT FROM THE SITE. ALL ACTIVITIES CONDUCTED ADJACENT TO ACTIVE RUNWAYS OR TAXIWAYS SHALL BE COORDINATED WITH THE CA TEAM.
- BARRICADES SHALL BE AS SHOWN IN DETAILS ON G-082. ALL INCIDENTAL CONNECTORS, SPACERS, SPLICE PLATES, ETC., SHALL BE PAINTED WHITE.
- ALTERNATE FORMS OF BARRICADES MAY BE PROPOSED BY THE CONTRACTOR WHICH MEET THESE FUNCTIONAL REQUIREMENTS. APPROVALS OF ANY SUCH SUBSTITUTION (IF GRANTED) SHALL BE BY THE AIRPORT OPERATIONS AND THE CA TEAM.
- THE FINAL LOCATION FOR THE BARRICADES SHALL BE ESTABLISHED IN THE FIELD WITH CONCURRENCE FROM THE CA TEAM AND AIRPORT OPERATIONS.
- THE CONTRACTOR SHALL HAVE REPLACEMENT BARRICADES, LIGHTS AND BATTERIES ON SITE AND SHALL REPLACE BARRICADES, LIGHTS AND/OR BATTERIES WITHIN ONE HOUR OF NOTIFICATION BY THE CA TEAM OR AIRPORT PERSONNEL. CONTRACTOR SHALL PROVIDE THE NAME AND TELEPHONE NUMBER FOR AN ON-CALL REPRESENTATIVE 24 HOURS PER DAY, SEVEN DAYS PER WEEK TO REPLACE BARRICADES, BATTERIES AND INOPERATIVE LIGHTS.
- RED STEADY BURN LIGHTS SHALL BE PLACED AT THE ENDS AND AT CORNERS OF EACH LINE OF BARRICADES; ALL OTHER LIGHTS ON BARRICADES SHALL BE RED FLASHING.
- CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING PROPER POSITIONING OF ALL BARRICADES.
- SANDBAGS, WATER AND/OR ANCHORS MAY BE REQUIRED TO HOLD THE BARRICADES IN PLACE WHERE EXPOSED TO JET BLAST.
- ALL COSTS ASSOCIATED WITH FURNISHING, PLACEMENT, MAINTENANCE AND SUBSEQUENT RELOCATION OF THE LOW PROFILE BARRICADES ARE INCIDENTAL TO ITEM M-2, SAFETY AND SECURITY.

### DELINEATING WORK AREAS

- SAFETY BARRICADES SHALL BE FURNISHED AND INSTALLED AT THE LOCATIONS AS INDICATED ON THE CONTRACT DOCUMENTS AND/OR DIRECTED BY THE CA TEAM. GENERALLY, SAFETY FENCE WILL BE USED FOR DELINEATING CONTRACTOR STAGING/STORAGE AREAS, PHASE LIMITS AND EXCAVATIONS, TRENCHES, DROPOFFS, ETC. THAT MAY POSE A HAZARD TO ONSITE SAFETY. IN NO INSTANCE SHALL THE USE OF SAFETY FENCE BE ALLOWED WITHIN AN ACTIVE TAXIWAY OBJECT FREE AREA OR WITHIN AN ACTIVE RUNWAYS SAFETY AREA.
- SAFETY BARRICADES SHALL BE AS SPECIFIED IN SECTION 70-08 OF THE GENERAL PROVISIONS.

### TAXIWAY ENDING MARKER

- TAXIWAY ENDING MARKER SHALL BE FURNISHED AND INSTALLED AT THE LOCATIONS AS INDICATED ON THE CONTRACT DOCUMENTS AND/OR DIRECTED BY THE CA TEAM. TAXIWAY ENDING MARKER SHALL MEET THE REQUIREMENTS OF FAA ADVISORY CIRCULAR 150/5345-44J, TYPE I-858C, OR CURRENT EDITION AS OF BID DATE.
- TAXIWAY ENDING MARKER SHALL BE PLACED AT THE ENTRANCE OF EACH TAXIWAY THAT IS CLOSED TO AIRCRAFT OPERATIONS.

## PROTECTION

- AT NO TIME SHALL PERSONNEL, VEHICLES OR EQUIPMENT BE LOCATED OR ENTER ANY OF THE FOLLOWING AREAS UNLESS AUTHORIZED BY AIRPORT OPERATION OR CA TEAM.  
A. WITHIN 250 FEET PARALLEL TO AN ACTIVE RUNWAY CENTERLINE (TO BE INDICATED ON THE CSPP AND/OR SPCD).  
B. NO STOCKPILES WILL BE PLACED WITHIN 400 FEET PARALLEL TO AN ACTIVE RUNWAY CENTERLINE NOR WILL EQUIPMENT BE LEFT UNATTENDED.  
C. WITHIN 1,000 FEET OF THE END OF ACTIVE RUNWAYS (EACH END TO BE INDICATED IN THE CSPP AND/OR SPCD).  
D. WITHIN 93 FEET PARALLEL TO AN ACTIVE TAXIWAY CENTERLINE OPERATING WITH AIRCRAFT WITH OUT PROPER APPROVAL.  
E. ACTIVE NAVAID CRITICAL AREAS.  
F. ON THE MOVEMENT AREA AND/OR ASSOCIATED SAFETY AREAS DURING TIMES OF INCLEMENT WEATHER OR UNUSUAL EVENTS AS DETERMINED BY THE AIRPORT OPERATION. DURING SUCH TIMES ALL WORK IS TO BE SUSPENDED. ALL EQUIPMENT SHALL BE REMOVED TO APPROVED STAGING AREAS.
- TRENCHES AND/OR EXCAVATIONS SHALL NOT BE ALLOWED IN THE FOLLOWING AREAS WITHOUT CLOSURE OR RESTRICTION OF THE ADJACENT MOVEMENT AREA:  
A. WITHIN 250 FEET PARALLEL TO A RUNWAY CENTERLINE.  
B. WITHIN 93 FEET PARALLEL TO A TAXIWAY CENTERLINE OPERATING WITH AIRCRAFT  
C. WITHIN 1,000 FEET OF THE END OF A RUNWAY.  
D. ACTIVE NAVAID CRITICAL AREAS.  
E. EQUIPMENT WITHIN 400 FEET OF AN ACTIVE RUNWAY SHALL BE REMOVED WHEN NOT IN USE.
- SOIL EROSION MUST BE CONTROLLED TO MAINTAIN RSA/TSA STANDARDS. ANY HOLES OR MOUNDS, BUMPS, OR OTHER FEATURES WITH A GRADE CHANGE GREATER THAN 3 INCHES WITHIN THE RSA OR TSA SHALL BE REPAIRED PRIOR TO RUNWAY OR TAXIWAY OPENING.

## OTHER LIMITATIONS ON CONSTRUCTION

- PROHIBITIONS  
A. OPEN FLAME WELDING OR TORCH CUTTING OPERATIONS ARE PROHIBITED UNLESS ADEQUATE FIRE AND SAFETY PRECAUTIONS ARE PROVIDED AND HAVE BEEN APPROVED FOR USE BY THE CA TEAM AND A BURN PERMIT HAS BEEN OBTAINED FROM THE ARFF. FLARE POTS SHALL NOT BE USED NEAR AIRCRAFT TURNING AREAS. ELECTRICAL BLASTING CAPS SHALL NOT BE USED WITHIN 1,000 FT OF THE AIRPORT PROPERTY.

## PROJECT SURVEY AND LAYOUT

- THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR PROVIDING THEIR OWN PROJECT SURVEY AND CONSTRUCTION LAYOUT IN ACCORDANCE WITH SPECIFICATION SP-90. CONTRACTOR SHALL BE COMPENSATED FOR PROJECT SURVEY AND LAYOUT UNDER ITEM SP-90-3.1.

## CONTRACTOR RESPONSIBILITIES

- THE CONTRACTOR SHALL SUBMIT A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) TO THE AIRPORT OPERATOR DESCRIBING HOW IT WILL COMPLY WITH THE REQUIREMENTS OF THE CSPP. THE SPCD MUST INCLUDE A CERTIFICATION STATEMENT BY THE CONTRACTOR THAT INDICATES IT UNDERSTANDS THE OPERATIONAL SAFETY REQUIREMENTS OF THE CSPP AND THEY WILL NOT DEVIATE FROM THE APPROVED CSPP AND SPCD UNLESS WRITTEN APPROVAL IS GRANTED BY THE AIRPORT. ANY CONSTRUCTION PRACTICE PROPOSED BY THE CONTRACTOR THAT DOES NOT CONFORM TO THE CSPP AND SPCD MAY IMPACT THE AIRPORT'S OPERATIONAL SAFETY AND WILL REQUIRE A REVISION TO THE CSPP AND SPCD AND RE-COORDINATION WITH THE AIRPORT OPERATOR AND THE FAA IN ADVANCE.
- THE CONTRACTOR SHALL HAVE AVAILABLE AT ALL TIMES COPIES OF THE CSPP AND SPCD FOR REFERENCE BY THE AIRPORT OPERATOR AND ITS REPRESENTATIVES, AND BY SUBCONTRACTORS AND CONTRACTOR EMPLOYEES.
- THE CONTRACTOR SHALL ENSURE THAT CONSTRUCTION PERSONNEL ARE FAMILIAR WITH SAFETY PROCEDURES AND REGULATIONS ON THE AIRPORT. PROVIDE A POINT OF CONTACT WHO WILL COORDINATE AN IMMEDIATE RESPONSE TO CORRECT ANY CONSTRUCTION-RELATED ACTIVITY THAT MAY ADVERSELY AFFECT THE OPERATIONAL SAFETY OF THE AIRPORT.
- THE CONTRACTOR SHALL IDENTIFY IN THE SPCD THE CONTRACTOR'S ON-SITE EMPLOYEES RESPONSIBLE FOR MONITORING COMPLIANCE WITH THE CSPP AND SPCD DURING CONSTRUCTION.
- THE CONTRACTOR SHALL CONDUCT INSPECTIONS TO ENSURE CONSTRUCTION PERSONNEL COMPLY WITH THE CSPP AND SPCD AND THAT THERE ARE NO ALTERED CONSTRUCTION ACTIVITIES THAT COULD CREATE POTENTIAL SAFETY HAZARDS.
- THE CONTRACTOR SHALL SUBMIT APPLICABLE 7460-1 FORMS.

THE CONTRACTOR'S SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) SHALL INCLUDE:

A STATEMENT BY THE CONSTRUCTION CONTRACTOR THAT HE/SHE HAS READ AND WILL ABIDE BY THE CSPP. IN ADDITION, THE SPCD MUST INCLUDE ALL SUPPLEMENTAL INFORMATION THAT COULD NOT BE INCLUDED IN THE CSPP PRIOR TO THE CONTRACT AWARD. THE CONTRACTOR STATEMENT SHOULD INCLUDE THE NAME OF THE CONTRACTOR, THE TITLE OF THE PROJECT CSPP, THE APPROVAL DATE OF THE CSPP, AND A REFERENCE TO ANY SUPPLEMENTAL INFORMATION (THAT IS, "I, NAME OF CONTRACTOR, HAVE READ THE TITLE OF PROJECT CSPP, APPROVED ON DATE, AND WILL ABIDE BY IT AS WRITTEN AND WITH THE FOLLOWING ADDITIONS AS NOTE:"). THE SUPPLEMENTAL INFORMATION IN THE SPCD SHOULD BE WRITTEN TO MATCH THE FORMAT OF THE CSPP INDICATING EACH SUBJECT BY SUPPLEMENTAL INFORMATION IS NECESSARY FOR ANY SPECIFIC SUBJECT, THE STATEMENT, "NO SUPPLEMENTAL INFORMATION," SHOULD BE WRITTEN AFTER THE CORRESPONDING SUBJECT TITLE. THE SPCD SHOULD NOT DUPLICATE INFORMATION IN THE CSPP.

## GENERAL NOTES:

- GENERAL WORK OUTLINED IS NOT ALL INCLUSIVE.
- A MINIMUM OF 10 CALENDAR DAYS NOTICE SHALL BE PROVIDED TO THE ENGINEER AND AIRPORT BY THE CONTRACTOR PRIOR TO MOBILIZATION.
- A MINIMUM OF 15 CALENDAR DAYS NOTICE SHALL BE PROVIDED TO THE ENGINEER AND AIRPORT BY THE CONTRACTOR PRIOR TO BEGINNING ANY CONSTRUCTION ACTIVITIES UNRELATED TO MOBILIZATION.
- THE CONTRACTOR MUST SUBMIT A PRELIMINARY CONSTRUCTION CRITICAL PATH METHOD SCHEDULE 15 DAYS PRIOR TO THE PRE-CONSTRUCTION MEETING OUTLINING THE FOLLOWING: MOBILIZATION TIMELINE, LOCATION OF PROJECT ACTIVITIES WITH CORRESPONDING TIMELINES, ANTICIPATED BID ITEM MILESTONE AND COMPLETION DATES, AND KEY CONSTRUCTION OPERATION DATES WHICH MAY AFFECT AIRFIELD OPERATIONS OR ROADWAY TRAFFIC.
- CONSTRUCTION BARRICADES (INCLUDING SUPPLEMENTARY LIGHTS) NEEDED FOR PROPER EXECUTION OF THE WORK SHALL BE FURNISHED BY THE CONTRACTOR. BARRICADE TYPE AND GEOMETRIC LAYOUT SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL NO LESS THAN 7 CALENDAR DAYS PRIOR TO THE DATE INTENDED TO PLACE THE BARRICADES AND DELINEATE A WORKING AREA.
- RUNWAY 8-26 SHALL BE CLOSED THROUGHOUT THE ENTIRE DURATION OF THE PROJECT.

**Mead  
& Hunt**

Mead and Hunt, Inc.  
878 South Lake Drive  
Lexington, SC 29072  
phone: 803-996-2900  
meadhunt.com

*Augusta*  
Regional Airport

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AUGUSTA REGIONAL AIRPORT  
CONSTRUCT TAXIWAY G

1501 AVIATION WAY  
AUGUSTA, GA 30906-9620

REVISION  
ISSUED FOR BID

NOT FOR CONSTRUCTION

DATE: 0119700-170869.02  
DATE: DECEMBER 6, 2022  
DRAWN BY: EJS  
CHECKED BY: NUH  
DESIGNED BY: DES

DO NOT SCALE DRAWINGS  
SHEET CONTENTS  
PHASING NOTES

SHEET NO.

G-072



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**AUGUSTA REGIONAL AIRPORT  
 CONSTRUCT TAXIWAY G**

1501 AVIATION WAY  
 AUGUSTA, GA 30906-9620

REVISION  
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SHEET CONTENTS  
**CONSTRUCTION  
 SAFETY PHASING  
 PLAN**

SHEET NO:










**G-081**

**WORK SCHEDULE PHASING**

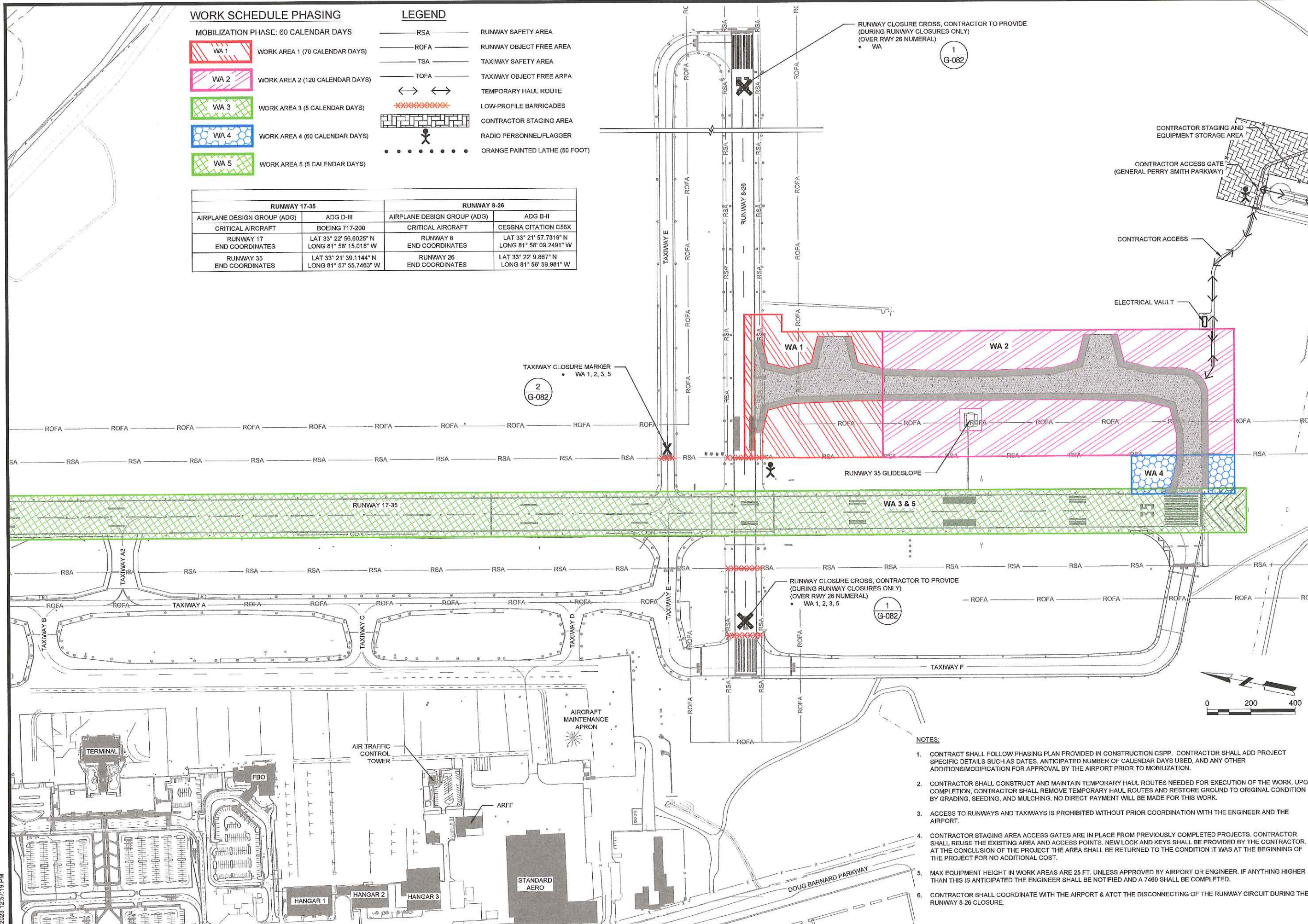
MOBILIZATION PHASE: 60 CALENDAR DAYS

-  **WA 1** WORK AREA 1 (70 CALENDAR DAYS)
-  **WA 2** WORK AREA 2 (120 CALENDAR DAYS)
-  **WA 3** WORK AREA 3 (5 CALENDAR DAYS)
-  **WA 4** WORK AREA 4 (60 CALENDAR DAYS)
-  **WA 5** WORK AREA 5 (5 CALENDAR DAYS)

**LEGEND**

-  **RSA** RUNWAY SAFETY AREA
-  **ROFA** RUNWAY OBJECT FREE AREA
-  **TSA** TAXIWAY SAFETY AREA
-  **TOFA** TAXIWAY OBJECT FREE AREA
-  **TEMPORARY HAUL ROUTE**
-  **LOW-PROFILE BARRICADES**
-  **CONTRACTOR STAGING AREA**
-  **RADIO PERSONNEL/FLAGGER**
-  **ORANGE PAINTED LATHE (50 FOOT)**

RUNWAY 17-35		RUNWAY 8-26	
AIRPLANE DESIGN GROUP (ADG)	ADG D-III	AIRPLANE DESIGN GROUP (ADG)	ADG B-II
CRITICAL AIRCRAFT	BOEING 717-200	CRITICAL AIRCRAFT	CESSNA CITATION C56X
RUNWAY 17 END COORDINATES	LAT 33° 22' 56.6025" N LONG 81° 58' 15.018" W	RUNWAY 8 END COORDINATES	LAT 33° 21' 57.7319" N LONG 81° 58' 09.2491" W
RUNWAY 35 END COORDINATES	LAT 33° 21' 39.1144" N LONG 81° 57' 55.7463" W	RUNWAY 26 END COORDINATES	LAT 33° 22' 9.867" N LONG 81° 56' 59.981" W



RUNWAY CLOSURE CROSS, CONTRACTOR TO PROVIDE (DURING RUNWAY CLOSURES ONLY) (OVER RWY 26 NUMERAL)  
 • WA

1  
 G-082

TAXIWAY CLOSURE MARKER  
 • WA 1, 2, 3, 5

2  
 G-082

RUNWAY CLOSURE CROSS, CONTRACTOR TO PROVIDE (DURING RUNWAY CLOSURES ONLY) (OVER RWY 26 NUMERAL)  
 • WA 1, 2, 3, 5

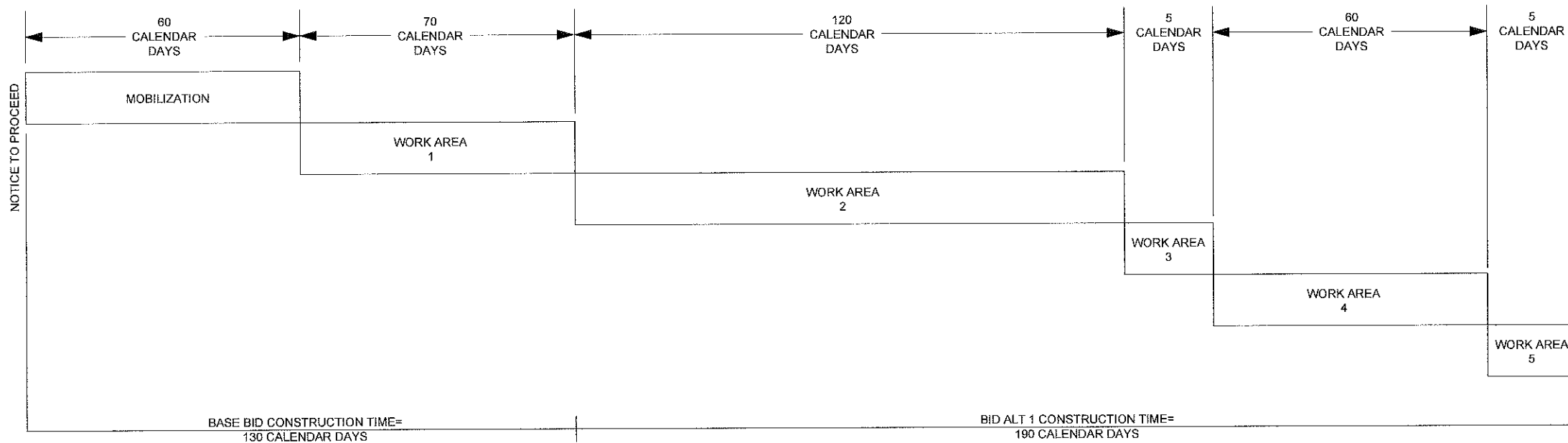
1  
 G-082

**NOTES:**

1. CONTRACTOR SHALL FOLLOW PHASING PLAN PROVIDED IN CONSTRUCTION CSP. CONTRACTOR SHALL ADD PROJECT SPECIFIC DETAILS SUCH AS DATES, ANTICIPATED NUMBER OF CALENDAR DAYS USED, AND ANY OTHER ADDITIONS/MODIFICATIONS FOR APPROVAL BY THE AIRPORT PRIOR TO MOBILIZATION.
2. CONTRACTOR SHALL CONSTRUCT AND MAINTAIN TEMPORARY HAUL ROUTES NEEDED FOR EXECUTION OF THE WORK. UPON COMPLETION, CONTRACTOR SHALL REMOVE TEMPORARY HAUL ROUTES AND RESTORE GROUND TO ORIGINAL CONDITION BY GRADING, SEEDING, AND MULCHING. NO DIRECT PAYMENT WILL BE MADE FOR THIS WORK.
3. ACCESS TO RUNWAYS AND TAXIWAYS IS PROHIBITED WITHOUT PRIOR COORDINATION WITH THE ENGINEER AND THE AIRPORT.
4. CONTRACTOR STAGING AREA ACCESS GATES ARE IN PLACE FROM PREVIOUSLY COMPLETED PROJECTS. CONTRACTOR SHALL REUSE THE EXISTING AREA AND ACCESS POINTS. NEW LOCK AND KEYS SHALL BE PROVIDED BY THE CONTRACTOR. AT THE CONCLUSION OF THE PROJECT THE AREA SHALL BE RETURNED TO THE CONDITION IT WAS AT THE BEGINNING OF THE PROJECT FOR NO ADDITIONAL COST.
5. MAX EQUIPMENT HEIGHT IN WORK AREAS ARE 25 FT. UNLESS APPROVED BY AIRPORT OR ENGINEER. IF ANYTHING HIGHER THAN THIS IS ANTICIPATED THE ENGINEER SHALL BE NOTIFIED AND A 7460 SHALL BE COMPLETED.
6. CONTRACTOR SHALL COORDINATE WITH THE AIRPORT & ATCT THE DISCONNECTING OF THE RUNWAY CIRCUIT DURING THE RUNWAY 8-26 CLOSURE.

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**CONTRACTOR NOTES:**

1. THE CONTRACTOR SHALL DESIGNATE A PERSON ON CALL 24 HOURS A DAY, 7 DAYS A WEEK, FOR EMERGENCY MAINTENANCE OF AIRPORT HAZARD LIGHTING AND BARRICADES. THIS CONTACT INFORMATION MUST BE ON FILE WITH THE AIRPORT OPERATOR.
2. ALL HAZARD LIGHTING, BARRICADES AND EQUIPMENT SHALL BE CHECKED A MINIMUM OF ONCE PER DAY TO ENSURE PROPER OPERATION, PREFERABLY AT DUSK.



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**MOBILIZATION PHASE (60 CALENDAR DAYS)**

THE MOBILIZATION PHASE SHALL BEGIN IMMEDIATELY AFTER THE MOBILIZATION NOTICE TO PROCEED IS ISSUED BY THE AIRPORT. DURING THIS PHASE OF THE PROJECT, NO WORK SHALL BE CONDUCTED THAT RESTRICTS AIRPORT OPERATIONS UNLESS AUTHORIZED BY THE AIRPORT. NOTICE TO PROCEED WITH SUBSEQUENT SCHEDULES MAY BE GIVEN DURING MOBILIZATION AT THE AIRPORTS DISCRETION.

MOBILIZATION WORK SHALL INCLUDE, BUT IS NOT LIMITED TO THE FOLLOWING:

1. SUBMITTALS.
  - A. PROCESSING OF REQUIRED MATERIALS/EQUIPMENT SUBMITTALS AND THE CONTRACTOR'S PROPOSED WORK SCHEDULE, INCLUDING REQUESTED PAVEMENT CLOSURE DATES.
  - B. ALL PRE-QUALIFICATION TESTING, REVIEW, AND APPROVALS.
  - C. MATERIAL DELIVERY SCHEDULE, INCLUDING MATERIAL DELIVERY DATE TO JOB SITE OR TO THE CONTRACTOR'S YARD.
2. DURING MOBILIZATION, THE CONTRACTOR SHALL BE ALLOWED TO PERFORM LAYOUT, STAKING, AND OTHER PREP WORK AS APPROVED BY THE AIRPORT.
3. IT IS THE AIRPORT'S INTENT THAT ALL PRELIMINARY WORK BE COMPLETED DURING THE MOBILIZATION PHASE TO ENSURE CONSTRUCTION CAN BE PURSUED DILIGENTLY AND WITHOUT UNNECESSARY DELAY. (THE AIRPORT RESERVES THE RIGHT TO WAIVE CERTAIN ELEMENTS OF MOBILIZATION AND ISSUE A NOTICE TO PROCEED WITH CONSTRUCTION AT ITS DISCRETION OR UPON THE CONTRACTOR'S REQUEST.) SCHEDULE DATE SHALL NOT BE CHANGED, ONCE ESTABLISHED, UNLESS COORDINATION WITH THE CA TEAM AND FINAL APPROVAL OF THE AIRPORT.

**GENERAL NOTES:**

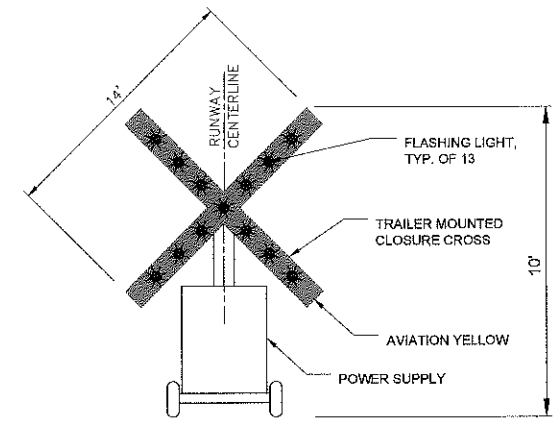
1. NIGHT WORK IS GENERALLY DEFINED AS WORK DONE FROM 11:30 PM TO 5:30 AM. IN WORK AREAS SUBJECT TO COMMERCIAL FLIGHT RESTRICTIONS, NIGHT WORK MUST BE COMPLETED THIRTY (30) MINUTES PRIOR TO THE FIRST DEPARTING COMMERCIAL FLIGHT.
2. HOURS OF AIR TRAFFIC CONTROL TOWER OPERATIONS ARE FROM 6:45AM TO 11:00PM LOCAL TIME.
3. SEE SPECIAL PROVISIONS FOR DETAILS ON CONSTRUCTION LIMITATIONS. CLOSURE OF EXISTING AIRFIELD PAVEMENTS WILL BE ALLOWED ONLY ONCE THE FOLLOWING CONDITIONS ARE MET:
  - A. 48-HOUR ADVANCE NOTIFICATION, VERIFYING THAT A PREVIOUSLY APPROVED CLOSURE PERIOD WILL BE AS SCHEDULED.
  - B. A CLOSURE SCHEDULE FOR EACH AREA OF CONSTRUCTION HAS BEEN SUBMITTED TO THE ENGINEER FOR REVIEW AND HAS BEEN APPROVED. THESE SCHEDULES SHALL DETAIL TEMPORARY ACCESS ROUTES (IF APPLICABLE), SAFETY MEASURES, AND TIME LIMITS OF CLOSURE FOR EACH AREA. FAILURE TO OPEN AIRFIELD PAVEMENTS WITHIN THE APPROVED TIME LIMITS MAY RESULT IN LIQUIDATED DAMAGES.
4. THE AUGUSTA AIRPORT EXPERIENCES HIGH TRAFFIC VOLUMES DURING THE MASTERS WEEK THAT OCCURS THE FIRST OR SECOND WEEK OF APRIL EVERY YEAR. THE BIDDER IS ADVISED THAT A TEMPORARY SUSPENSION OF WORK WILL OCCUR DURING THIS PERIOD OF TIME. (APRIL 3RD THROUGH APRIL 9TH, 2023)
6. ANY ADDITIONAL LOW-PROFILE BARRICADES (INCLUDING SUPPLEMENTARY LIGHTS) NEEDED FOR PROPER EXECUTION OF THE WORK SHALL BE PROVIDED BY THE CONTRACTOR.
7. CONTRACTOR SHALL MAINTAIN ALL LIGHTS IN WORKING ORDER FOR THE DURATION OF THE PROJECT.

**AUGUSTA RICHMOND COUNTY GENERAL NOTES:**

1. ALL DRAINAGE EASEMENTS AND DISTURBED AREAS MUST BE GRASSED AND/OR RIP-RAPPED AS REQUIRED TO CONTROL EROSION.
2. ALL CONSTRUCTION WITHIN AUGUSTA RIGHTS-OF WAY SHALL CONFORM TO AUGUSTA, GEORGIA STANDARDS AND SPECIFICATIONS.
3. ALL SILT BARRIERS MUST BE PLACED IMMEDIATELY FOLLOWING CLEARING. NO GRADING SHALL BE DONE UNTIL SILT BARRIER INSTALLATION IS COMPLETED.
4. CONTRACTOR SHALL CONTACT THE INSPECTION DIVISION OF THE PUBLIC WORKS DEPARTMENT AT LEAST 48 HOURS PRIOR TO STARTING WORK ON THE PROJECT. THE PHONE NUMBER FOR THIS OFFICE IS (706) 821-1706.
5. THE COST OF INSPECTION BY THE CITY OF AUGUSTA-RICHMOND COUNTY'S DEPARTMENT OF PUBLIC WORKS AND ENGINEERING, BEFORE OR AFTER REGULAR WORKING HOURS, ON SATURDAYS, SUNDAYS, OR LEGAL HOLIDAYS, SHALL BE PAID FOR BY THE INDIVIDUAL REQUESTING THE INSPECTION AT A RATE OF 1 1/2 TIMES THE REGULAR SALARY PER HOUR OF THE INSPECTOR PLUS 7.65% FROM THE EMPLOYER'S FICA/MEDICARE MATCH. APPROVAL FOR THE INSPECTION OUTSIDE OF NORMAL WORKING HOURS SHALL BE OBTAINED FROM THE CITY ENGINEER 48 HOURS IN ADVANCE. PRIOR TO THE COMMENCEMENT OF WORK REQUIRING INSPECTION OUTSIDE OF NORMAL WORKING HOURS, THE INDIVIDUAL REQUESTING THE INSPECTION SHALL SIGN A FORM WHICH IS FURNISHED BY THE DEPARTMENT OF PUBLIC WORKS AND ENGINEERING AGREEING TO PAY THE OVERTIME. THE INDIVIDUAL REQUESTING THE INSPECTION SHALL SIGN A FORM WHICH IS FURNISHED BY THE DEPARTMENT OF PUBLIC WORKS AND ENGINEERING AGREEING TO PAY THE OVERTIME. THE INDIVIDUAL REQUESTING THE INSPECTION WILL BE BILLED BY THE DEPARTMENT OF PUBLIC WORKS AND ENGINEERING FOR PAYMENT.
6. A PRECONSTRUCTION CONFERENCE SHALL BE HELD WITH THE CITY ENGINEER OR HIS DESIGNATED REPRESENTATIVE PRIOR TO BEGINNING CONSTRUCTION. THIS MEETING SHALL BE SCHEDULED WITH THE DEPARTMENT OF PUBLIC WORKS AT THE TIME THE NOTIFICATION OF WORK COMMENCEMENT IS GIVEN.

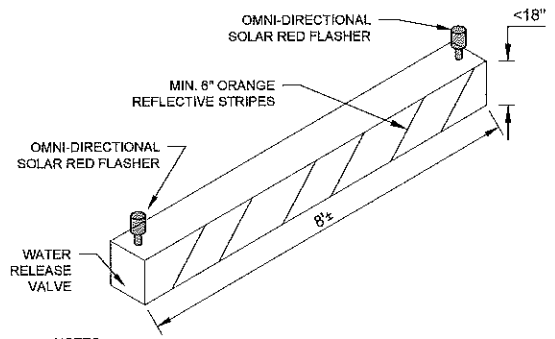
**REQUIREMENTS FOR AIRPORT SECURITY, SAFETY AND CONTRACTOR OPERATIONS:**

1. ALL CONSTRUCTION SHALL BE IN COMPLIANCE WITH THE SPECIFICATION GENERAL PROVISIONS, SAFETY, AIRPORT SECURITY, AND OPERATING REGULATIONS AND THE CONSTRUCTION SAFETY AND PHASING PLAN (CSPP).
2. RUNWAY 17-35 AND RUNWAY 8-26, ASSOCIATED TAXIWAYS, AND THEIR SAFETY AREAS SHALL REMAIN OPERATIONAL AT ALL TIMES UNLESS APPROVED BY THE AIRPORT.
3. CONTRACTOR SHALL NOT ENTER ANY ACTIVE SAFETY AREAS OR OBJECT FREE AREAS WITHOUT AUTHORIZATION FROM THE AIRPORT.
4. A RUNWAY SAFETY AREA (RSA) AND OBSTACLE FREE ZONE (OFZ) EXISTS AROUND EACH RUNWAY WHICH LIMITS CONSTRUCTION ACTIVITIES ADJACENT TO OPERATIONAL RUNWAYS. CONSTRUCTION PERSONNEL AND EQUIPMENT SHALL NOT CROSS RSAs OR OFZs WITHOUT AIRPORT AUTHORIZATION.
5. THE RUNWAY 17-35 SAFETY AREA (RSA) IS 250-FEET EACH SIDE OF THE RUNWAY CENTERLINE AND 200 FEET BEYOND THE RUNWAY ENDS. THE RUNWAY 8-26 SAFETY AREA (RSA) IS 75-FEET EACH SIDE OF THE RUNWAY CENTERLINE. IT SHALL REMAIN CLEAR OF PERSONNEL, MATERIAL AND EQUIPMENT AT ALL TIMES.
6. THE RUNWAY 17-35 OBSTACLE FREE ZONE (OFZ) IS 200-FEET EACH SIDE OF CENTERLINE AND 200-FEET BEYOND THE RUNWAY ENDS. THE RUNWAY 8-26 OBSTACLE FREE ZONE (OFZ) IS 125-FEET EACH SIDE OF CENTERLINE AND 200-FEET BEYOND THE RUNWAY ENDS. IT SHALL REMAIN CLEAR OF PERSONNEL, MATERIALS, AND EQUIPMENT AT ALL TIMES.
7. IN TRANSITIONS FROM PAVED TO UNPAVED AREAS, A TEMPORARY 3 INCH MAXIMUM VERTICAL DROP IS ALLOWED.
8. DAILY SAFETY INSPECTIONS SHALL BE PERFORMED AS REQUIRED IN THE CSPP.



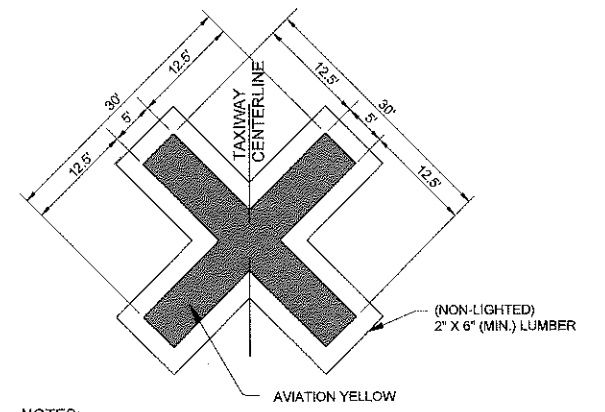
- NOTES:**
1. TWO (2) LIGHTED, GENERATOR POWERED RUNWAY CLOSURE CROSSES SHALL BE SUPPLIED BY THE CONTRACTOR. THE RUNWAY CLOSURE CROSSES SHALL BE INSTALLED AND REMOVED BY THE CONTRACTOR DURING CONSTRUCTION PER THE SCHEDULE APPROVED BY THE ENGINEER. THE EQUIPMENT SHALL BE FUELED, OILED AND MAINTAINED BY THE CONTRACTOR THROUGHOUT THE PROJECT.

PORTABLE LIGHTED RUNWAY CLOSURE CROSS  
SCALE: NTS



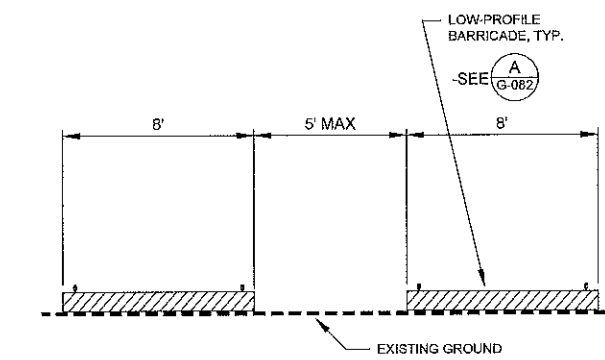
- NOTES:**
1. MAINTENANCE OF LOW-PROFILE BARRICADES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR FOR THE DURATION OF THE PROJECT.
  2. GAPS IN BARRICADES SHALL NOT EXCEED 5-FEET.

LOW-PROFILE BARRICADE DETAIL  
SCALE: NTS



- NOTES:**
1. TAXIWAY CLOSURE MARKERS SHALL BE PLACED AS DESIGNATED ON THE CONSTRUCTION SAFETY AND PHASING PLANS.
  2. TAXIWAY CLOSURE MARKERS WILL BE SUPPLIED AND MAINTAINED BY THE CONTRACTOR.
  3. CLOSURE MARKERS SHALL BE ADEQUATELY SECURED SO AS TO NOT BE AFFECTED BY WIND OR AIRCRAFT MOVEMENT.
  4. CONTRACTOR IS RESPONSIBLE FOR PLACING AND MAINTAINING TAXIWAY CLOSURE MARKERS FOR THE DURATION OF THE PROJECT AND REMOVING THE CROSSES UPON COMPLETION OF THE PROJECT OR OPENING OF THE ASSOCIATED TAXIWAY.

PORTABLE VINYL CLOSED TAXIWAY MARKER  
SCALE: NTS



LOW-PROFILE BARRICADE LAYOUT DETAIL  
SCALE: NTS

**AUGUSTA REGIONAL AIRPORT  
CONSTRUCT TAXIWAY G**

1501 AVIATION WAY  
AUGUSTA, GA 30906-9620

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SHEET CONTAINS  
CONSTRUCTION SAFETY PHASING PLAN - WA MATRIX

SHEET NO

**G-082**

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## AUGUSTA REGIONAL AIRPORT CONSTRUCT TAXIWAY G

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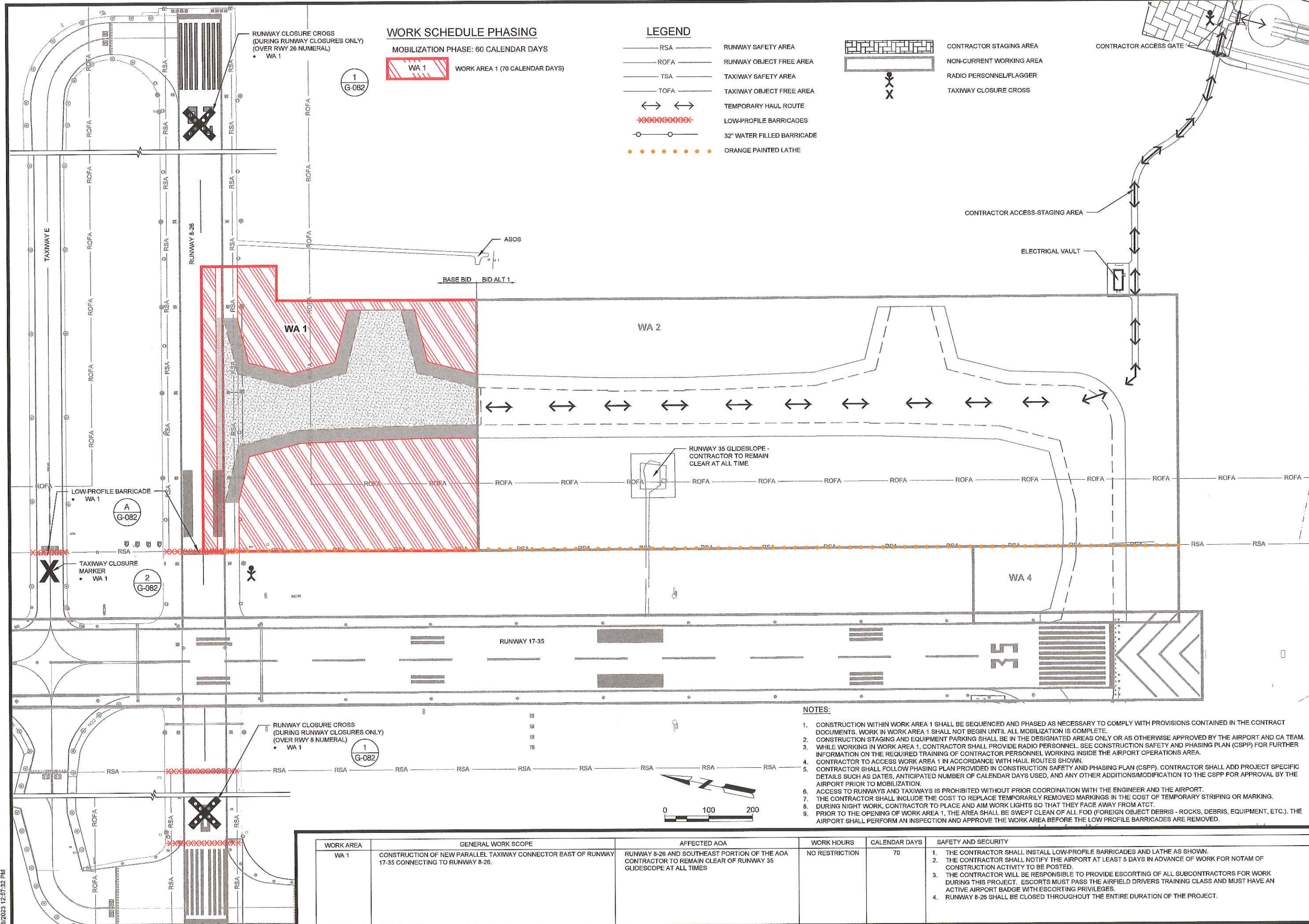
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SHEET CONTENTS  
**CONSTRUCTION  
SAFETY PHASING  
PLAN WA 1 - BASE BID**

SHEET NO.

# G-083



### WORK SCHEDULE PHASING

MOBILIZATION PHASE: 60 CALENDAR DAYS  
**WA 1** WORK AREA 1 (70 CALENDAR DAYS)

### LEGEND

- RSA — RUNWAY SAFETY AREA
  - ROFA — RUNWAY OBJECT FREE AREA
  - TSA — TAXIWAY SAFETY AREA
  - TOFA — TAXIWAY OBJECT FREE AREA
  - ↔ ↔ TEMPORARY HAUL ROUTE
  - ⊘ ⊘ LOW-PROFILE BARRICADES
  - ○ 32" WATER FILLED BARRICADE
  - ● ORANGE PAINTED LATHE
- CONTRACTOR STAGING AREA
  - NON-CURRENT WORKING AREA
  - RADIO PERSONNEL/FLAGGER
  - TAXIWAY CLOSURE CROSS

1  
G-082

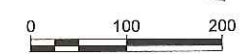
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G-082

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G-082

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G-082

### NOTES:

1. CONSTRUCTION WITHIN WORK AREA 1 SHALL BE SEQUENCED AND PHASED AS NECESSARY TO COMPLY WITH PROVISIONS CONTAINED IN THE CONTRACT DOCUMENTS. WORK IN WORK AREA 1 SHALL NOT BEGIN UNTIL ALL MOBILIZATION IS COMPLETE.
2. CONSTRUCTION STAGING AND EQUIPMENT PARKING SHALL BE IN THE DESIGNATED AREAS ONLY OR AS OTHERWISE APPROVED BY THE AIRPORT AND CA TEAM.
3. WHILE WORKING IN WORK AREA 1, CONTRACTOR SHALL PROVIDE RADIO PERSONNEL. SEE CONSTRUCTION SAFETY AND PHASING PLAN (CSPP) FOR FURTHER INFORMATION ON THE REQUIRED TRAINING OF CONTRACTOR PERSONNEL WORKING INSIDE THE AIRPORT OPERATIONS AREA.
4. CONTRACTOR TO ACCESS WORK AREA 1 IN ACCORDANCE WITH HAUL ROUTES SHOWN.
5. CONTRACTOR SHALL FOLLOW PHASING PLAN PROVIDED IN CONSTRUCTION SAFETY AND PHASING PLAN (CSPP). CONTRACTOR SHALL ADD PROJECT SPECIFIC DETAILS SUCH AS DATES, ANTICIPATED NUMBER OF CALENDAR DAYS USED, AND ANY OTHER ADDITIONS/MODIFICATION TO THE CSPP FOR APPROVAL BY THE AIRPORT PRIOR TO MOBILIZATION.
6. ACCESS TO RUNWAYS AND TAXIWAYS IS PROHIBITED WITHOUT PRIOR COORDINATION WITH THE ENGINEER AND THE AIRPORT.
7. THE CONTRACTOR SHALL INCLUDE THE COST TO REPLACE TEMPORARILY REMOVED MARKINGS IN THE COST OF TEMPORARY STRIPPING OR MARKING.
8. DURING NIGHT WORK, CONTRACTOR TO PLACE AND AIM WORK LIGHTS SO THAT THEY FACE AWAY FROM ATCT.
9. PRIOR TO THE OPENING OF WORK AREA 1, THE AREA SHALL BE SWEEPED CLEAN OF ALL FOD (FOREIGN OBJECT DEBRIS - ROCKS, DEBRIS, EQUIPMENT, ETC.). THE AIRPORT SHALL PERFORM AN INSPECTION AND APPROVE THE WORK AREA BEFORE THE LOW PROFILE BARRICADES ARE REMOVED.



WORK AREA	GENERAL WORK SCOPE	AFFECTED AOA	WORK HOURS	CALENDAR DAYS	SAFETY AND SECURITY
WA 1	CONSTRUCTION OF NEW PARALLEL TAXIWAY CONNECTOR EAST OF RUNWAY 17-35 CONNECTING TO RUNWAY 8-26.	RUNWAY 8-26 AND SOUTHEAST PORTION OF THE AOA CONTRACTOR TO REMAIN CLEAR OF RUNWAY 35 GLIDESCOPE AT ALL TIMES	NO RESTRICTION	70	<ol style="list-style-type: none"> <li>1. THE CONTRACTOR SHALL INSTALL LOW-PROFILE BARRICADES AND LATHE AS SHOWN.</li> <li>2. THE CONTRACTOR SHALL NOTIFY THE AIRPORT AT LEAST 5 DAYS IN ADVANCE OF WORK FOR NOTAM OF CONSTRUCTION ACTIVITY TO BE POSTED.</li> <li>3. THE CONTRACTOR WILL BE RESPONSIBLE TO PROVIDE ESCORTING OF ALL SUBCONTRACTORS FOR WORK DURING THIS PROJECT. ESCORTS MUST PASS THE AIRFIELD DRIVERS TRAINING CLASS AND MUST HAVE AN ACTIVE AIRPORT BADGE WITH ESCORTING PRIVILEGES.</li> <li>4. RUNWAY 8-26 SHALL BE CLOSED THROUGHOUT THE ENTIRE DURATION OF THE PROJECT.</li> </ol>

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**AUGUSTA REGIONAL AIRPORT  
 CONSTRUCT TAXIWAY G**

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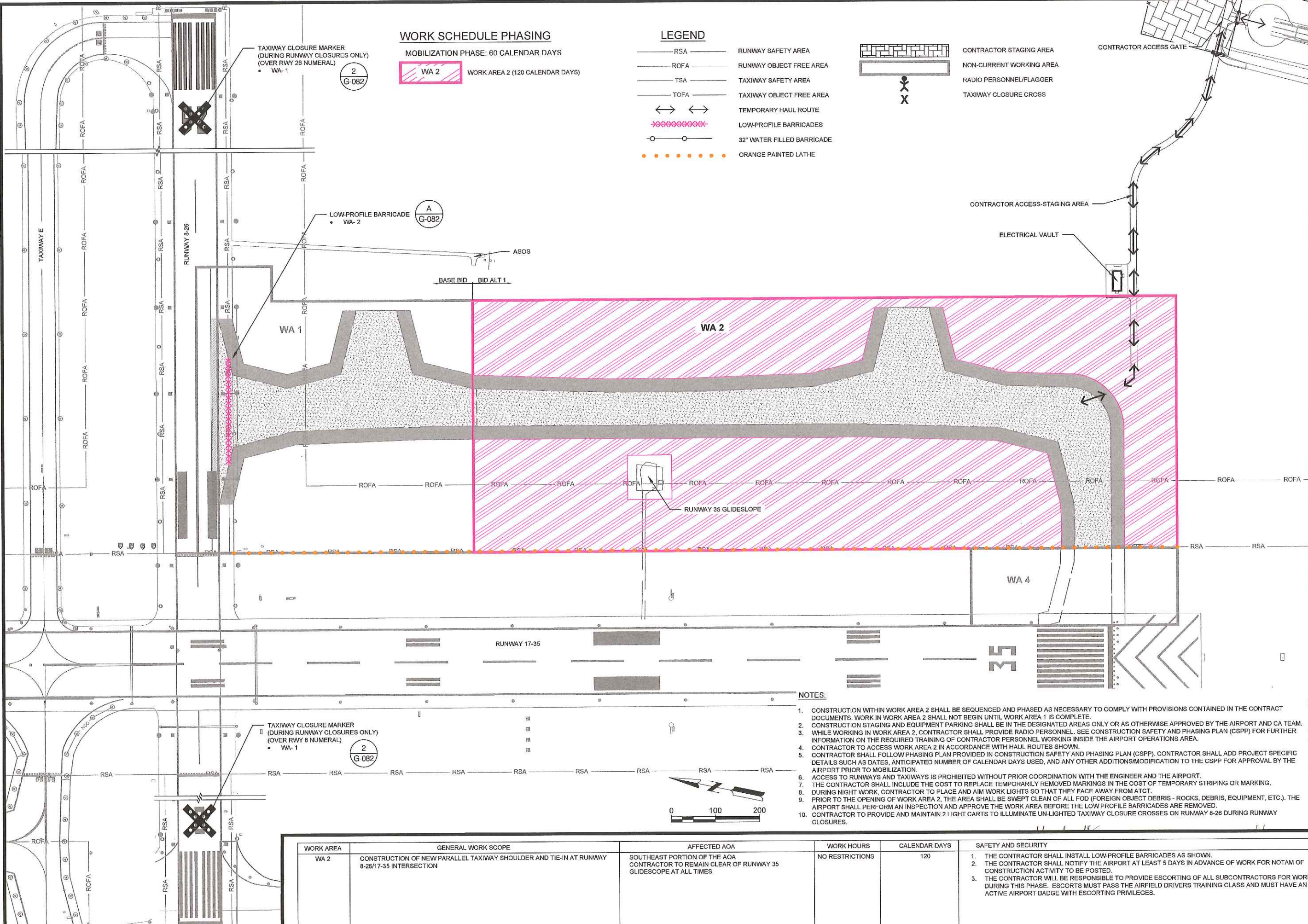
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**CONSTRUCTION  
 SAFETY PHASING  
 PLAN WA 2 - BID ALT 1**

SHEET NO.

**G-084**



**WORK SCHEDULE PHASING**

MOBILIZATION PHASE: 60 CALENDAR DAYS  
 WA 2 WORK AREA 2 (120 CALENDAR DAYS)

**LEGEND**

- RSA — RUNWAY SAFETY AREA
- ROFA — RUNWAY OBJECT FREE AREA
- TSA — TAXIWAY SAFETY AREA
- TOFA — TAXIWAY OBJECT FREE AREA
- ↔ ↔ TEMPORARY HAUL ROUTE
- ⊗⊗⊗⊗⊗⊗ LOW-PROFILE BARRICADES
- ○ 32" WATER FILLED BARRICADE
- ● ● ● ● ORANGE PAINTED LATHE
- ▭ CONTRACTOR STAGING AREA
- ▭ NON-CURRENT WORKING AREA
- ⊗ RADIO PERSONNEL/FLAGGER
- ⊗ TAXIWAY CLOSURE CROSS

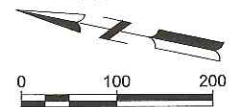
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 G-082

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**NOTES:**

1. CONSTRUCTION WITHIN WORK AREA 2 SHALL BE SEQUENCED AND PHASED AS NECESSARY TO COMPLY WITH PROVISIONS CONTAINED IN THE CONTRACT DOCUMENTS. WORK IN WORK AREA 2 SHALL NOT BEGIN UNTIL WORK AREA 1 IS COMPLETE.
2. CONSTRUCTION STAGING AND EQUIPMENT PARKING SHALL BE IN THE DESIGNATED AREAS ONLY OR AS OTHERWISE APPROVED BY THE AIRPORT AND CA TEAM.
3. WHILE WORKING IN WORK AREA 2, CONTRACTOR SHALL PROVIDE RADIO PERSONNEL. SEE CONSTRUCTION SAFETY AND PHASING PLAN (CSPP) FOR FURTHER INFORMATION ON THE REQUIRED TRAINING OF CONTRACTOR PERSONNEL WORKING INSIDE THE AIRPORT OPERATIONS AREA.
4. CONTRACTOR TO ACCESS WORK AREA 2 IN ACCORDANCE WITH HAUL ROUTES SHOWN.
5. CONTRACTOR SHALL FOLLOW PHASING PLAN PROVIDED IN CONSTRUCTION SAFETY AND PHASING PLAN (CSPP). CONTRACTOR SHALL ADD PROJECT SPECIFIC DETAILS SUCH AS DATES, ANTICIPATED NUMBER OF CALENDAR DAYS USED, AND ANY OTHER ADDITIONS/MODIFICATION TO THE CSPP FOR APPROVAL BY THE AIRPORT PRIOR TO MOBILIZATION.
6. ACCESS TO RUNWAYS AND TAXIWAYS IS PROHIBITED WITHOUT PRIOR COORDINATION WITH THE ENGINEER AND THE AIRPORT.
7. THE CONTRACTOR SHALL INCLUDE THE COST TO REPLACE TEMPORARILY REMOVED MARKINGS IN THE COST OF TEMPORARY STRIPING OR MARKING.
8. DURING NIGHT WORK, CONTRACTOR TO PLACE AND AIM WORK LIGHTS SO THAT THEY FACE AWAY FROM ATCT.
9. PRIOR TO THE OPENING OF WORK AREA 2, THE AREA SHALL BE SWEEPED CLEAN OF ALL FOD (FOREIGN OBJECT DEBRIS - ROCKS, DEBRIS, EQUIPMENT, ETC.), THE AIRPORT SHALL PERFORM AN INSPECTION AND APPROVE THE WORK AREA BEFORE THE LOW PROFILE BARRICADES ARE REMOVED.
10. CONTRACTOR TO PROVIDE AND MAINTAIN 2 LIGHT CARTS TO ILLUMINATE UN-LIGHTED TAXIWAY CLOSURE CROSSES ON RUNWAY 8-26 DURING RUNWAY CLOSURES.



WORK AREA	GENERAL WORK SCOPE	AFFECTED AOA	WORK HOURS	CALENDAR DAYS	SAFETY AND SECURITY
WA 2	CONSTRUCTION OF NEW PARALLEL TAXIWAY SHOULDER AND TIE-IN AT RUNWAY 8-26/17-35 INTERSECTION	SOUTHEAST PORTION OF THE AOA CONTRACTOR TO REMAIN CLEAR OF RUNWAY 35 GLIDESCOPE AT ALL TIMES	NO RESTRICTIONS	120	1. THE CONTRACTOR SHALL INSTALL LOW-PROFILE BARRICADES AS SHOWN. 2. THE CONTRACTOR SHALL NOTIFY THE AIRPORT AT LEAST 5 DAYS IN ADVANCE OF WORK FOR NOTAM OF CONSTRUCTION ACTIVITY TO BE POSTED. 3. THE CONTRACTOR WILL BE RESPONSIBLE TO PROVIDE ESCORTING OF ALL SUBCONTRACTORS FOR WORK DURING THIS PHASE. ESCORTS MUST PASS THE AIRFIELD DRIVERS TRAINING CLASS AND MUST HAVE AN ACTIVE AIRPORT BADGE WITH ESCORTING PRIVILEGES.

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**AUGUSTA REGIONAL AIRPORT  
 CONSTRUCT TAXIWAY G**

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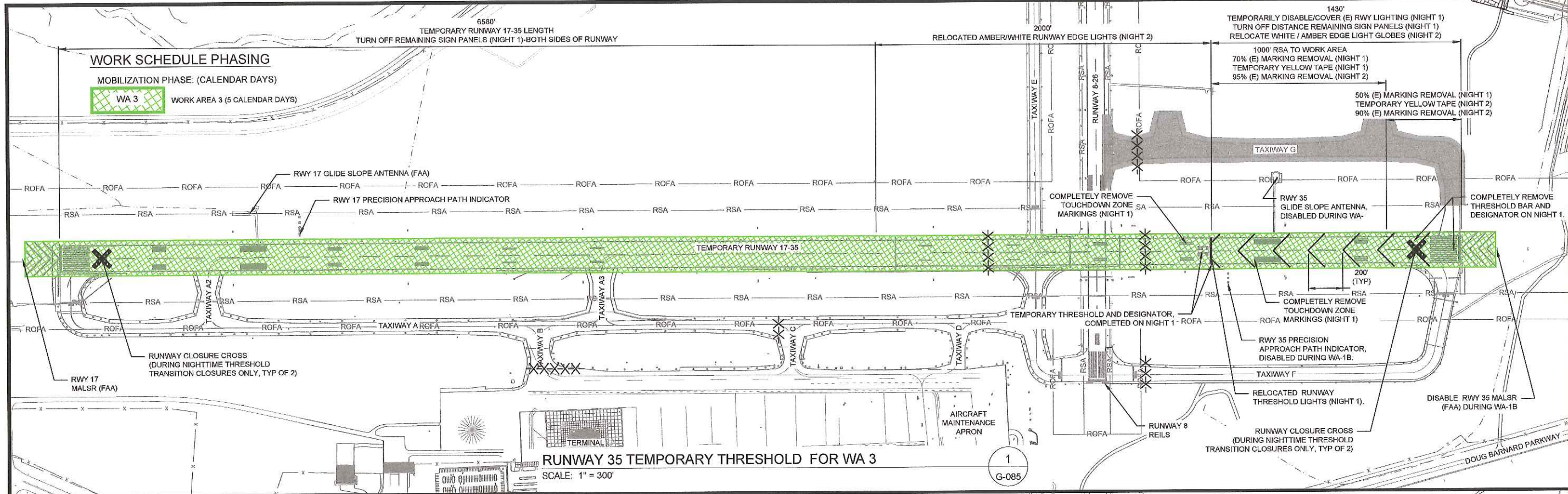
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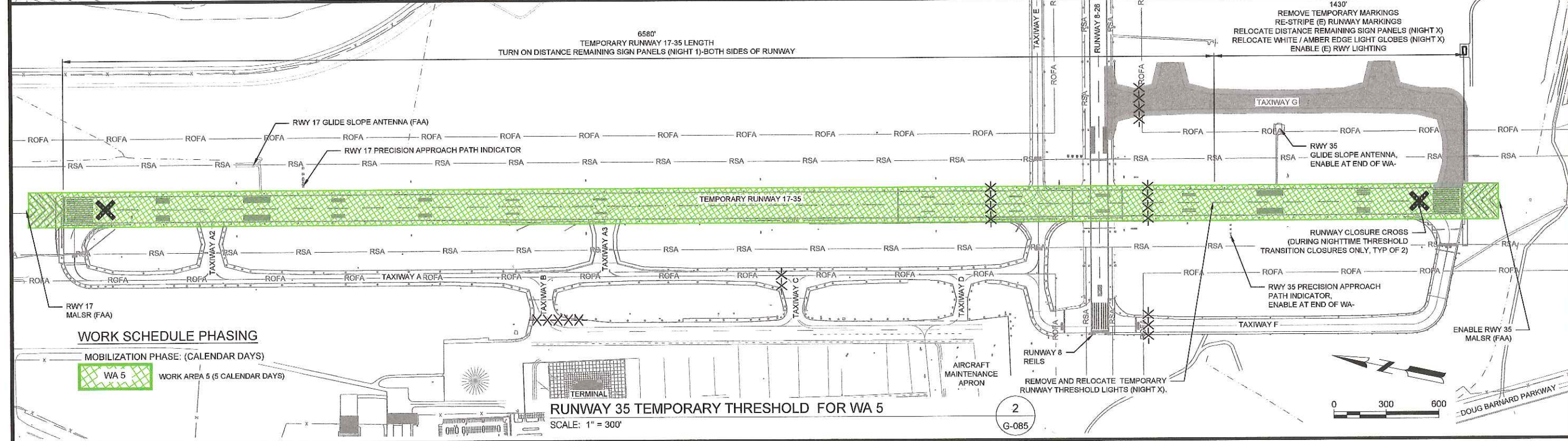
SHEET CONTENTS  
**CONSTRUCTION  
 SAFETY PHASING  
 PLAN WA 3 & 5 - BID  
 ALT 1**

SHEET NO

**G-085**



**RUNWAY 35 TEMPORARY THRESHOLD FOR WA 3**  
 SCALE: 1" = 300'



**RUNWAY 35 TEMPORARY THRESHOLD FOR WA 5**  
 SCALE: 1" = 300'

**TEMPORARY RUNWAY 35 THRESHOLD RELOCATION NOTES:**

- THE CONTRACTOR SHALL INCLUDE THE COST OF ITEMS IN THE LUMP SUM COST FOR "TEMPORARY THRESHOLD RELOCATION" AS FOLLOWS: REMOVAL OF (E) RUNWAY MARKINGS, REMOVAL OF TEMPORARY RUNWAY MARKINGS, REPLACING THE (E) RUNWAY MARKINGS, INSTALLATION AND REMOVAL OF TEMPORARY THRESHOLD LIGHTS, RELOCATING THE "WHITE/AMBER" RUNWAY EDGE LIGHT GLOBES, ACTIVATING AND DEACTIVATING ALL NAVAIDS AS INDICATED, ACTIVATING AND DEACTIVATING ALL RUNWAY LIGHTING AS INDICATED. REFER TO SHEET C-681 FOR ALL WORK RELATED TO THE TEMPORARY RELOCATION OF THE RUNWAY 35 THRESHOLD.
- CONTRACTOR SHALL FOLLOW PHASING PLAN PROVIDED IN CSPP. CONTRACTOR SHALL ADD PROJECT SPECIFIC DETAILS SUCH AS DATES, ANTICIPATED NUMBER OF CALENDAR DAYS USED, AND ANY OTHER ADDITIONS/MODIFICATION FOR APPROVAL BY THE AIRPORT PRIOR TO MOBILIZATION. NO CONSTRUCTION IN WA 4 SHALL OCCUR UNTIL WA 3 TEMPORARY RUNWAY 35 THRESHOLD RELOCATION HAS BEEN COMPLETED IN ITS ENTIRETY.
- THE CURRENT RUNWAY 35 THRESHOLD SHALL BE TEMPORARILY DISPLACED 1430 FEET TO THE NORTH TO PROVIDE 6580 FEET OF USABLE RUNWAY WITH A MINIMUM HEIGHT OF 20 FEET CLEARANCE OVER PROPOSED WORK ON A 50:1 APPROACH.
- PHASE 1 DURATION** - RUNWAY 17-35 SHALL BE CLOSED FOR A TOTAL OF TWO (2) NIGHTTIME RUNWAY CLOSURES, AND SHALL BE OPERATIONAL DURING THE INTERIM DAYTIME LIMITATION HOURS AS DEFINED IN THE WORK AREA MATRIX ON SHEET G-082.
- PHASE 2 DURATION** - RUNWAY 17-35 SHALL BE CLOSED FOR A TOTAL OF THREE (THREE) NIGHTTIME RUNWAY CLOSURES, AND SHALL BE OPERATIONAL DURING THE INTERIM DAYTIME LIMITATION HOURS AS DEFINED IN THE WORK AREA MATRIX ON SHEET G-082.
- MARKING CHANGES** - TEMPORARY PAVEMENT MARKING APPLICATION RATES SHALL BE APPLIED FOR THE TEMPORARILY RELOCATED RUNWAY 35 THRESHOLD, WHICH INCLUDE THE YELLOW CHEVRON MARKINGS FOR 150-FEET LEADING UP TO THE TEMPORARY RUNWAY 35 THRESHOLD. TEMPORARY SURFACE ADHESION TAPE SHALL BE USED FOR THE REMAINDER OF THE YELLOW CHEVRON MARKINGS.
- RUNWAY LIGHTING CHANGES** - THE TEMPORARILY RELOCATED THRESHOLD WILL HAVE TEMPORARY THRESHOLD LIGHTS. ALL AIRFIELD LIGHTING, EXCEPT RUNWAY 8-26 AND ITS ASSOCIATED TAXIWAYS, WILL BE DISABLED DURING THE NIGHTTIME RUNWAY 17-35 CLOSURE PERIODS. ALL LIGHTING AND SIGNAGE ALTERATIONS AS SHOWN ON THIS SHEET SHALL BE COMPLETED ON THE INITIAL RUNWAY CLOSURE NIGHT PRIOR TO OPENING RUNWAY 17-35 THE FOLLOWING DAY.
- IMPACTS TO NAVAIDS** - THE TEMPORARY RUNWAY 35 RELOCATED THRESHOLD WILL HAVE TEMPORARILY RELOCATED THRESHOLD LIGHTS, EDGE LIGHT GLOBES, AND DISTANCE REMAINING SIGNS; HOWEVER, THE CURRENT RUNWAY 35 PAPI, MALSF, GLIDE SLOPE, SHALL BE DEACTIVATED. RUNWAY 17 NAVAIDS TO REMAIN FUNCTIONAL WHEN RUNWAY 17-35 IS OPERATIONAL.
- RUNWAY 17/35 RE-PAINTING** - THE ENTIRETY OF THE PRECONSTRUCTION RUNWAY 17-35 MARKINGS WILL BE RE-PAINTED DURING

**WA X FOR RELOCATING THE RUNWAY 35 THRESHOLD.**

WORK AREA	GENERAL WORK SCOPE	AFFECTED AOA	WORK HOURS	CALENDAR DAYS
WA 3	DISPLACE RUNWAY 17-35 THRESHOLD 1430 FEET	RUNWAY 17-35	NIGHTTIME CLOSURES	5
WA 5	REMOVE DISPLACED RUNWAY 17-35 THRESHOLD AND REPAINT RUNWAY MARKINGS	RUNWAY 17-35	NIGHTTIME CLOSURES	5

**SAFETY AND SECURITY**

- THE CONTRACTOR SHALL INSTALL LOW-PROFILE BARRICADES AS SHOWN.
- THE CONTRACTOR SHALL NOTIFY THE AIRPORT AT LEAST 5 DAYS IN ADVANCE OF WORK FOR NOTAM OF CONSTRUCTION ACTIVITY TO BE POSTED.
- THE CONTRACTOR WILL BE RESPONSIBLE TO PROVIDE ESCORTING OF ALL SUBCONTRACTORS FOR WORK DURING THIS PHASE. ESCORTS MUST PASS THE AIRFIELD DRIVERS TRAINING CLASS AND MUST HAVE AN ACTIVE AIRPORT BADGE WITH ESCORTING PRIVILEGES.

X:\0119700\170869.02\TECH\DRAWINGS\G-085 CONSTRUCTION SAFETY PHASING PLAN WA 1.DWG  
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**AUGUSTA REGIONAL AIRPORT  
 CONSTRUCT TAXIWAY G**

1501 AVIATION WAY  
 AUGUSTA, GA 30906-9620

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SHEET CONTENTS  
 CONSTRUCTION  
 SAFETY PHASING  
 PLAN WA 4 - BID ALT 1









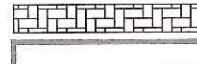




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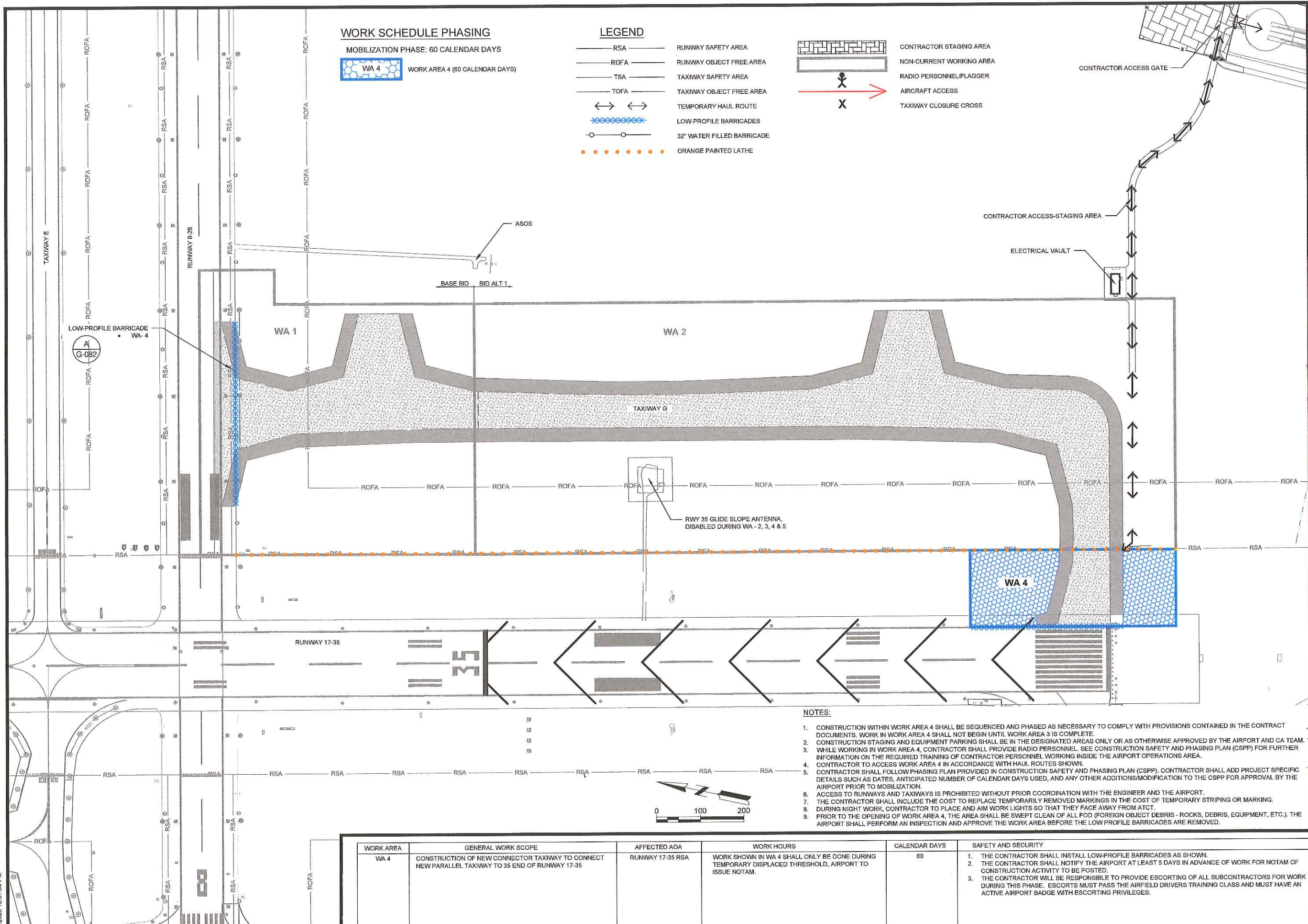
**G-086**

**WORK SCHEDULE PHASING**

MOBILIZATION PHASE: 60 CALENDAR DAYS  
 **WA 4** WORK AREA 4 (60 CALENDAR DAYS)

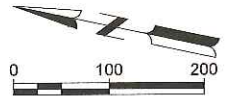
**LEGEND**

-  RSA RUNWAY SAFETY AREA
-  ROFA RUNWAY OBJECT FREE AREA
-  TSA TAXIWAY SAFETY AREA
-  TOFA TAXIWAY OBJECT FREE AREA
-  TEMPORARY HAUL ROUTE
-  LOW-PROFILE BARRICADES
-  32" WATER FILLED BARRICADE
-  ORANGE PAINTED LATHE
-  CONTRACTOR STAGING AREA
-  NON-CURRENT WORKING AREA
-  RADIO PERSONNEL/FLAGGER
-  AIRCRAFT ACCESS
-  TAXIWAY CLOSURE CROSS



**NOTES:**

1. CONSTRUCTION WITHIN WORK AREA 4 SHALL BE SEQUENCED AND PHASED AS NECESSARY TO COMPLY WITH PROVISIONS CONTAINED IN THE CONTRACT DOCUMENTS. WORK IN WORK AREA 4 SHALL NOT BEGIN UNTIL WORK AREA 3 IS COMPLETE.
2. CONSTRUCTION STAGING AND EQUIPMENT PARKING SHALL BE IN THE DESIGNATED AREAS ONLY OR AS OTHERWISE APPROVED BY THE AIRPORT AND CA TEAM.
3. WHILE WORKING IN WORK AREA 4, CONTRACTOR SHALL PROVIDE RADIO PERSONNEL. SEE CONSTRUCTION SAFETY AND PHASING PLAN (CSPP) FOR FURTHER INFORMATION ON THE REQUIRED TRAINING OF CONTRACTOR PERSONNEL WORKING INSIDE THE AIRPORT OPERATIONS AREA.
4. CONTRACTOR TO ACCESS WORK AREA 4 IN ACCORDANCE WITH HAUL ROUTES SHOWN.
5. CONTRACTOR SHALL FOLLOW PHASING PLAN PROVIDED IN CONSTRUCTION SAFETY AND PHASING PLAN (CSPP). CONTRACTOR SHALL ADD PROJECT SPECIFIC DETAILS SUCH AS DATES, ANTICIPATED NUMBER OF CALENDAR DAYS USED, AND ANY OTHER ADDITIONS/MODIFICATIONS TO THE CSPP FOR APPROVAL BY THE AIRPORT PRIOR TO MOBILIZATION.
6. ACCESS TO RUNWAYS AND TAXIWAYS IS PROHIBITED WITHOUT PRIOR COORDINATION WITH THE ENGINEER AND THE AIRPORT.
7. THE CONTRACTOR SHALL INCLUDE THE COST TO REPLACE TEMPORARILY REMOVED MARKINGS IN THE COST OF TEMPORARY STRIPING OR MARKING.
8. DURING NIGHT WORK, CONTRACTOR TO PLACE AND AIM WORK LIGHTS SO THAT THEY FACE AWAY FROM ATCT.
9. PRIOR TO THE OPENING OF WORK AREA 4, THE AREA SHALL BE SWEEPED CLEAN OF ALL FOD (FOREIGN OBJECT DEBRIS - ROCKS, DEBRIS, EQUIPMENT, ETC.). THE AIRPORT SHALL PERFORM AN INSPECTION AND APPROVE THE WORK AREA BEFORE THE LOW PROFILE BARRICADES ARE REMOVED.

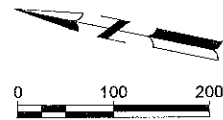
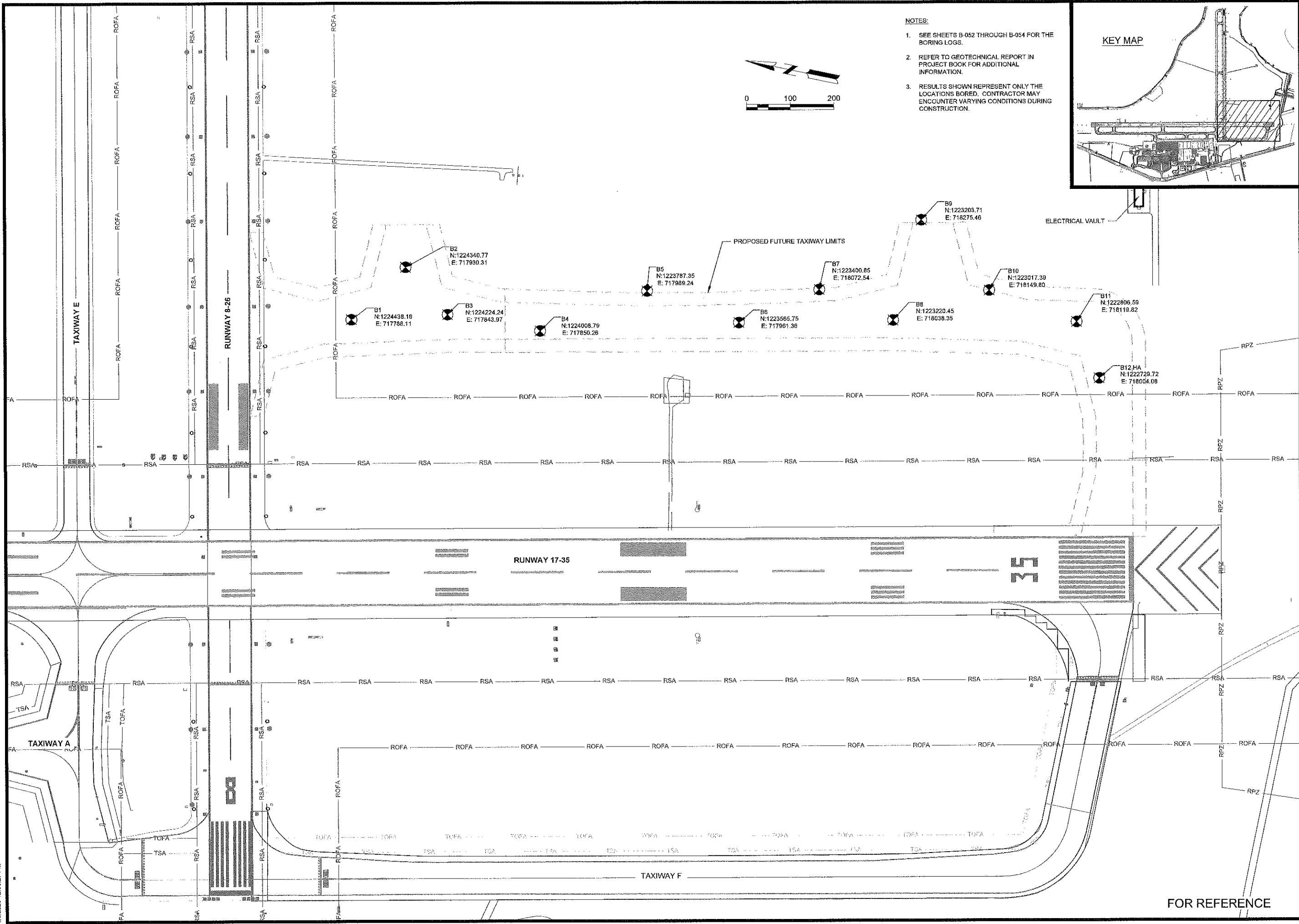


WORK AREA	GENERAL WORK SCOPE	AFFECTED AOA	WORK HOURS	CALENDAR DAYS	SAFETY AND SECURITY
WA 4	CONSTRUCTION OF NEW CONNECTOR TAXIWAY TO CONNECT NEW PARALLEL TAXIWAY TO 35 END OF RUNWAY 17-35	RUNWAY 17-35 RSA	WORK SHOWN IN WA 4 SHALL ONLY BE DONE DURING TEMPORARY DISPLACED THRESHOLD, AIRPORT TO ISSUE NOTAM.	60	<ol style="list-style-type: none"> <li>1. THE CONTRACTOR SHALL INSTALL LOW-PROFILE BARRICADES AS SHOWN.</li> <li>2. THE CONTRACTOR SHALL NOTIFY THE AIRPORT AT LEAST 5 DAYS IN ADVANCE OF WORK FOR NOTAM OF CONSTRUCTION ACTIVITY TO BE POSTED.</li> <li>3. THE CONTRACTOR WILL BE RESPONSIBLE TO PROVIDE ESCORTING OF ALL SUBCONTRACTORS FOR WORK DURING THIS PHASE. ESCORTS MUST PASS THE AIRFIELD DRIVERS TRAINING CLASS AND MUST HAVE AN ACTIVE AIRPORT BADGE WITH ESCORTING PRIVILEGES.</li> </ol>

X:\0119700\170869\02\TECH\DRAWINGS\G-086 CONSTRUCTION SAFETY PHASING PLAN WA 1.DWG  
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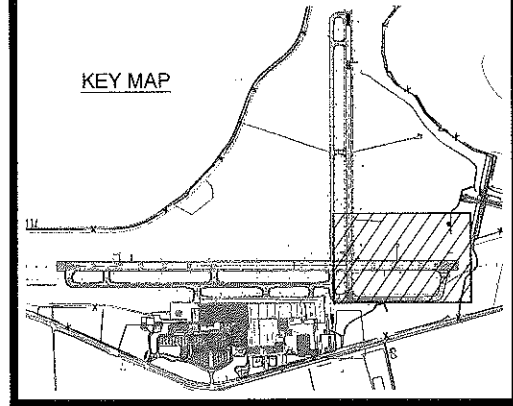
X:\01197001\70899.02\TECH\CAD\DRAWINGS\B-051 SOIL BORING PLAN.DWG  
1/6/2023 12:58:27 PM



**NOTES:**

1. SEE SHEETS B-052 THROUGH B-054 FOR THE BORING LOGS.
2. REFER TO GEOTECHNICAL REPORT IN PROJECT BOOK FOR ADDITIONAL INFORMATION.
3. RESULTS SHOWN REPRESENT ONLY THE LOCATIONS BORED. CONTRACTOR MAY ENCOUNTER VARYING CONDITIONS DURING CONSTRUCTION.

**KEY MAP**



**Mead & Hunt**  
Mead and Hunt, Inc.  
878 South Lake Drive  
Lexington, SC 29072  
phone: 803-996-2900  
meadhunt.com



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SHEET CONTENTS  
SOIL BORING PLAN

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**B-051**

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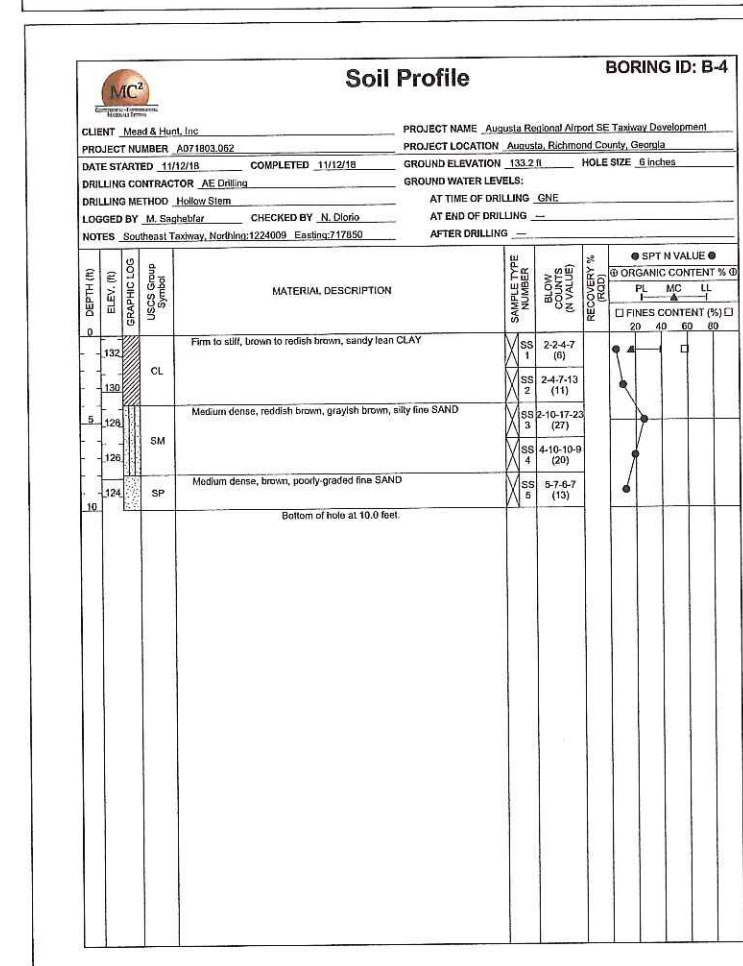
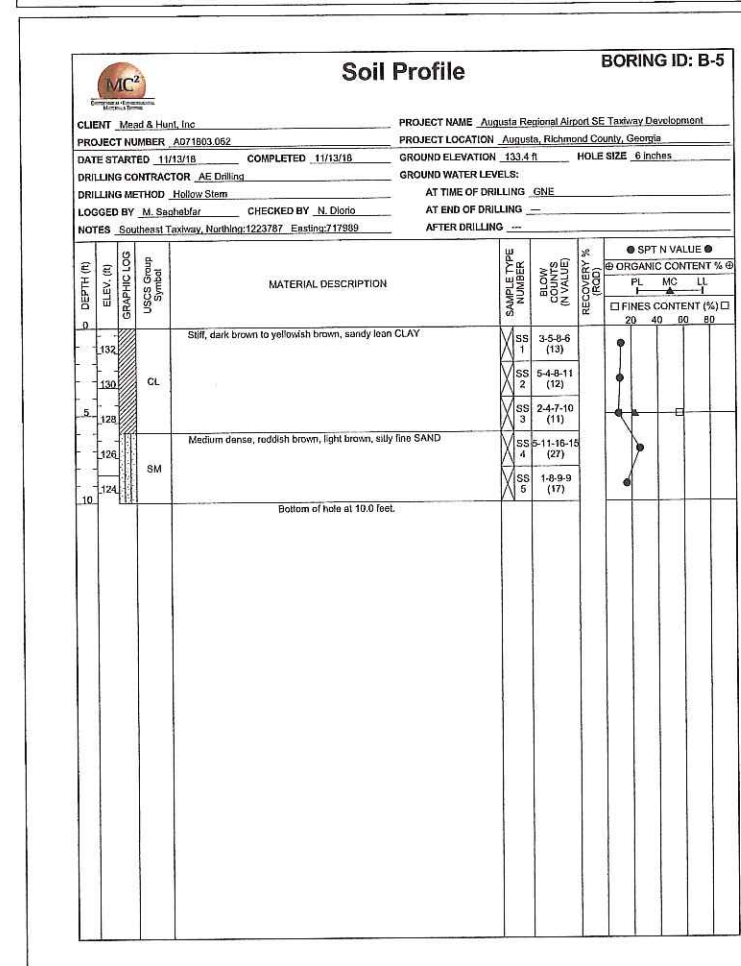
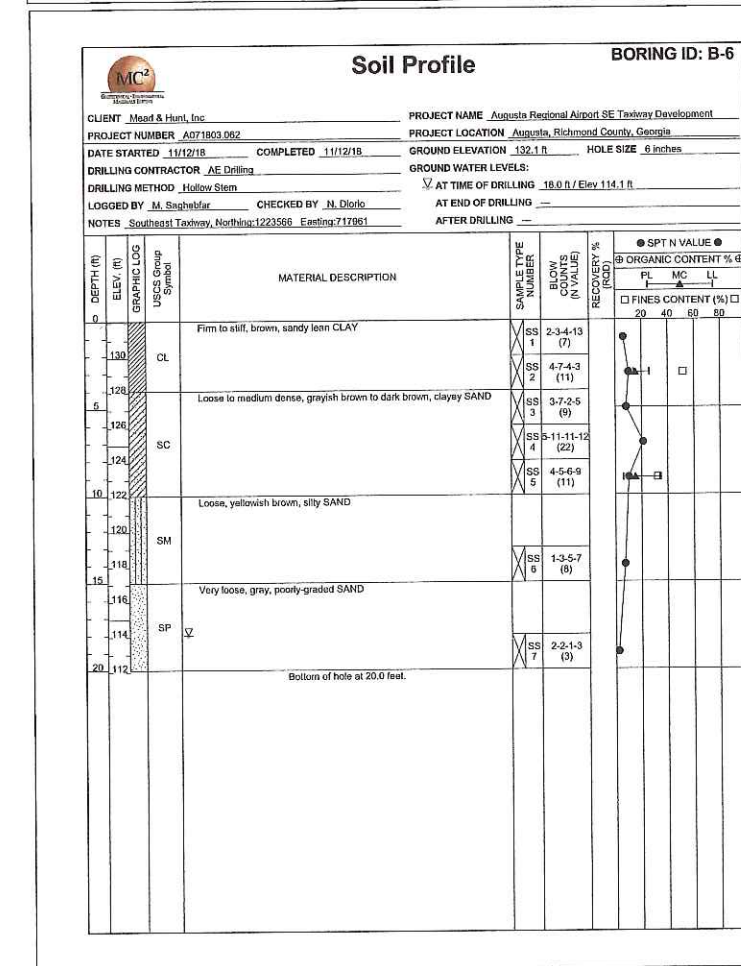
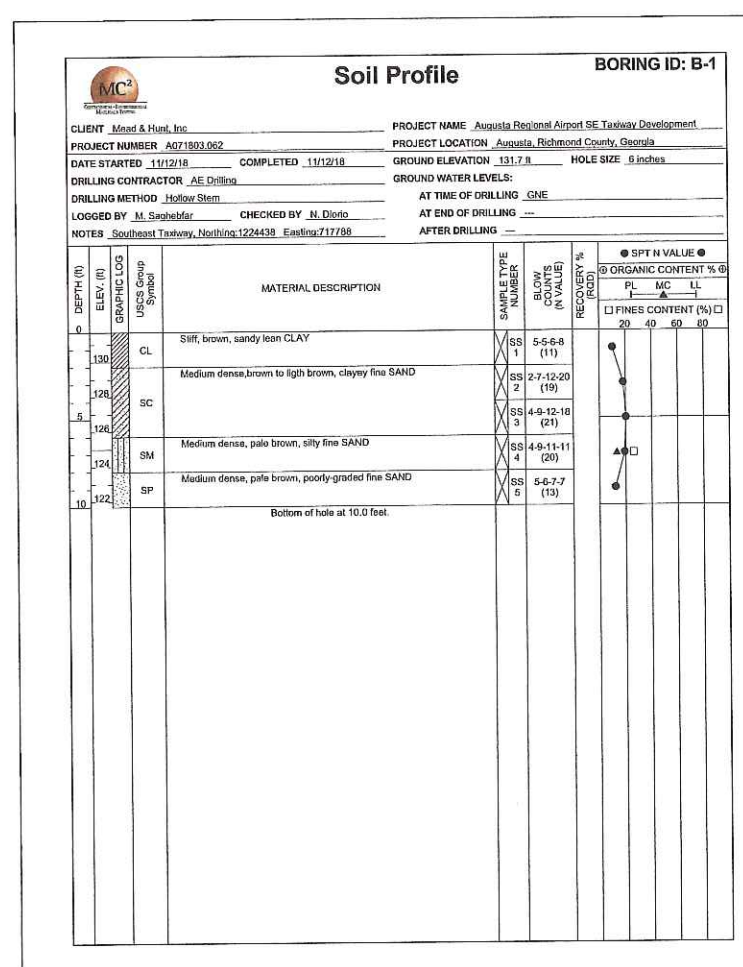
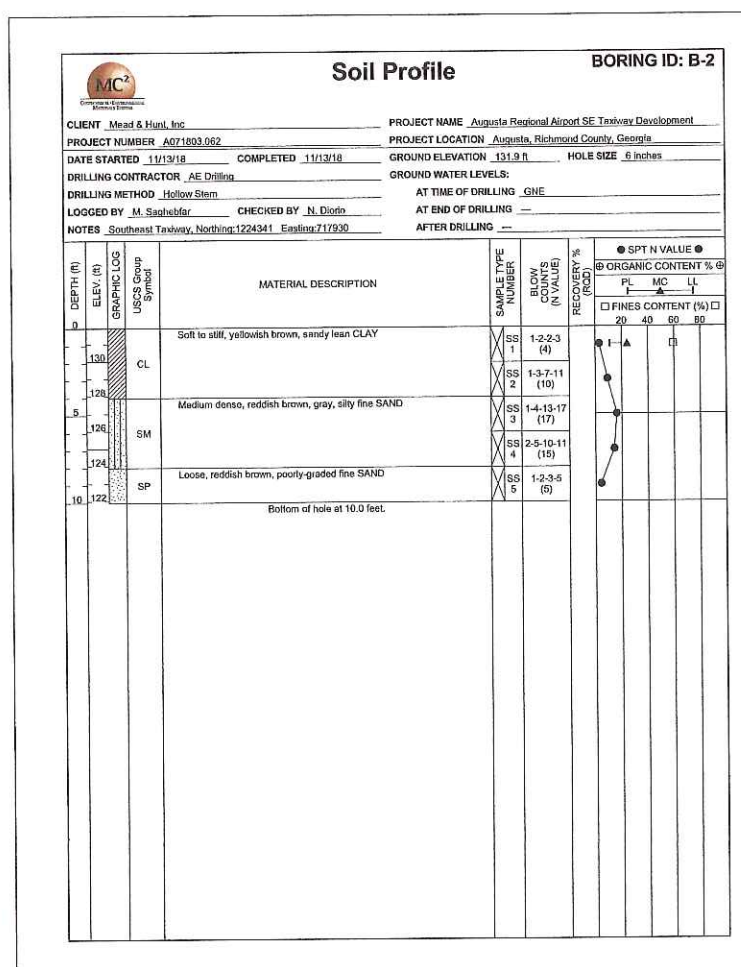
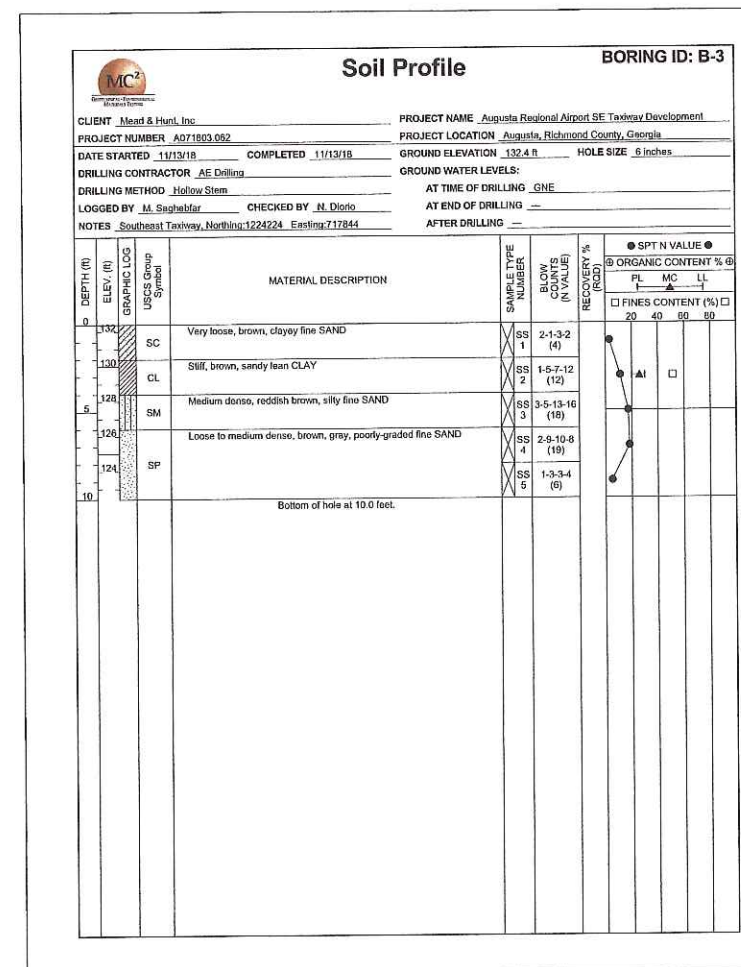
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SHEET CONTENTS  
 SOIL BORING LOG

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**B-052**

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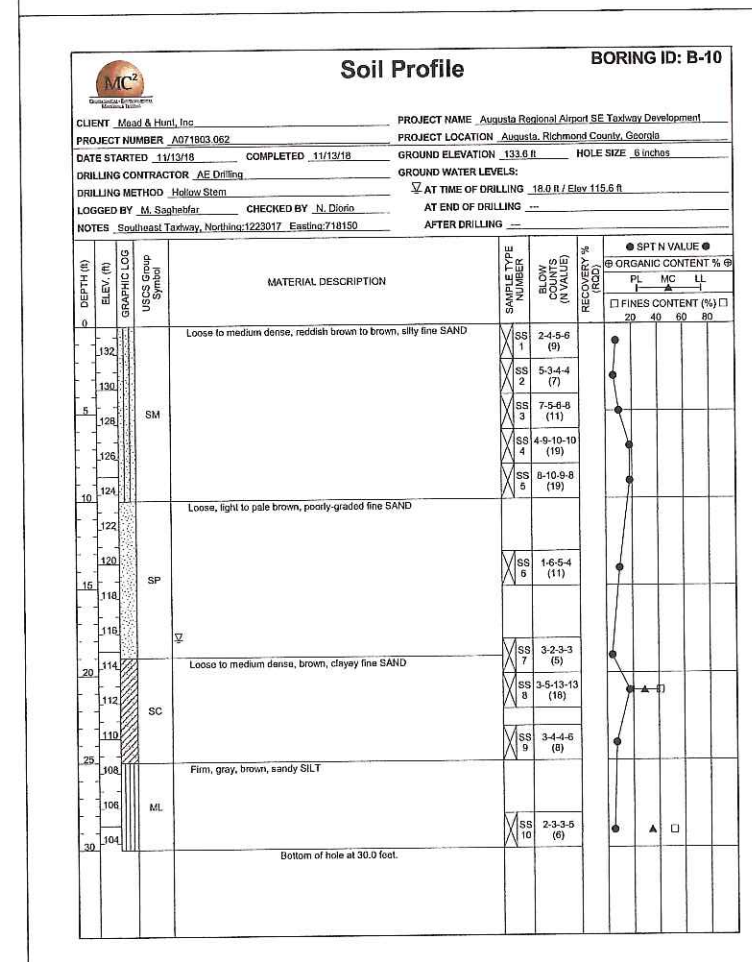
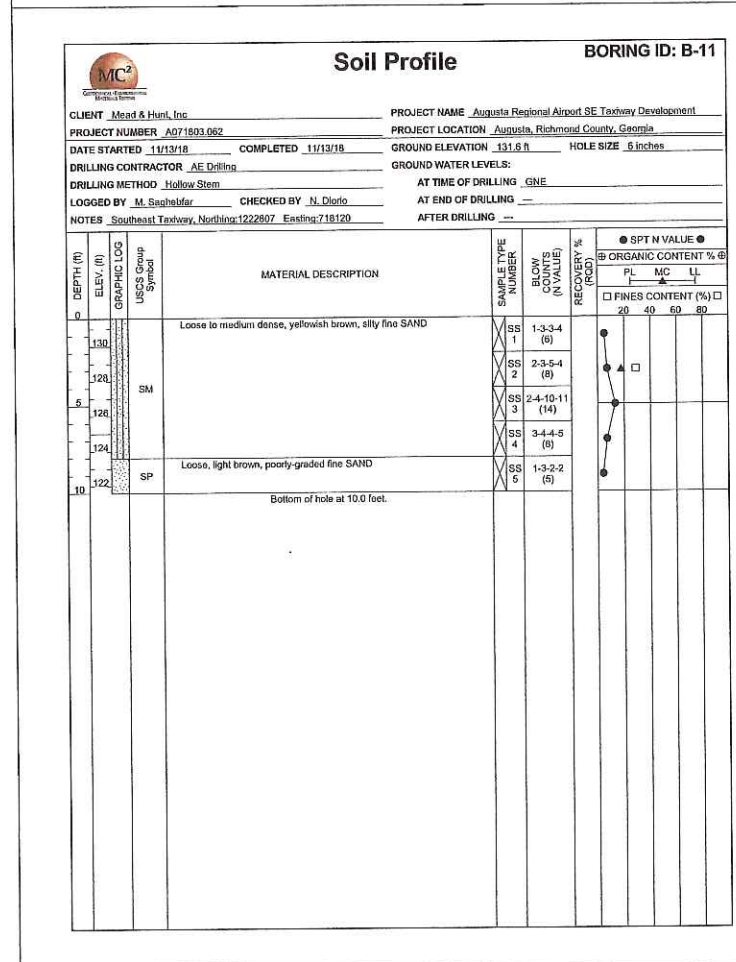
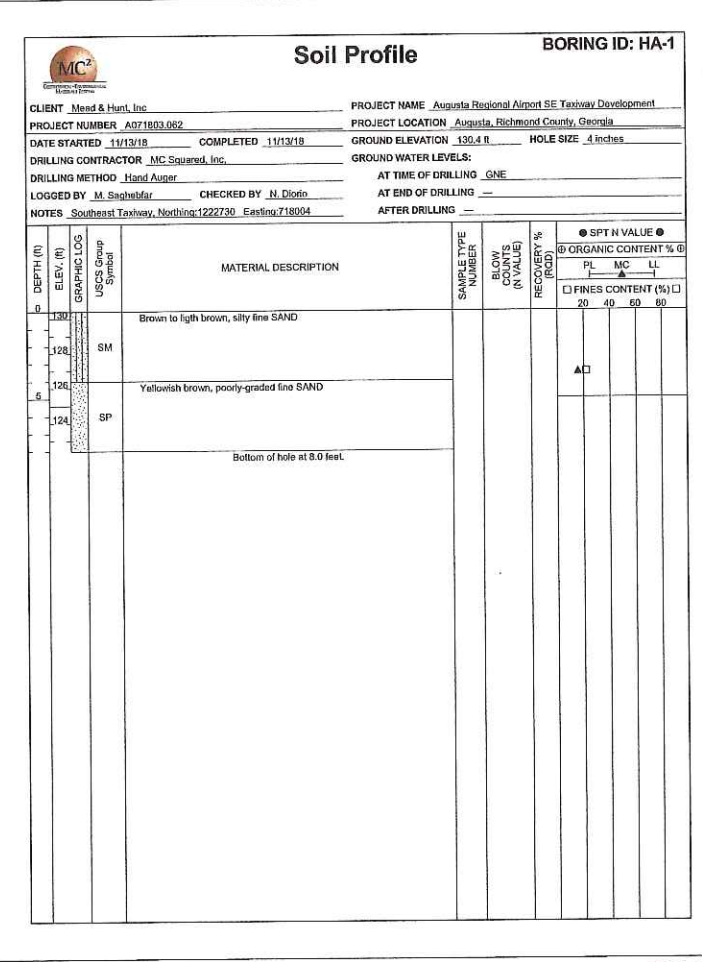
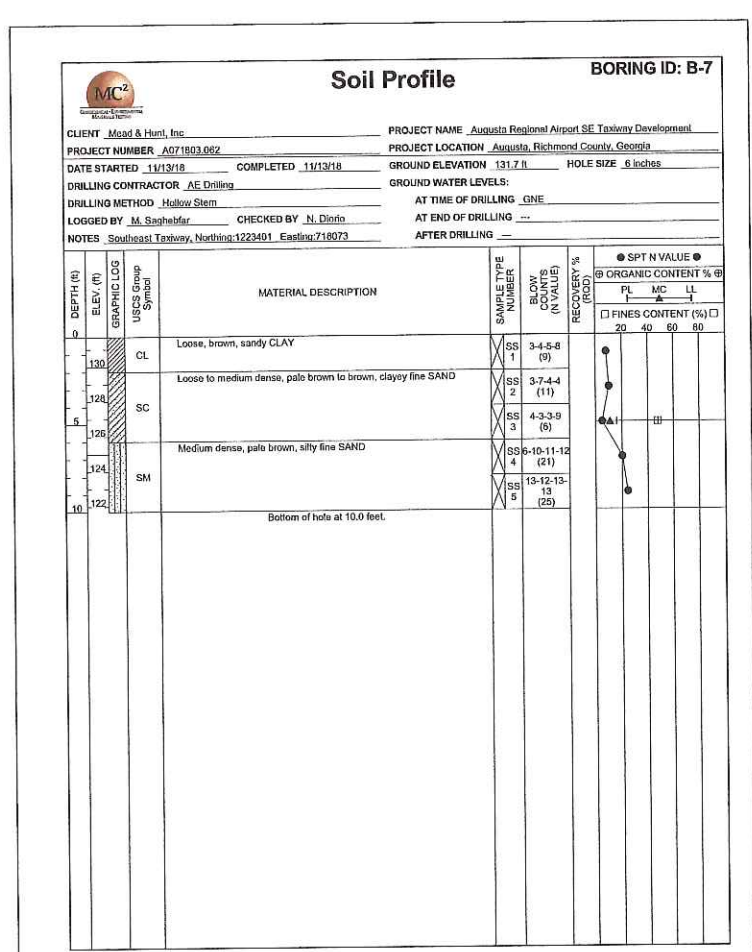
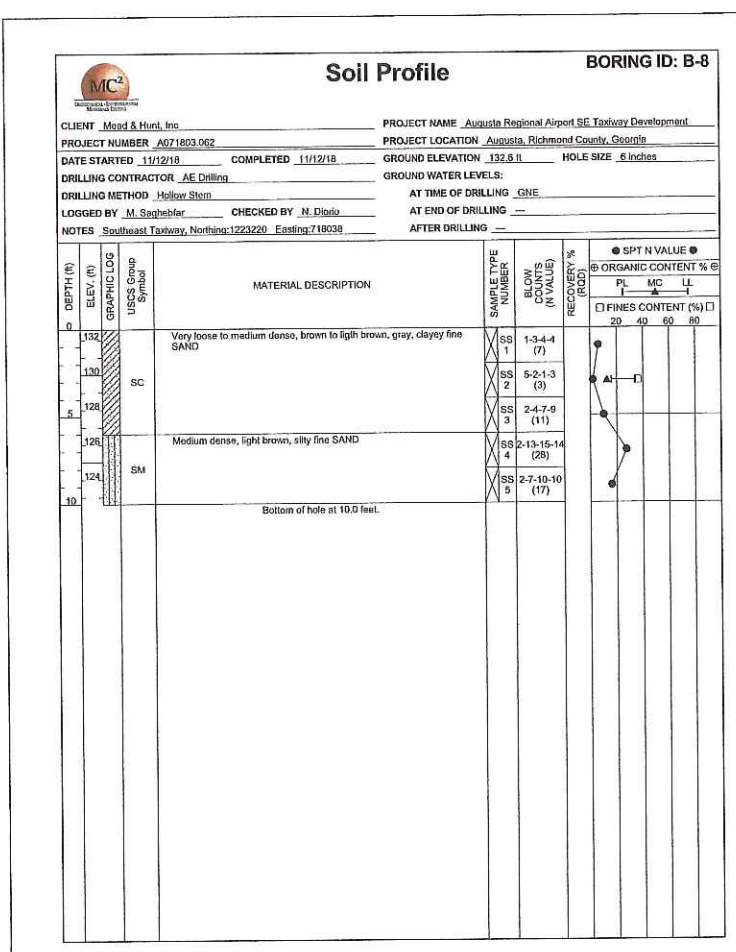
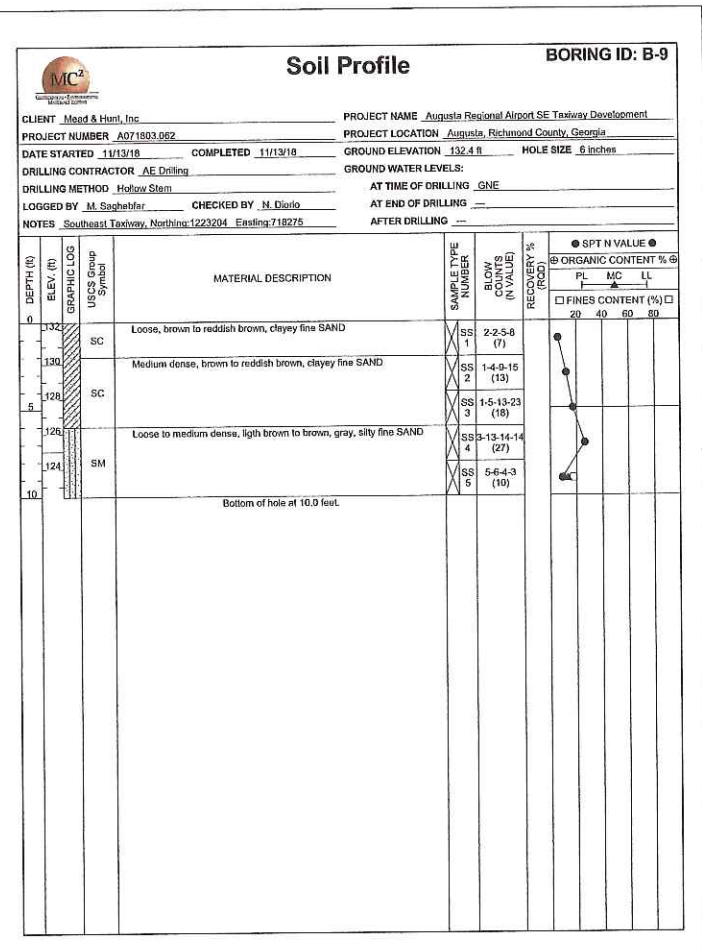
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SHEET CONTENTS  
 SOIL BORING LOG

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Soil Profile		BORING ID: HA-2	
CLIENT: Mead & Hunt, Inc.		PROJECT NAME: Augusta Regional Airport SE Taxiway Development	
PROJECT NUMBER: A071803.062		PROJECT LOCATION: Augusta, Richmond County, Georgia	
DATE STARTED: 11/13/18	COMPLETED: 11/13/18	GROUND ELEVATION:	HOLE SIZE: 4 inches
DRILLING CONTRACTOR: MC Squared, Inc.		GROUND WATER LEVELS:	
DRILLING METHOD: Hand Auger		AT TIME OF DRILLING: GNE	
LOGGED BY: M. Saghbafar		CHECKED BY: N. Diorio	
NOTES: Southeast Taxiway, Not Surveyed		AFTER DRILLING: -	

DEPTH (ft)	ELEV. (ft)	GRAPHIC LOG USCS Group Symbol	MATERIAL DESCRIPTION	SAMPLE TYPE NUMBER	SPT BLows (N VALUE)	RECOVERY % (RUD)	SPT N VALUE			ORGANIC CONTENT %		FINES CONTENT (%)	
							FL	MC	LL	PL	MC	LL	FL
0		SC	Reddish brown, clayey fine SAND light gray, dark brown, silty fine SAND										
5		SM											
Bottom of hole at 7.5 feet.													

**AUGUSTA REGIONAL AIRPORT  
CONSTRUCT TAXIWAY G**

1501 AVIATION WAY  
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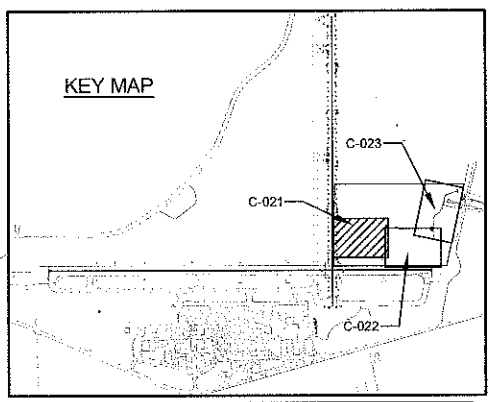
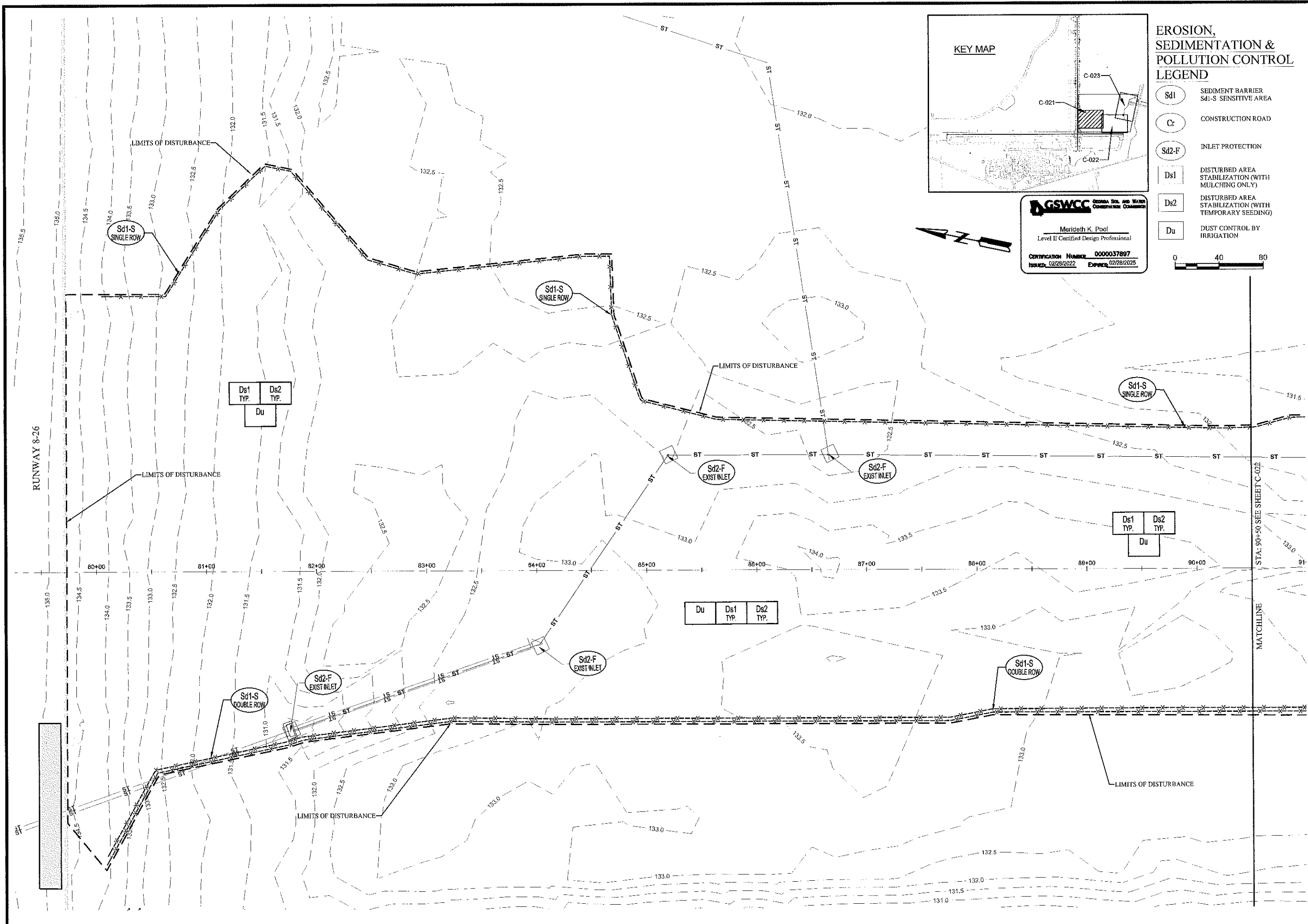
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SHEET CONTENTS  
 SOIL BORING LOG

SHEET NO.

**B-054**

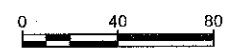
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**GSWCC** GEORGIA SOIL AND WATER CONSERVATION COMMISSION  
 Merideth K. Pool  
 Level II Certified Design Professional  
 CERTIFICATION NUMBER: 0000037897  
 ISSUED: 02/28/2022 EXPIRES: 02/28/2025

**EROSION, SEDIMENTATION & POLLUTION CONTROL LEGEND**

- Sd1** SEDIMENT BARRIER  
Sd1-S SENSITIVE AREA
- Cr** CONSTRUCTION ROAD
- Sd2-F** INLET PROTECTION
- Ds1** DISTURBED AREA STABILIZATION (WITH MULCHING ONLY)
- Ds2** DISTURBED AREA STABILIZATION (WITH TEMPORARY SEEDING)
- Du** DUST CONTROL BY IRRIGATION



**Mead & Hunt**  
 Mead and Hunt, Inc.  
 878 South Lake Drive  
 Lexington, SC 29072  
 phone: 803-996-2900  
 meadhunt.com

Augusta Regional Airport

**ISM**  
 Infrastructure Systems Management, LLC  
 1000 Peachtree Street, NE  
 Atlanta, GA 30309

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SHEET CONTENTS  
 ES & PC PLAN  
 PHASE I  
 STA 79+50 TO 90+50

SHEET NO. of  
**C-021**



**AUGUSTA REGIONAL AIRPORT  
CONSTRUCT TAXIWAY G**

1501 AVIATION WAY  
AUGUSTA, GA 30906-9620

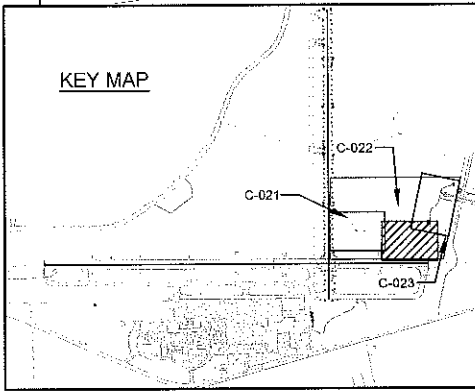
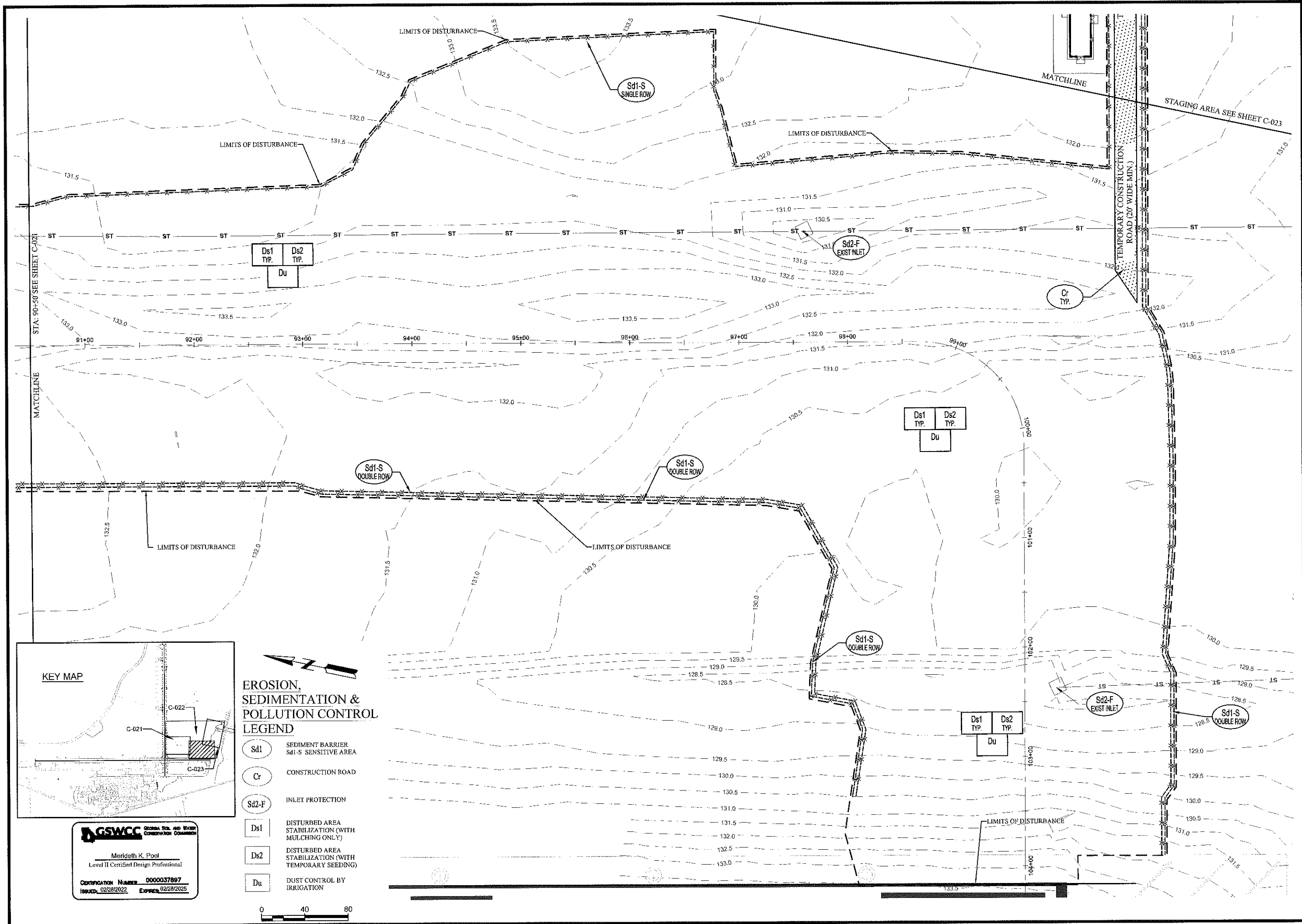
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SHEET CONTENTS  
ES & PC PLAN  
PHASE I  
STA 90+50 TO 104+25

SHEET NO. of  
**C-022**



**EROSION,  
SEDIMENTATION &  
POLLUTION CONTROL  
LEGEND**

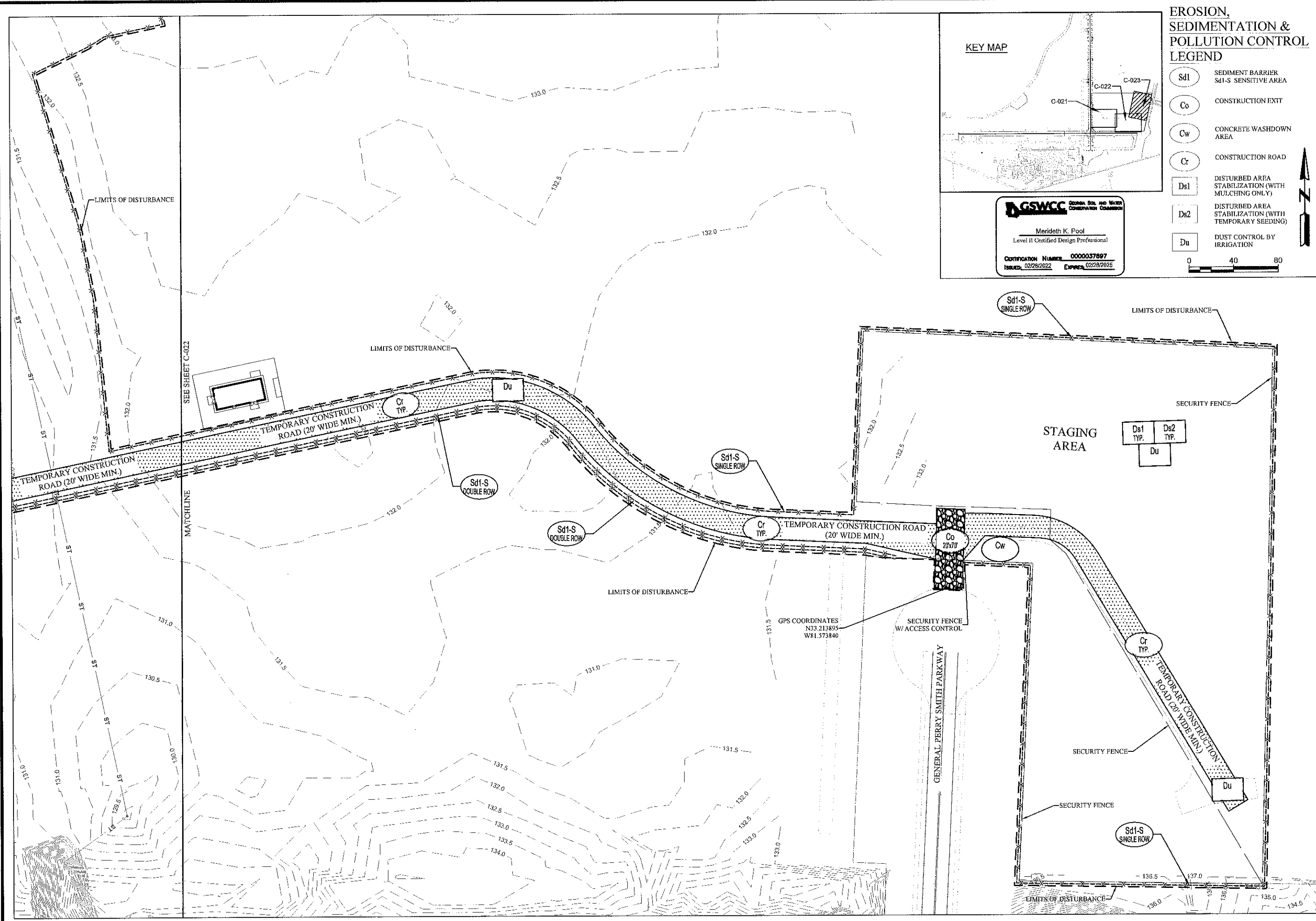
- Sd1 SEDIMENT BARRIER  
Sd1-S SENSITIVE AREA
- Cr CONSTRUCTION ROAD
- Sd2-F INLET PROTECTION
- Ds1 DISTURBED AREA STABILIZATION (WITH MULCHING ONLY)
- Ds2 DISTURBED AREA STABILIZATION (WITH TEMPORARY SEEDING)
- Du DUST CONTROL BY IRRIGATION

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Merideth K. Pool  
Level II Certified Design Professional

CERTIFICATION NUMBER 0000037897  
ISSUED 02/28/2022 EXPIRES 02/28/2025





**KEY MAP**

**EROSION, SEDIMENTATION & POLLUTION CONTROL LEGEND**

- SEDIMENT BARRIER Sd1-S SENSITIVE AREA
- CONSTRUCTION EXIT
- CONCRETE WASHDOWN AREA
- CONSTRUCTION ROAD
- DISTURBED AREA STABILIZATION (WITH MULCHING ONLY)
- DISTURBED AREA STABILIZATION (WITH TEMPORARY SEEDING)
- DUST CONTROL BY IRRIGATION

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0 40 80

**Mead & Hunt**

Mead and Hunt, Inc.  
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Lexington, SC 29072  
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CONSTRUCT TAXIWAY G**

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**GEORGIA REGISTERED PROFESSIONAL ENGINEER**  
No. 32385  
MERIDETH K. POOL

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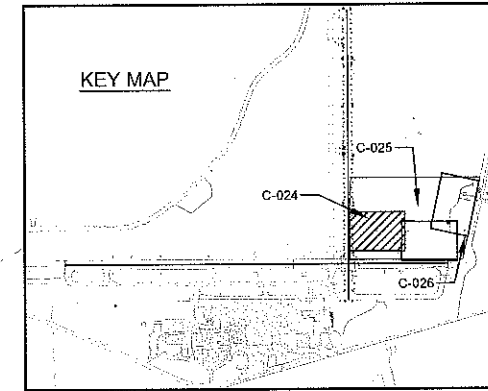
SHEET CONTENTS  
ES & PC PLAN  
PHASE I  
STAGING AREA

SHEET NO. of  
**C-023**



**EROSION, SEDIMENTATION & POLLUTION CONTROL LEGEND**

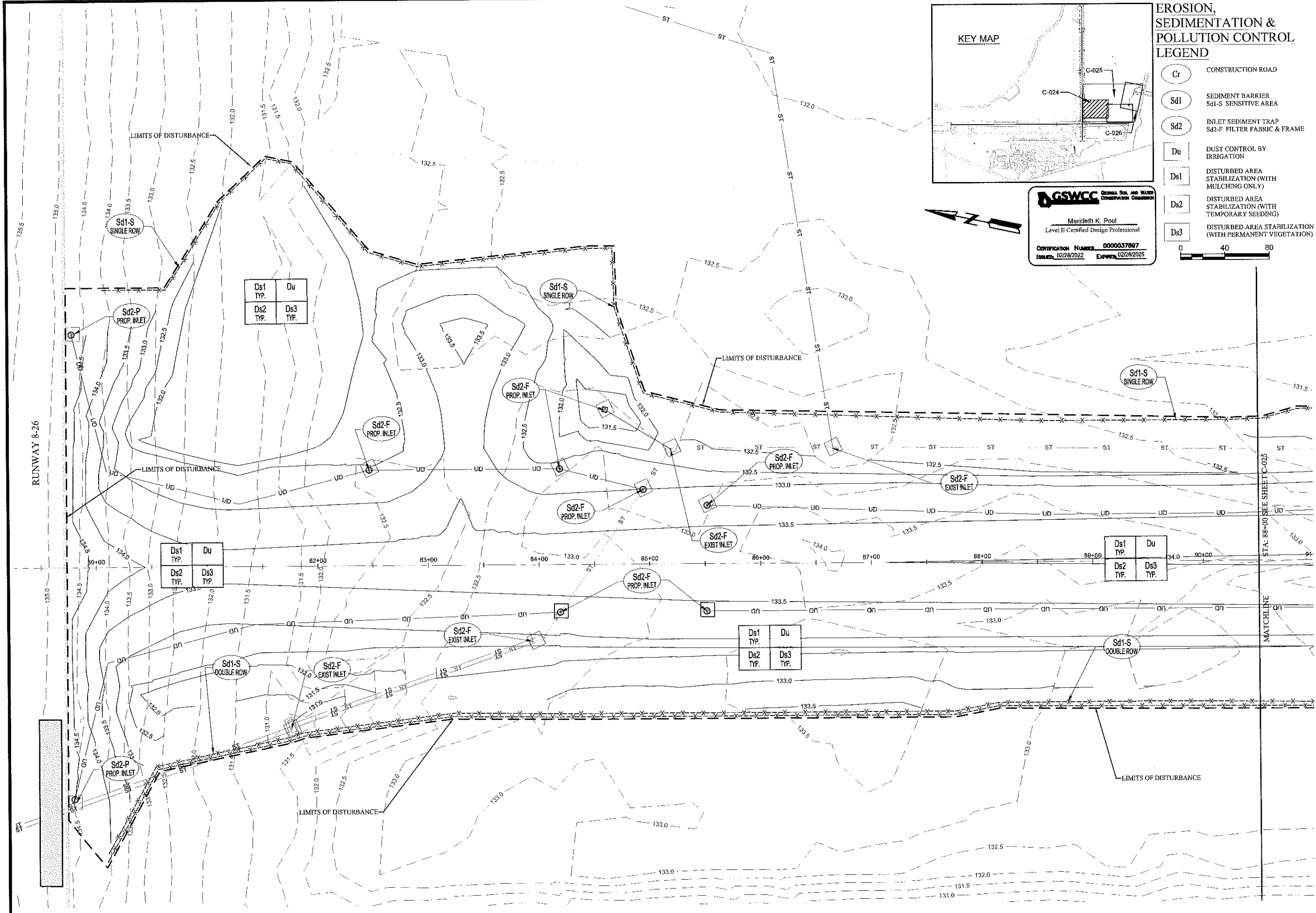
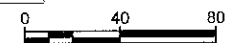
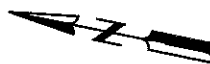
- CONSTRUCTION ROAD
- SEDIMENT BARRIER  
Sd1-S SENSITIVE AREA
- INLET SEDIMENT TRAP  
Sd2-F FILTER FABRIC & FRAME
- DUST CONTROL BY IRRIGATION
- DISTURBED AREA STABILIZATION (WITH MULCHING ONLY)
- DISTURBED AREA STABILIZATION (WITH TEMPORARY SEEDING)
- DISTURBED AREA STABILIZATION (WITH PERMANENT VEGETATION)



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SHEET CONTENTS  
ES & PC PLAN  
PHASE II  
STA 79+50 TO 90+50

SHEET NO. of

**C-024**



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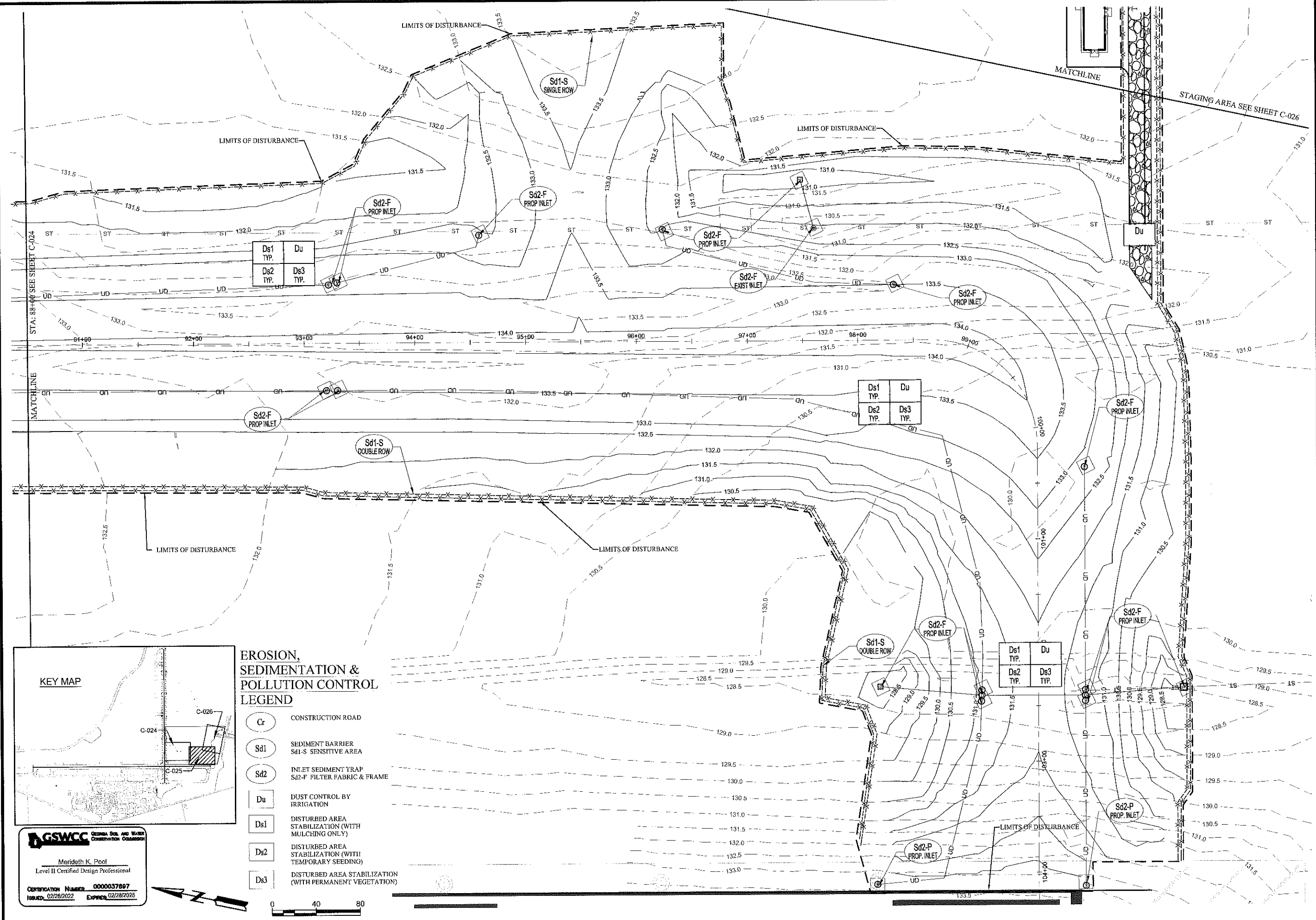


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SHEET CONTENTS  
 ES & PC PLAN  
 PHASE II  
 STA 90+50 TO 104+25

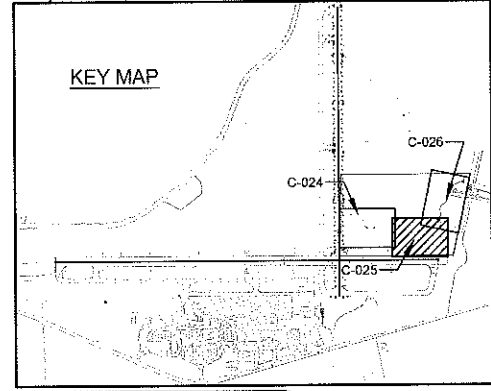
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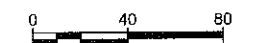
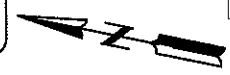
**EROSION,  
 SEDIMENTATION &  
 POLLUTION CONTROL  
 LEGEND**

- CONSTRUCTION ROAD
- SEDIMENT BARRIER  
Sd1-S SENSITIVE AREA
- INLET SEDIMENT TRAP  
Sd2-F FILTER FABRIC & FRAME
- DUST CONTROL BY IRRIGATION
- DISTURBED AREA STABILIZATION (WITH MULCHING ONLY)
- DISTURBED AREA STABILIZATION (WITH TEMPORARY SEEDING)
- DISTURBED AREA STABILIZATION (WITH PERMANENT VEGETATION)

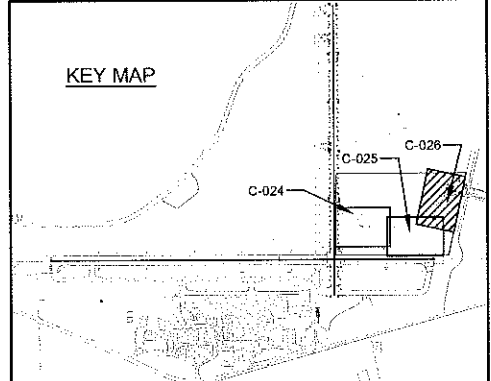
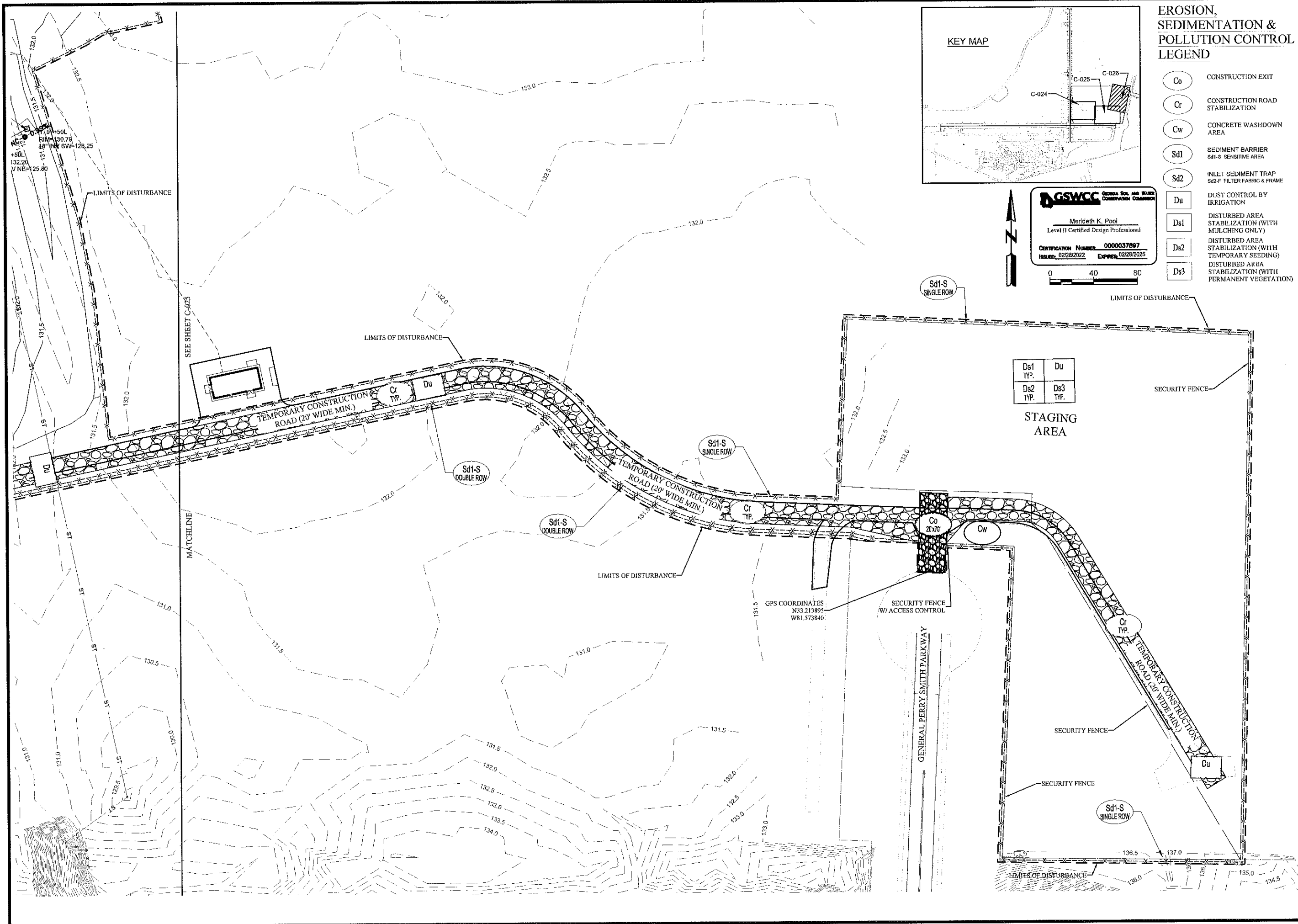


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Merideth K. Pool  
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 CERTIFICATION NUMBER: 0000037897  
 ISSUED: 02/28/2022 EXPIRES: 02/28/2025







**EROSION, SEDIMENTATION & POLLUTION CONTROL LEGEND**

- Co CONSTRUCTION EXIT
- Cr CONSTRUCTION ROAD STABILIZATION
- Cw CONCRETE WASHDOWN AREA
- Sd1 SEDIMENT BARRIER Sd1-S SENSITIVE AREA
- Sd2 INLET SEDIMENT TRAP Sd2-F FILTER FABRIC & FRAME
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- Ds1 DISTURBED AREA STABILIZATION (WITH MULCHING ONLY)
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- Ds3 DISTURBED AREA STABILIZATION (WITH PERMANENT VEGETATION)

**GSWCC** GEORGIA SOIL AND WATER CONSERVATION COMMISSION

Merideth K. Pool  
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Ds1 TYP.	Du
Ds2 TYP.	Ds3 TYP.

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Augusta Regional Airport

**ISM**  
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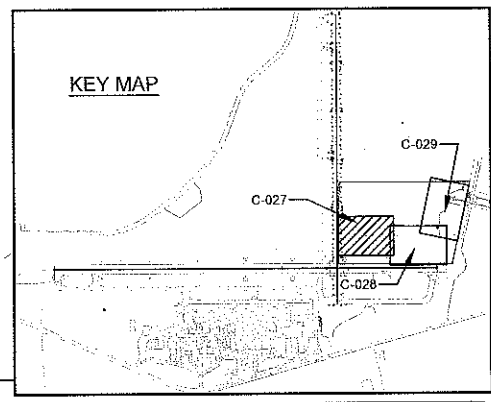
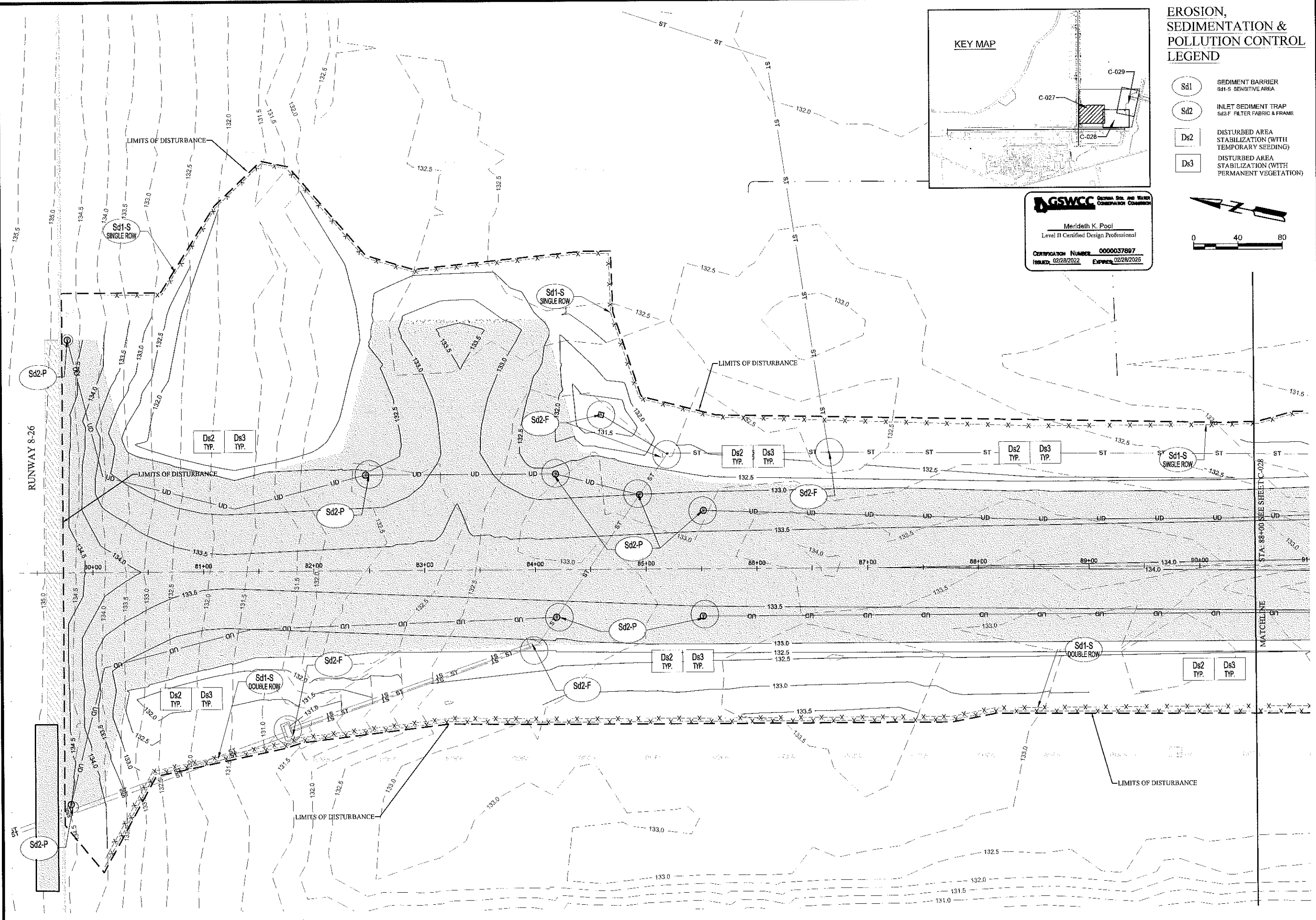
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GEORGIA REGISTERED  
No. 32385  
PROFESSIONAL ENGINEER  
MERIDETH K. POOL

MSH NO. 0119700-170869.01  
DATE: NOVEMBER 15, 2022  
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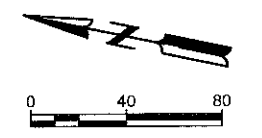
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**C-026**



**EROSION, SEDIMENTATION & POLLUTION CONTROL LEGEND**

- Sd1 SEDIMENT BARRIER  
Sd1-S SENSITIVE AREA
- Sd2 INLET SEDIMENT TRAP  
Sd2-F FILTER FABRIC & FRAME
- Ds2 DISTURBED AREA STABILIZATION (WITH TEMPORARY SEEDING)
- Ds3 DISTURBED AREA STABILIZATION (WITH PERMANENT VEGETATION)



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**Augusta Regional Airport**

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 PHASE III  
 STA 79+50 TO 90+50

SHEET NO. of  
**C-027**



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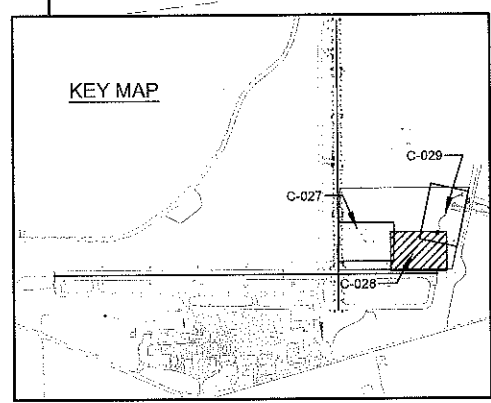
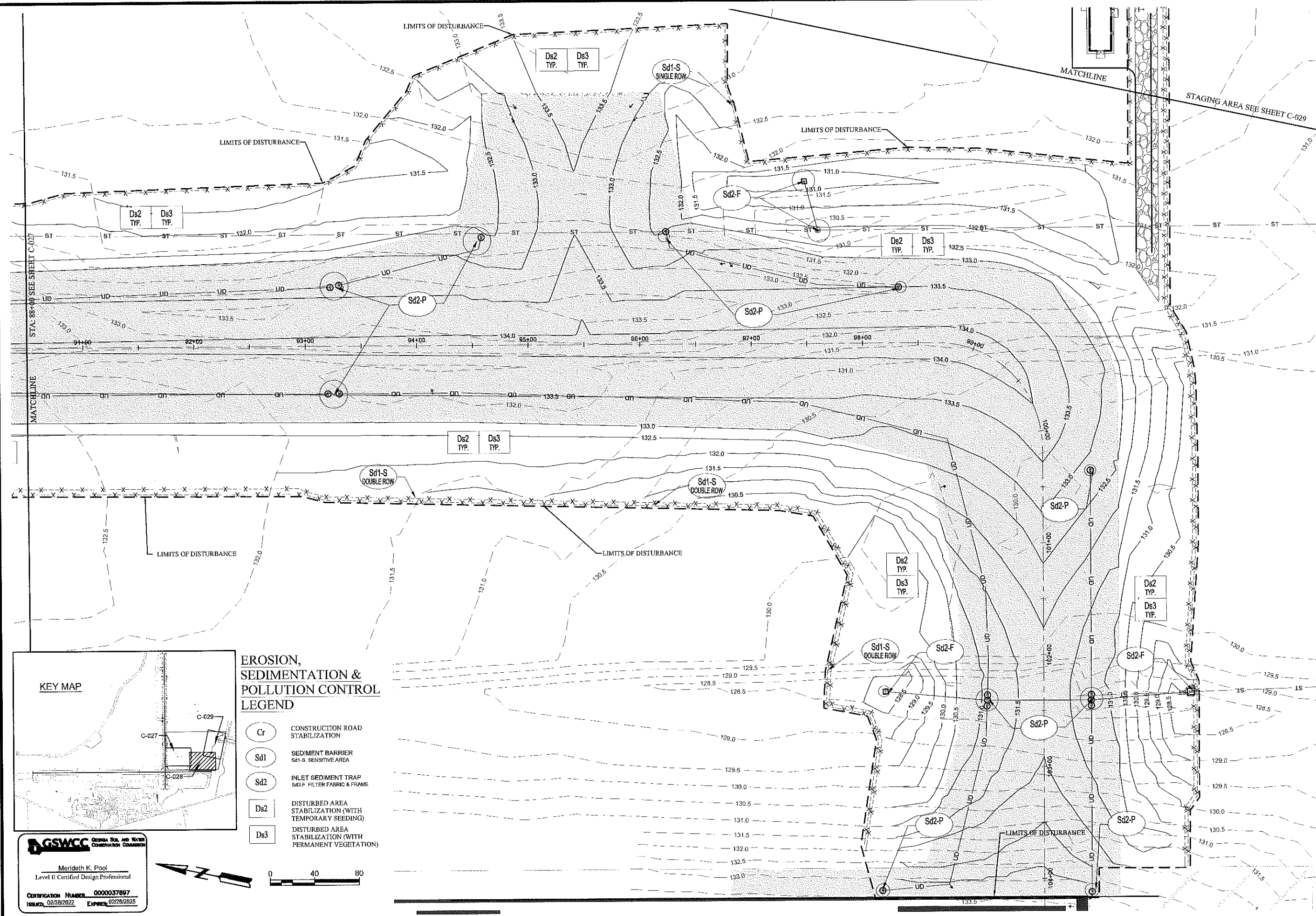


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STA 90+50 TO 104+25

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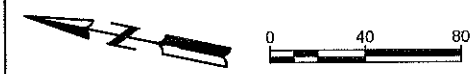
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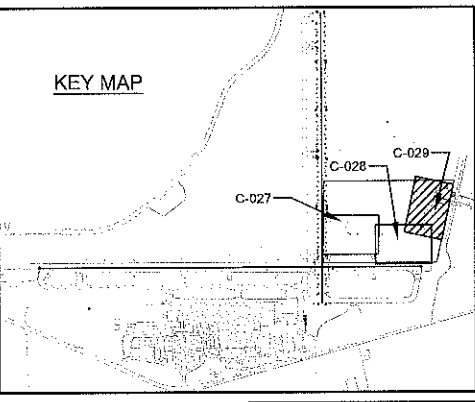
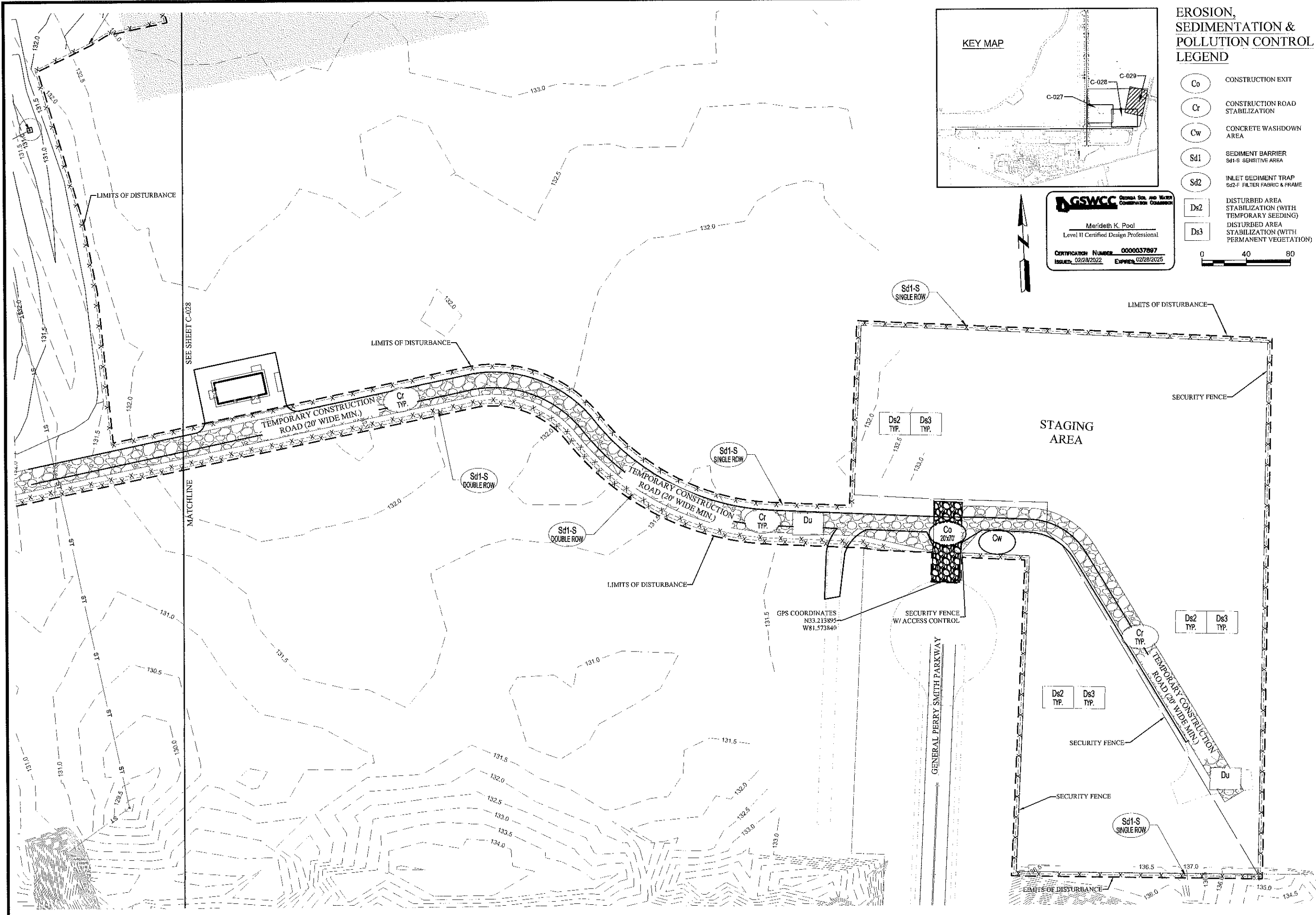


**EROSION,  
SEDIMENTATION &  
POLLUTION CONTROL  
LEGEND**

- Cr** CONSTRUCTION ROAD STABILIZATION
- Sd1** SEDIMENT BARRIER  
Sd1-S SENSITIVE AREA
- Sd2** INLET SEDIMENT TRAP  
Sd2-F FILTER FABRIC & FRAME
- Ds2** DISTURBED AREA STABILIZATION (WITH TEMPORARY SEEDING)
- Ds3** DISTURBED AREA STABILIZATION (WITH PERMANENT VEGETATION)

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SHEET NO. of  
**C-029**





**GEORGIA UNIFORM CODING SYSTEM FOR SOIL EROSION AND SEDIMENT CONTROL PRACTICES**

**STRUCTURAL PRACTICES**

CODE	PRACTICE	DETAIL	MAP SYMBOL	DESCRIPTION
Cs	CONSTRUCTION EXIT			A CRUSHED STONE PAD LOCATED AT THE CONSTRUCTION SITE EXIT TO PROVIDE A PLACE FOR REMOVING MUD FROM TIRES THEREBY PROTECTING PUBLIC STREETS.
Cr	ROAD CONSTRUCTION STABILIZATION			A TRAVELWAY CONSTRUCTED AS PART OF A CONSTRUCTION PLAN INCLUDING ACCESS ROADS, SUBDIVISION ROADS, PARKING AREAS AND OTHER ON-SITE VEHICLE TRANSPORTATION ROUTES.
Sd1	SEDIMENT BARRIER			A BARRIER TO PREVENT SEDIMENT FROM LEAVING THE CONSTRUCTION SITE. IT MAY BE SANDBAGS, BALES OF STRAW OR HAY, BRUSH, LOGS AND POLES, GRAVEL, OR A SEDIMENT FENCE. THE BARRIERS ARE USUALLY TEMPORARY AND INEXPENSIVE.
Sd2	SEDIMENT TRAP INLET			AN IMPOUNDING AREA CREATED BY EXCAVATING AROUND A STORM DRAIN DROP INLET. THE EXCAVATED AREA WILL BE FILLED AND STABILIZED ON COMPLETION OF CONSTRUCTION ACTIVITIES.
Tr	TREE PROTECTION			TO PROTECT DESIRABLE TREES FROM INJURY DURING CONSTRUCTION ACTIVITY.
Cw	CONCRETE WASTE MANAGEMENT			PREVENT OR REDUCE THE DISCHARGE OF POLLUTANTS TO STORMWATER FROM CONCRETE WASTE BY CONDUCTING WASHOUT OFFSITE, PERFORMING ONSITE WASHOUT IN A DESIGNATED AREA AND TRAINING EMPLOYEES AND SUBCONTRACTORS.
Sr	TEMPORARY STREAM CROSSING			A TEMPORARY BRIDGE OR CULVERT-TYPE STRUCTURE PROTECTING A STREAM OR WATERCOURSE FROM DAMAGE BY CROSSING CONSTRUCTION EQUIPMENT.

**SITE MAINTENANCE NOTES:**

- THE DESIGN PROFESSIONAL WHO PREPARED E.S. & P.C. PLAN SHALL INSPECT THE INSTALLATION OF THE INITIAL SEDIMENT STORAGE REQUIREMENTS AND PERIMETER CONTROL BMP'S WITHIN 7 DAYS AFTER INSTALLATION.
- ANY AMENDMENTS AND/OR REVISIONS TO THE E.S. & P.C. PLANS WHICH WILL HAVE A SIGNIFICANT EFFECT ON BMP'S WITH HYDRAULIC COMPONENTS MUST BE CERTIFIED BY THE DESIGN PROFESSIONAL.
- WASTE MATERIALS SHALL NOT BE DISCHARGED TO WATERS OF THE STATE, EXCEPT AS AUTHORIZED BY A SECTION 404 PERMIT.
- THE ESCAPE OF SEDIMENT FROM THE SITE SHALL BE PREVENTED BY THE INSTALLATION OF EROSION SEDIMENT CONTROL MEASURES AND PRACTICES PRIOR TO LAND-DISTURBING ACTIVITIES.
- EROSION CONTROL MEASURES WILL BE MAINTAINED AT ALL TIMES. IF FULL IMPLEMENTATION OF THE APPROVED PLAN DOES NOT PROVIDE FOR EFFECTIVE EROSION CONTROL, ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE IMPLEMENTED TO CONTROL OR TREAT SEDIMENT SOURCE.
- ANY DISTURBED AREA LEFT EXPOSED FOR A PERIOD GREATER THAN 14 DAYS SHALL BE STABILIZED WITH MULCH OR TEMPORARY SEEDING.
- DEVELOPERS AND/OR CONTRACTORS ARE RESPONSIBLE TO REMOVE OR CLEAN OUT ANY SILT, DIRT, MUD OR ANY OTHER TYPE OF DEBRIS THAT COMES OFF THEIR SITE AND FINDS ITS WAY INTO A PRIVATE POND, ONTO PRIVATE PROPERTY, INTO A COUNTY OWNED POND OR COUNTY OWNED PROPERTY TO INCLUDE RIGHTS-OF-WAY.
- THE CONTRACTOR SHALL COMPLY WITH THE "GEORGIA MANUAL FOR ON-SITE SEWAGE MANAGEMENT SYSTEMS" FOR WASTE DISPOSAL, SANITARY SEWER AND/OR SEPTIC TANK INCLUDING ALL TEMPORARY MEASURES DURING CONSTRUCTION AND AFTER CONSTRUCTION ACTIVITIES HAVE BEEN COMPLETED.

**FUGITIVE DUST CONTROL NOTES** GA. RULE [391-3-1-.02] - (N) - FUGITIVE DUST CONTROL

- ALL PERSONS RESPONSIBLE FOR ANY OPERATION, PROCESS, HANDLING, TRANSPORTATION OR STORAGE FACILITY WHICH MAY RESULT IN FUGITIVE DUST SHALL TAKE ALL REASONABLE PRECAUTIONS TO PREVENT SUCH DUST FROM BECOMING AIRBORNE. SOME REASONABLE PRECAUTIONS WHICH COULD BE TAKEN TO PREVENT DUST FROM BECOMING AIRBORNE INCLUDE, BUT ARE NOT LIMITED TO, THE FOLLOWING:
  - USE, WHERE POSSIBLE, OF WATER OR CHEMICALS FOR CONTROL OF DUST IN THE DEMOLITION OF EXISTING BUILDINGS OR STRUCTURES, CONSTRUCTION OPERATIONS, THE GRADING OF ROADS OR THE CLEARING OF LAND;
  - APPLICATION OF ASPHALT, OIL, WATER, OR SUITABLE CHEMICALS ON DIRT ROADS, MATERIALS, STOCKPILES, AND OTHER SURFACES WHICH CAN GIVE RISE TO AIRBORNE DUSTS;
  - INSTALLATION AND USE OF HOODS, FANS, AND FABRIC FILTERS TO ENCLOSE AND VENT THE HANDLING OF DUSTY MATERIALS. ADEQUATE CONTAINMENT METHODS CAN BE EMPLOYED DURING SANDBLASTING OR OTHER SIMILAR OPERATIONS;
  - COVERING, AT ALL TIMES WHEN IN MOTION, OPEN BODIED TRUCKS, TRANSPORTING MATERIALS LIKELY TO GIVE RISE TO AIRBORNE DUSTS;
  - THE PROMPT REMOVAL OF EARTH OR OTHER MATERIAL FROM PAVED STREETS ONTO WHICH EARTH OR OTHER MATERIAL HAS BEEN DEPOSITED.
- THE PERCENT OPACITY FROM ANY FUGITIVE DUST SOURCE LISTED IN PARAGRAPH 2(X)(1). ABOVE SHALL NOT EQUAL OR EXCEED 20 PERCENT.

**VEGETATIVE PRACTICES**

CODE	PRACTICE	DETAIL	MAP SYMBOL	DESCRIPTION
Ds1	DISTURBED AREA STABILIZATION (WITH MULCHING ONLY)			ESTABLISHING TEMPORARY PROTECTION FOR DISTURBED AREAS WHERE SEEDING MAY NOT HAVE A SUITABLE GROWING SEASON TO PRODUCE AN EROSION RETARDING COVER.
Ds2	DISTURBED AREA STABILIZATION (WITH TEMPORARY SEEDING)			ESTABLISHING A TEMPORARY VEGETATIVE COVER WITH FAST GROWING SEEDINGS ON DISTURBED AREAS.
Ds3	DISTURBED AREA STABILIZATION (WITH PERMANENT VEGETATION)			ESTABLISHING PERMANENT VEGETATIVE COVER SUCH AS TREES, SHRUBS, VINES, GRASSES, SOD, OR LEGUMES ON DISTURBED AREAS.
Du	DUST CONTROL ON DISTURBED AREAS			CONTROLLING SURFACE AND AIR MOVEMENT DUST ON CONSTRUCTION SITES, ROADWAYS AND SIMILAR SITES.

**STANDARDS & SPECIFICATIONS:**  
ALL DESIGNS WILL CONFORM TO AND ALL WORK WILL BE PERFORMED IN ACCORDANCE WITH THE STANDARDS AND SPECIFICATIONS OF THE "GREEN BOOK" ALSO KNOWN AS THE "MANUAL FOR EROSION AND SEDIMENT CONTROL IN GEORGIA", LATEST EDITION.

**SPILL RESPONSE PLAN**

THE GOAL OF THE SPILL RESPONSE PLAN IS TO REDUCE SAFETY, HEALTH, AND ENVIRONMENTAL RISKS ASSOCIATED WITH A HAZARDOUS SUBSTANCE INCIDENT. IN THE EVENT OF A SPILL, THE FOLLOWING ACTIONS SHOULD BE IMPLEMENTED:

- SECURE AND EVACUATE THE AREA - KEEP UNAUTHORIZED PERSONS OUT OF THE AREA.
  - REPORT THE SPILL -
  - ALL SPILLS > 5 GALLONS MUST BE IMMEDIATELY REPORTED TO THE AIRPORT POC.
  - SPILLS THAT ARE < 5 GALLONS MUST BE REPORTED IF THE SPILL ENTERS A STORM DRAIN, CREEK, LAKE, OR OTHER BODY OF WATER, OR CANNOT BE SAFELY CONTAINED AND CLEANED UP BY ORGANIZATION PERSONNEL.
  - PROVIDE ANY PERTINENT INFORMATION, INCLUDING:
    - SUBSTANCE SPILLED
    - LOCATION OF SPILL
    - NATURE AND EXTENT OF INJURIES
    - EXTENT TO WHICH SPILL TRAVELED
    - ESTIMATED AMOUNT SPILLED
    - TIME SPILL OCCURRED
- PROTECT YOURSELF - EXTINGUISH SMOKING MATERIAL AND IGNITION SOURCES. IDENTIFY THE SUBSTANCE SPILLED AND OBTAIN APPROPRIATE PERSONAL PROTECTIVE EQUIPMENT, SUCH AS:
  - PROTECTIVE GOGGLES
  - PROTECTIVE APRON
  - RUBBER OVERBOOTS
  - COMPATIBLE RUBBER GLOVES
  - RESPIRATORS
- STOP THE FLOW - STOP OR SLOW FLOW OF HAZARDOUS SUBSTANCE IF IT CAN BE DONE SAFELY.
  - PLUG OR PATCH PUNCTURED CONTAINER(S)
  - UPRIGHT OVERTURNED OR TIPPED CONTAINER(S)
  - CLOSE APPROPRIATE VALVE(S)
- CONTAIN THE SPILL - THE SPILLED SUBSTANCE SHOULD BE CONTAINED WITHIN THE IMMEDIATE AREA. PREVENT FLOW TO DRAINS, DRAINAGE DITCHES, AND SEWER SYSTEMS IF IT CAN BE DONE SAFELY.
  - PLACE NONREACTIVE ABSORBENT MATERIAL SUCH AS SAND, EARTH, STRAW, VERMICULITE, ABSORBENT PILLOWS OR BOOMS ON THE SPILL.
  - BLOCK THE SPILL FROM ENTERING STORM DRAINS OR SEWERS BY CONSTRUCTING A DIKE AROUND ALL POINTS OF ENTRY.
  - IF THE SPILL IS ON THE GROUND, CLEAN IT UP IMMEDIATELY BY DIGGING UP THE CONTAMINATED SOIL, PLACING IT IN PROPER CONTAINERS, AND DISPOSING OF IT PROPERLY.

**E.S.P.C. NOTES**

- THIS PROJECT DOES NOT LIE WITHIN THE ZONE AE (100-YR FLOOD PLAIN) PER FIRM NO. 13245C PANEL 0230H DATED NOVEMBER 15, 2019.
- THERE ARE NO STATE WATERS PRESENT ON OR WITHIN 200 FEET OF THE PROJECT LIMITS WITH NO BUFFER REQUIREMENT. (CHECKLIST ITEMS No. 16, 40 & 41)
- WETLANDS ARE NOT PRESENT ON OR WITHIN 200 FEET OF THE PROJECT LIMITS. (CHECKLIST ITEM No. 41)
- ALL CRITICAL AREAS SUCH AS STREAM CROSSING, WETLANDS, CEMETERY, ETC. ARE NOTED IN THE PLANS WITHIN THE PROJECT LIMIT.
- EROSION AND TREE PROTECTION MEASURES SHALL BE INSTALLED PRIOR TO ANY CONSTRUCTION ACTIVITY AND MAINTAINED UNTIL PERMANENT GROUND COVER IS ESTABLISHED WITHIN THE DISTURBED AREA.
- "THE ESCAPE OF SEDIMENT FROM THE SITE SHALL BE PREVENTED BY THE INSTALLATION OF EROSION AND SEDIMENT CONTROL MEASURES AND PRACTICES PRIOR TO LAND-DISTURBING ACTIVITIES". (CHECKLIST ITEM No. 19)
- "EROSION CONTROL MEASURES WILL BE MAINTAINED AT ALL TIMES. IF FULL IMPLEMENTATION OF THE APPROVED PLAN DOES NOT PROVIDE FOR EFFECTIVE EROSION CONTROL, ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE IMPLEMENTED TO CONTROL OR TREAT THE SEDIMENT SOURCE". (CHECKLIST ITEM No. 20)
- "ANY DISTURBED AREA LEFT EXPOSED FOR A PERIOD GREATER THAN 14 DAYS SHALL BE STABILIZED WITH MULCH OR TEMPORARY SEEDING". (CHECKLIST ITEM No. 21)
- THERE ARE NO ENDANGERED SPECIES AND ARCHEOLOGICAL SITES ON OR WITHIN 200 FEET OF THE PROJECT AREA.
- THIS CONSTRUCTION ACTIVITY DOES NOT DISCHARGE DIRECTLY TO OR IS NOT WITHIN 1 LINEAR MILE OF A BIOTA IMPAIRED STREAM SEGMENT PER 2016 INTEGRATED 305(b)/303(d) LIST OF STREAMS. (CHECKLIST ITEMS No. 22 & 23)
- DEVELOPERS AND/OR CONTRACTORS ARE RESPONSIBLE TO REMOVE OR CLEAN OUT ANY SILT, DIRT, MUD OR ANY OTHER TYPE OF DEBRIS THAT COMES OFF THEIR SITE AND FINDS ITS WAY INTO A PRIVATE POND OR A COUNTY OWNED POND. THEY ARE RESPONSIBLE TO REMOVE ANY OF THE ABOVE MENTIONED ITEMS THAT COME OFF THEIR SITE ONTO PRIVATE OR COUNTY OWNED PROPERTIES TO INCLUDE RIGHTS OF WAY.
- NON-EXEMPT ACTIVITIES SHALL NOT BE CONDUCTED WITHIN THE 25 OR 50-FOOT UNDISTURBED STREAM BUFFERS AS MEASURED FROM THE POINT OF WRESTED VEGETATION OR WITHIN 25-FEET OF THE COSTAL MARSHLAND BUFFER AS MEASURED FROM THE JURISDICTIONAL DETERMINATION LINE WITHOUT FIRST ACQUIRING THE NECESSARY VARIANCES AND PERMITS. (CHECKLIST ITEM No. 15)
- AMENDMENTS/REVISIONS TO THE EROSION, SEDIMENTATION & POLLUTION CONTROL PLAN WHICH HAVE A SIGNIFICANT EFFECT ON BMP'S WITH A HYDRAULIC COMPONENT MUST BE CERTIFIED BY THE DESIGN PROFESSIONAL. (CHECKLIST ITEM No. 17)
- WASTE MATERIALS SHALL NOT BE DISCHARGED TO WATERS OF THE STATE, EXCEPT AS AUTHORIZED BY A SECTION 404 PERMIT. (CHECKLIST ITEM No. 18)
- THE CONTRACTOR SHALL COMPLY WITH THE "GEORGIA MANUAL FOR ON-SITE SEWAGE MANAGEMENT SYSTEMS" FOR WASTE DISPOSAL, SANITARY SEWER AND/OR SEPTIC TANK INCLUDING ALL TEMPORARY MEASURES DURING CONSTRUCTION AND AFTER CONSTRUCTION ACTIVITIES HAVE BEEN COMPLETED.
- FINAL GRADING WILL CONSIST OF SPREADING TOPSOIL (4" MIN. TO FINISHED GRADES INDICATED) ON ALL DISTURBED AREAS NOT PAVED.
- ALL DISTURBED AREAS WILL BE GRASSED AND STABILIZED IMMEDIATELY AFTER GRADING ACCORDING TO THE GRASSING, FERTILIZATION AND MULCH SPECIFICATION. STABILIZATION MEASURES SHALL BE INITIATED ON DISTURBED AREAS WITHIN 14 DAYS OF CESSATION OF CONSTRUCTION ACTIVITY IN THAT AREA, OR WHEN CONSTRUCTION ACTIVITY WILL NOT RESUME WITHIN TWENTY-ONE (21) DAYS OR WITHIN FOURTEEN (14) DAYS OF COMPLETION OF CONSTRUCTION ACTIVITY.
- APPENDIX B RATIONALE FOR NTU: STORM WATER SAMPLING WILL BE DONE UTILIZING THE OUTFALL SAMPLING METHOD; THEREFORE, THE ALLOWABLE NTU VALUES FOR THE STORM WATER OUTFALLS ARE BASED ON THE TABLE LOCATED IN APPENDIX B OF THE PERMIT. 22.25± ACRES OF THE SITE WILL BE DISTURBED BY CONSTRUCTION. THE TOTAL SURFACE WATER DRAINAGE AREA IS APPROXIMATELY 1.67 SQUARE MILES. SAMPLE POINTS ARE LOCATED IN A WARM WATER FISHERY; THEREFORE, THE NTU VALUE FOR OUTFALLS IS 50. SEE DRAWINGS FOR SAMPLE POINT LOCATION(S). (CHECKLIST ITEM No. 33)
- THE CONTRACTOR IS RESPONSIBLE TO REMOVE OR CLEAN UP ANY AND ALL SEDIMENT THAT LEAVES THE PROJECT SITE.
- PROJECT DISTURBED AREA DOES NOT EXCEED 50 ACRES. (CHECKLIST ITEM No. 3)
- DURING CONSTRUCTION, PORT-O-LET FACILITIES WILL BE UTILIZED FOR WASTE DISPOSAL.
- THE DESIGN PROFESSIONAL WHO PREPARED THE ES&PC PLAN IS TO INSPECT THE INSTALLATION OF THE INITIAL SEDIMENT STORAGE REQUIREMENTS AND PERIMETER CONTROL BMP'S WITHIN 7 DAYS AFTER INSTALLATION. (CHECKLIST ITEM No. 14)
- THE DESIGN PROFESSIONAL WHO PREPARED THE DESIGN OF THE LOW IMPACT DEVELOPMENT (LID) FEATURES SHALL CONDUCT A POST-CONSTRUCTION SITE VISIT TO ASSESS THE INSTALLATION OF THE AS-BUILT FEATURES.

**LAND GRADING FOR MINIMIZING EROSION**

- ONLY DISTURB, CLEAR, OR GRADE AREAS NECESSARY FOR CONSTRUCTION. FLAG OR OTHERWISE DELINEATE AREAS NOT TO BE DISTURBED. EXCLUDE VEHICLES AND CONSTRUCTION EQUIPMENT FROM THESE AREAS TO PRESERVE
- ALL GRADED OR DISTURBED AREAS INCLUDING SLOPES SHALL BE PROTECTED DURING CLEARING AND CONSTRUCTION IN ACCORDANCE WITH THE APPROVED EROSION AND SEDIMENT CONTROL PLAN UNTIL THEY ARE PERMANENTLY STABILIZED.
- ALL SEDIMENT CONTROL MEASURES SHALL BE CONSTRUCTED AND MAINTAINED IN ACCORDANCE WITH THE APPROVED EROSION AND SEDIMENT CONTROL PLAN AND ACCORDING TO THE STANDARDS AND SPECIFICATIONS FOR THE APPROPRIATE EROSION CONTROL PRACTICES.
- IF TOPSOIL IS REQUIRED FOR THE ESTABLISHMENT OF VEGETATION, IT SHALL BE STOCKPILED IN THE AMOUNT NECESSARY TO COMPLETE FINISHED GRADING AND PROTECTED FROM EROSION DURING THE INTERIM.
- AREAS TO BE FILLED SHALL BE CLEARED, GRUBBED TO REMOVE TREES, VEGETATION, ROOTS AND OTHER OBJECTIONABLE MATERIAL, AND STRIPPED OF TOPSOIL.
- AREAS TO RECEIVE TOPSOIL SHALL BE SCARIFIED TO A MINIMUM DEPTH OF 3 INCHES (76 MM) PRIOR TO PLACEMENT OF TOPSOIL.
- ALL FILLS SHALL BE COMPACTED AS REQUIRED BY BUILDING STANDARDS TO REDUCE EROSION, SLIPPAGE, SETTLEMENT, SUBSIDENCE AND OTHER RELATED PROBLEMS. FILL INTENDED TO SUPPORT BUILDINGS, STRUCTURES, CONDUITS, ETC., SHALL BE COMPACTED IN ACCORDANCE WITH LOCAL REQUIREMENTS OR CODES.
- THE OUTER FACE OF THE FILL SLOPE SHOULD BE ALLOWED TO STAY LOOSE, NOT ROLLED, COMPACTED, OR BLADED SMOOTH. A BULLDOZER MAY RUN UP AND DOWN THE FILL SLOPE SO THE DOZER TREADS (CLEAT TRACKS) CREATE GROOVES PERPENDICULAR TO THE SLOPE. IF THE SOIL IS NOT TOO MOIST, EXCESSIVE COMPACTION WILL NOT OCCUR.
- ALL FILL SHALL BE PLACED AND COMPACTED IN LAYERS NOT TO EXCEED 8 INCHES (0.2 M) PER LIFT.
- USE SLOPE BREAKS, SUCH AS DIVERSIONS, BENCHES, OR CONTOUR FURROWS AS APPROPRIATE, TO REDUCE THE LENGTH OF CUT-AND-FILL SLOPES TO LIMIT SHEET AND RILL EROSION AND PREVENT GULLY EROSION.
- THE FINISHED CUT-AND-FILL SLOPES, WHICH ARE TO BE VEGETATED WITH GRASS AND LEGUMES, SHOULD NOT BE STEEPER THAN 2:1.
- SLOPES TO BE MAINTAINED BY TRACTOR OR OTHER EQUIPMENT SHOULD NOT BE STEEPER THAN 3:1.
- SLOPES IN EXCESS OF 2:1 MAY REQUIRE HYDROSEEDING, HYDROMULCHING, TACTIFYING, AND/OR "PUNCHING-IN" STRAW, BIOENGINEERING TECHNIQUES, OR RETAINING WALLS.
- ROUGHEN THE SURFACE OF ALL SLOPES DURING THE CONSTRUCTION OPERATION TO RETAIN WATER, INCREASE INFILTRATION, AND FACILITATE VEGETATION ESTABLISHMENT.
- SEEPS OR SPRINGS ENCOUNTERED DURING CONSTRUCTION SHALL BE HANDLED IN ACCORDANCE WITH APPROVED METHODS.
- STABILIZE ALL GRADED AREAS WITH VEGETATION, CRUSHED STONE, RIPRAP, OR OTHER GROUND COVER AS SOON AS GRADING IS COMPLETED OR IF WORK IS INTERRUPTED FOR 21 WORKING DAYS OR MORE.
- USE MULCH TO STABILIZE AREAS TEMPORARILY WHERE FINAL GRADING MUST BE DELAYED.
- STOCKPILES, BORROW AREAS AND SPOIL AREAS SHALL BE SHOWN ON THE PLANS AND SHALL BE STABILIZED TO PREVENT EROSION AND SEDIMENTATION.

**SITE CONSTRUCTION POLLUTION NOTES**

- CONSTRUCTION WASTES INCLUDES DEMOLITION RUBBLE, PACKAGING MATERIALS, SCRAP BUILDING SUPPLIES, ETC. CONTRACTOR SHALL SELECT A DESIGNATED WASTE COLLECTION AREA AND PROVIDE IDS FOR WASTE CONTAINERS. CONSTRUCTION WASTES SHALL BE REMOVED ON A CONSISTENT SCHEDULE.
- PESTICIDES SHALL BE STORED IN A DRY, COVERED AREA. CONTRACTOR SHALL PROVIDE CURBS, DIKES, OR BERMS SURROUNDING STORAGE AREAS. APPLICATION RATES SHALL BE FOLLOWED STRICTLY.
- FERTILIZER AND DETERGENT APPLICATIONS ARE TO BE LIMITED TO THE MINIMUM NEEDED. CONTRACTOR SHALL NOT DISCHARGE WASH WATER INTO THE STORM WATER SYSTEM.
- PETROLEUM PRODUCTS INCLUDE OIL, GASOLINE, LUBRICANTS, AND ASPHALTIC SUBSTANCES AND SHALL BE STORED IN COVERED AREAS PROTECTED BY DIKES. CONTRACTOR SHALL HAVE EQUIPMENT TO CONTAIN AND CLEAN UP PETROLEUM SPILLS IN FUEL STORAGE AREAS OR MAINTENANCE AND FUELING VEHICLES.
- SANITARY AND SEPTIC WASTES INCLUDE ON-SITE SANITARY FACILITIES. LOCATION OF THESE FACILITIES SHALL BE OUT OF HIGH FLOW AREAS. REGULAR SERVICING BY A QUALIFIED DOMESTIC WASTE HAULER IS REQUIRED. PROPOSED SANITARY SEWER WILL TIE INTO AN EXISTING COUNTY SYSTEM.
- CONTRACTOR SHALL STORE AND HANDLE MATERIALS TO PREVENT SPILLS. IF A SPILL OCCURS, CONTACT TO STORM WATER SHALL BE MINIMIZED.
- PRIOR TO MOBILIZATION, THE CONTRACTOR SHALL SUBMIT PROPOSED LOCATIONS OF ANY POTENTIAL POLLUTANT TO THE CONTRACTING OFFICER FOR APPROVAL.
- WASTE MATERIALS SHALL NOT BE DISCHARGED TO WATERS OF THE STATE, EXCEPT AS AUTHORIZED BY A SECTION 404 PERMIT.
- BUILDING MATERIALS, BUILDING PRODUCTS, CONSTRUCTION WASTES, TRASH, LANDSCAPE MATERIALS, FERTILIZERS, PESTICIDES, HERBICIDES, DETERGENTS, SANITARY WASTE, AND OTHER MATERIALS SHALL BE COVERED WITH PLASTIC SHEETING OR TEMPORARY ROOFS IN ORDER TO MINIMIZE EXPOSURE TO PRECIPITATION AND TO STORMWATER.



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DESIGNED BY: MKP  
DRAWN BY: MKP  
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SHEET CONTENTS  
EROSION,  
SEDIMENTATION &  
POLLUTION CONTROL  
NOTES & LEGEND

SHEET NO. of

C-031

**GSWCC** GEORGIA SOIL AND WATER CONSERVATION COMMISSION

Merideth K. Pool  
Level II Certified Design Professional

CERTIFICATION NUMBER: 0000037697  
ISSUED: 02/28/2022 EXPIRES: 02/28/2025



## NARRATIVE OF LAND DISTURBING ACTIVITY

THE PROPOSED CONSTRUCTION PROJECT IS LOCATED ON THE AUGUSTA REGIONAL AIRPORT CAMPUS IN AUGUSTA, GEORGIA. THE PROJECT IS LOCATED WEST OF RUNWAY 17/35. THE CURRENT SITE CONDITIONS ARE MOSTLY OPEN GRASS AND HARDSCAPES ASSOCIATED WITH THE EXISTING AIRPORT FACILITIES. THE PROJECT INCLUDES CONSTRUCTION OF AN ADDITIONAL TAXIWAY FOR RUNWAY 17/35 AS WELL AS CONSTRUCTION OF WATER QUALITY STORMWATER FEATURES.

TOPOGRAPHIC INFORMATION: EXISTING TOPOGRAPHIC VARIES FROM 0% TO 2% SLOPES. THE PROJECT SITE DRAINS MOSTLY FROM THE SOUTHWEST TO THE NORTHEAST TO A TRIBUTARY OF TWIGGS DEAD RIVER.

VEGETATION: THE PROJECT SITE IS MOSTLY COVERED IN OPEN GRASSES.

PRACTICES THAT WILL BE USED TO REDUCE THE POLLUTANTS IN STORM WATER DISCHARGES DURING CONSTRUCTION INCLUDE, BUT NOT LIMITED TO (CHECKLIST ITEMS 26 & 35):

### INITIAL PHASE: PERIMETER CONTROL

- SILT FENCE - TO PREVENT SEDIMENT CARRIED BY SHEET FLOW FROM LEAVING THE SITE AND ENTERING NATURAL DRAINAGE WAYS OR STORM DRAINAGE SYSTEM BY SLOWING STORM WATER RUNOFF AND CAUSING DEPOSITION OF SEDIMENT AT THE STRUCTURE.
- TEMPORARY SEDIMENT TRAP - TO PREVENT SEDIMENT FLOW FROM LEAVING THE SITE AND ALLOW SEDIMENT TO SETTLE OUT OF STORM WATER PRIOR TO DISCHARGE OFF SITE.
- CONSTRUCTION EXIT - TO REDUCE AND ELIMINATE TRANSPORT OF MUD FROM CONSTRUCTION AREA INTO ROADWAYS BY MOTOR VEHICLES OR BY RUNOFF.
- DUST CONTROL - TO PREVENT SURFACE AND AIR MOVEMENT OF DUST FROM EXPOSED SOIL SURFACE AND TO REDUCE PRESENCE OF AIRBORNE SUBSTANCES WHICH MAY BE HARMFUL OR INJURIOUS TO HUMAN HEALTH WELFARE, OR SAFETY, OR TO ANIMALS OR PLANT LIFE.
- MULCHING & TEMPORARY SEEDING AS INDICATED ON THE PLANS.
- CONSTRUCTION ROAD TO PROVIDE A FIXED TRAVEL ROUTE FOR CONSTRUCTION TRAFFIC AND REDUCE EROSION AND SUBSEQUENT REGRADING OF PERMANENT ROADBEDS BETWEEN TIME OF INITIAL GRADING AND FINAL STABILIZATION.

### INTERMEDIATE PHASE: INTERMEDIATE CONSTRUCTION CONTROL

- SILT FENCE - TO PREVENT SEDIMENT CARRIED BY SHEET FLOW FROM LEAVING THE SITE AND ENTERING NATURAL DRAINAGE WAYS OR STORM DRAINAGE SYSTEM BY SLOWING STORM WATER RUNOFF AND CAUSING DEPOSITION OF SEDIMENT AT THE STRUCTURE.
- TEMPORARY SEDIMENT TRAP - TO PREVENT SEDIMENT FLOW FROM LEAVING THE SITE AND ENTERING STORM DRAINAGE SYSTEMS PRIOR TO PERMANENT STABILIZATION OF THE DISTURBED AREA.
- CONSTRUCTION EXIT - TO REDUCE AND ELIMINATE TRANSPORT OF MUD FROM CONSTRUCTION AREA INTO PUBLIC RIGHTS-OF-WAY BY MOTOR VEHICLES OR BY RUNOFF.
- DUST CONTROL - TO PREVENT SURFACE AND AIR MOVEMENT OF DUST FROM EXPOSED SOIL SURFACE AND TO REDUCE PRESENCE OF AIRBORNE SUBSTANCES WHICH MAY BE HARMFUL OR INJURIOUS TO HUMAN HEALTH WELFARE, OR SAFETY, OR TO ANIMALS OR PLANT LIFE.
- EROSION CONTROL MATTING ON SLOPES 3:1 OR GREATER.
- TEMPORARY GRASSING TO REDUCE RUNOFF AND SEDIMENT DAMAGE OF DOWNSTREAM RESOURCES.

### FINAL PHASE: SITE STABILIZATION

- PERMANENT GRASSING AND/OR VEGETATION AS SPECIFIED ON THE LANDSCAPE PLAN
- EROSION CONTROL MATTING ON SLOPES 3:1 OR GREATER.

MEASURES THAT WILL BE INSTALLED DURING THE CONSTRUCTION PROCESS TO CONTROL POLLUTANTS IN STORM WATER THAT WILL OCCUR AFTER CONSTRUCTION OPERATIONS WILL INCLUDE (CHECKLIST ITEM 26):

- PERMANENT GRASSING - TO REDUCE RUNOFF AND SEDIMENT DRAINAGE, PROTECT SOIL SURFACE FROM EROSION AND TO IMPROVE INFILTRATION AND AERATION AS WELL AS ORGANIC MATTER FOR PERMANENT PLANTINGS.
- EROSION CONTROL MATTING ON SLOPES 3:1 OR GREATER.

## PROJECT DATA

### OWNER/PRIMARY PERMITTEE:

AUGUSTA REGIONAL AIRPORT  
CONTACT PERSON: TIM WEEGAR  
1501 AVIATION WAY  
AUGUSTA, GEORGIA 30906-9620  
(706) 796-4009 TWEEGAR@AUGUSTAGA.GOV

### PROJECT ADDRESS:

1501 AVIATION WAY  
AUGUSTA, GEORGIA 30906-9620

24-HOUR CONTACT FOR ESPC: TBD

PROJECT AREA: 99.39± ACRES

DISTURBED AREA: 22.25± ACRES

ADDED IMPERVIOUS AREA: 8.80± ACRES

RECEIVING WATER: TRIBUTARY TO TWIGGS DEAD RIVER

## NPDES INFORMATION

CONSTRUCTION EXIT (Co) LOCATION:  
N 33.21096° W 81.575274°

RUNOFF COEFFICIENT (CN VALUE)  
PRE-DEVELOPED: 73  
POST-DEVELOPED: 83

## SOIL SEDIMENTATION AND POLLUTION CONTROL NOTES (CHECK LIST #34)

### CLEARING PHASE - INITIAL PERIMETER CONTROL

- PRIOR TO THE LAND DISTURBING ACTIVITY, THE CONTRACTOR SHALL SCHEDULE A PRE-CONSTRUCTION MEETING WITH THE CONTRACTING OFFICER.
- NO STAGING AREAS, MATERIAL STORAGE, CONCRETE WASH OUT AREAS, OR DEBRIS BURN AND BURIAL HOLES SHALL BE LOCATED WITHIN 50 FT OF DESIGNATED TREE PROTECTION AREAS OR STREAM BUFFERS.
- A COPY OF THE APPROVED LAND DISTURBANCE PLAN AND PERMIT SHALL BE PRESENT ON THE SITE AT ALL TIMES, POST ON DAY ONE.
- PRIOR TO COMMENCING LAND DISTURBANCE ACTIVITY, THE LIMITS OF LAND DISTURBANCE SHALL BE CLEARLY AND ACCURATELY DEMARCATED WITH STAKES, RIBBONS, OR OTHER APPROPRIATE MEANS. THE LOCATION AND EXTENT OF ALL LAND DISTURBANCE ACTIVITY SHALL BE DEMARCATED FOR THE DURATION OF THE CONSTRUCTION ACTIVITY. NO LAND DISTURBANCE SHALL OCCUR OUTSIDE THE APPROVED LIMITS INDICATED ON THE APPROVED PLANS.
- PRIOR TO ANY OTHER CONSTRUCTION, A STABILIZED CONSTRUCTION ENTRANCE SHALL BE CONSTRUCTED AT EACH POINT OF ENTRY OR EXIT FROM THE SITE OR ONTO ANY PUBLIC ROADWAY AS SHOWN ON THE PLANS.
- IMMEDIATELY AFTER THE ESTABLISHMENT OF CONSTRUCTION ENTRANCE/EXITS, ALL PERIMETER EROSION CONTROL AND STORMWATER MANAGEMENT DEVICES SHALL BE INSTALLED AS SHOWN ON THE PHASE I E.S.P.C. PLAN.
- TREE PROTECTION FENCING AND STREAM BUFFER LIMITS SHOULD BE INSTALLED PRIOR TO THE START OF ANY LAND DISTURBANCE ACTIVITY AND MAINTAINED UNTIL FINAL LANDSCAPE IS INSTALLED. THE TREE PROTECTION FENCING SHOULD BE INSPECTED DAILY. ANY FAILURES OF SAID FENCING SHOULD BE REPAIRED IMMEDIATELY.
- AFTER INSTALLATION OF INITIAL EROSION CONTROL MEASURES THE SITE CONTRACTOR SHALL SCHEDULE AN INSPECTION BY THE PROJECT DESIGN PROFESSIONAL WITHIN 7 DAYS. NO OTHER CONSTRUCTION ACTIVITIES SHALL OCCUR UNTIL THE PROJECT PROFESSIONAL APPROVES THE INSTALLATION OF SAID EROSION CONTROL MEASURES. IF UNFORESEEN OCCURRENCES OCCUR IN THE FIELD THAT WARRANT ADDITIONAL EROSION CONTROL MEASURES, THE CONTRACTOR MUST CONSTRUCT ANY ADDITIONAL EROSION CONTROL DEVICES DEEMED NECESSARY BY THE SITE INSPECTION WITH CONSULTATION WITH DESIGN PROFESSIONAL.
- AFTER APPROVAL OF THE INITIAL EROSION CONTROL INSTALLATION, THE CONTRACTOR MAY PROCEED WITH CLEARING AND GRUBBING ACTIVITIES. AS CLEARING PERMITS, THE CONTRACTOR SHALL CONSTRUCT TEMPORARY SEDIMENT PONDS AND DIVERSION DIKES TO CONTROL EROSION AND STORMWATER RUNOFF.
- THE CONTRACTOR SHALL IMPLEMENT NPDES REQUIREMENTS PART IV OF GAR 100001 THRU THE DURATION OF PROJECT.
- MULCH OR TEMPORARY GRASSING SHALL BE APPLIED TO ALL EXPOSED AREAS WITHIN 14 DAYS OF LAND DISTURBANCE (LIA'S MAY REQUIRE LESS).
- TEMPORARY VEGETATION MAY BE EMPLOYED INSTEAD OF MULCH IF THE AREA REMAINS UNDISTURBED FOR LESS THAN 6 MONTHS. IF AN AREA WILL REMAIN UNDISTURBED FOR MORE THAN 6 MONTHS, PERMANENT VEGETATION TECHNIQUES SHALL BE EMPLOYED.
- CONTRACTOR SHALL INSPECT CONTROL MEASURES AT THE END OF EACH WORKING DAY TO ENSURE MEASURES ARE FUNCTIONING PROPERLY.
- FAILURE TO INSTALL, OPERATE, OR MAINTAIN ALL EROSION CONTROL MEASURES WILL RESULT IN ALL CONSTRUCTION BEING STOPPED ON THE JOB UNTIL SUCH MEASURES ARE CORRECTED BACK TO THE APPROVED E.S.P.C. PLANS, I.E., MANDATORY STOP WORK ORDER. THE CONTRACTOR SHALL OBSERVE THE PROJECT SEQUENCE.
- AS SHOWN ON THE PLANS, THE CONTRACTOR SHALL MAINTAIN CAREFUL SCHEDULING AND PERFORMANCE TO INSURE THAT LAND STRIPPED OF ITS NATURAL COVER IS EXPOSED ONLY IN SMALL QUANTITIES.
- NO BURN OR BURY PITS SHALL BE PERMITTED ON THE CONSTRUCTION SITE WITHOUT WRITTEN PERMISSION OF THE OWNER AND/OR THE ENGINEER OF RECORD. ADDITIONAL SILT BARRIERS MUST BE PLACED AS SHOWN ON THE
- PLAN AS ACCESS IS OBTAINED DURING CLEARING, NO GRADING SHALL TAKE PLACE UNTIL SILT BARRIER INSTALLATION AND SEDIMENT PONDS ARE CONSTRUCTED AS SHOWN ON THE PHASE I - E.S.P.C. PLAN.
- SEDIMENT AND EROSION CONTROL MEASURES SHOULD BE CHECKED AFTER EACH RAIN EVENT. EACH DEVICE IS TO BE MAINTAINED OR REPLACED IF SEDIMENT ACCUMULATION HAS REACHED 1/3 THE CAPACITY OF THE DEVICE. ADDITIONAL DEVICES MUST BE INSTALLED IF NEW CHANNELS HAVE DEVELOPED.
- THE CONSTRUCTION EXIT SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACK OR FLOW OF MUD ONTO PUBLIC RIGHT-OF-WAY. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH 1"-3" OF STONE AS CONDITIONS DEMAND. ALL MATERIALS SPILLED, DROPPED, WASHED, OR TRACKED FROM VEHICLE ONTO PUBLIC ROADWAY OR INTO STORM DRAIN MUST BE REMOVED IMMEDIATELY.

### GRADING PHASE - INTERMEDIATE EROSION & SEDIMENT CONTROL

- DURING CONSTRUCTION, THE CONTRACTOR SHALL MAINTAIN CAREFUL SCHEDULING AND PERFORMANCE TO ENSURE THAT LAND STRIPPED OF ITS NATURAL GROUND COVER IS EXPOSED ONLY IN SMALL QUANTITIES AND THEREFORE LIMITED DURATION, BEFORE PERMANENT EROSION PROTECTION IS ESTABLISHED.
- MAINTAIN FULL COORDINATION WITH THE DESIGN PROFESSIONAL, CONTRACTOR AND REGULATORY INSPECTOR AT ALL TIMES REGARDING PROJECT SEQUENCE.
- EARTHWORK OPERATIONS IN THE VICINITY OF STREAM BUFFERS SHALL BE CAREFULLY CONTROLLED TO AVOID DUMPING OR SLOUGHING INTO THE BUFFER AREAS.
- SEDIMENT SHALL NOT BE WASHED INTO INLETS. IT SHALL BE REMOVED FROM THE SEDIMENT TRAPS AND DISPOSED OF AND STABILIZED SO THAT IT WILL NOT ENTER THE INLETS AGAIN.
- SILT SHALL BE REMOVED WHEN ACCUMULATION REACHES 1/3 HEIGHT OF BARRIER. ADDITIONALLY, DIVERSION DIKES SHALL BE CONSTRUCTED ALONG THE TOP OF SAID FILL SLOPES WITH THE USE OF TEMPORARY DOWN DRAINS TO CONTROL STORMWATER RUNOFF AS SHOWN ON THE PLANS.
- EROSION CONTROL DEVICES SHALL BE INSTALLED IMMEDIATELY AFTER GROUND DISTURBANCE OCCURS. THE LOCATION OF SOME OF THE EROSION CONTROL DEVICES MAY HAVE TO BE ALTERED FROM THAT SHOWN ON THE APPROVED PLANS IF DRAINAGE PATTERNS DURING CONSTRUCTION ARE DIFFERENT FROM THE FINAL PROPOSED DRAINAGE PATTERNS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO ACCOMPLISH EROSION CONTROL FOR ALL DRAINAGE PATTERNS CREATED AT VARIOUS STAGES DURING CONSTRUCTION. ANY DIFFICULTY IN CONTROLLING EROSION DURING ANY PHASE OF CONSTRUCTION SHALL BE REPORTED TO THE DESIGN PROFESSIONAL IMMEDIATELY.
- TYPE "C", SENSITIVE SILT FENCE SHOULD BE INSTALLED AT THE TOE OF ALL FILL SLOPES 10FT OR GREATER IN HEIGHT UNLESS NOTED OTHERWISE ON PLAN. THE SILT FENCE SHALL BE MAINTAINED UNTIL PERMANENT GROUND COVER IS ESTABLISHED ON THE SLOPE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ESTABLISHING BARRIERS AT THE TOE OF SLOPES UNDER CONSTRUCTION. THESE BARRIERS SHALL BE AS SHOWN ON THE PLANS. THESE BARRIERS MAY BE RELOCATED AND REUSED AFTER PERMANENT SLOPE STABILIZATION BECOMES FULLY ESTABLISHED. AS THEY ARE RELOCATED, ANY DEFECTIVE MATERIALS IN THE BARRIER SHALL BE REPLACED. IN ADDITION, ALL DEBRIS AND SILT AT THE PREVIOUS LOCATION SHALL BE REMOVED AND GRADE SHALL BE REESTABLISHED PER PROPOSED.
- ALL SLOPES STEEPER THAN 3:1 AND WITH A HEIGHT OF 10 FT. OR GREATER, AND CUTS AND FILLS WITHIN STREAM BUFFERS, SHALL BE STABILIZED WITH APPROPRIATE EROSION CONTROL MATTING OR BLANKETS.
- TYPE "A", NON-SENSITIVE SILT FENCE SHALL BE PLACED AT THE TOE OF ALL DIRT STOCK PILE AREAS.
- INLET SEDIMENT PROTECTION MEASURES SHALL BE INSTALLED ON ALL STORM STRUCTURES AS THEY ARE CONSTRUCTED.
- MULCH OR TEMPORARY VEGETATION SHALL BE APPLIED TO ALL EXPOSED AREAS WITHIN 14 DAYS OF LAND DISTURBANCE.
- ALL DISTURBED AREAS LEFT MULCHED AFTER 30 DAYS SHALL BE STABILIZED WITH TEMPORARY VEGETATION.
- THE CONTRACTOR SHALL MAINTAIN THE SEDIMENT POND UNTIL PERMANENT GROUND COVER IS ESTABLISHED. SEDIMENT SHALL BE CLEANED OUT OF THE POND WHEN IT REACHES THE 1/3 DEPTH OF BASIN.
- SEDIMENT AND EROSION CONTROL MEASURES SHOULD BE CHECKED AFTER EACH RAIN EVENT. EACH DEVICE IS TO BE MAINTAINED OR REPLACED IF SEDIMENT ACCUMULATION HAS REACHED 1/3 THE CAPACITY OF THE DEVICE. ADDITIONAL DEVICES MUST BE INSTALLED IF NEW CHANNELS HAVE DEVELOPED.
- THE CONSTRUCTION EXIT SHALL BE MAINTAINED IN A CONDITION, WHICH WILL PREVENT TRACK OR FLOW OF MUD ONTO PUBLIC RIGHT-OF-WAY. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH 1"-3" OF STONE, AS CONDITIONS DEMAND. ALL MATERIALS SPILLED, DROPPED, WASHED, OR TRACKED FROM A VEHICLE ONTO A PUBLIC ROADWAY OR INTO STORM DRAIN MUST BE REMOVED IMMEDIATELY.
- CONTRACTOR SHALL INSPECT CONTROL MEASURES AT THE END OF EACH WORKING DAY TO ENSURE MEASURES ARE FUNCTIONING PROPERLY.
- FAILURE TO INSTALL, OPERATE, OR MAINTAIN ALL EROSION CONTROL MEASURES WILL RESULT IN ALL CONSTRUCTION BEING STOPPED ON THE JOB UNTIL SUCH MEASURES ARE CORRECTED BACK TO THE APPROVED E.S.P.C. PLANS.

### FINAL PHASE - FINAL STABILIZATION

- MAINTAIN FULL COORDINATION WITH THE DESIGN PROFESSIONAL, CONTRACTOR AND REGULATORY INSPECTOR AT ALL TIMES UNTIL FINAL PHASE.
- SEDIMENT SHALL NOT BE WASHED INTO INLETS. IT SHALL BE REMOVED FROM THE SEDIMENT TRAPS AND DISPOSED OF AND STABILIZED SO THAT IT WILL NOT ENTER THE INLETS AGAIN.
- THE CONTRACTOR SHALL MAINTAIN ALL SEDIMENT TRAPS AND EROSION CONTROL MEASURES UNTIL PERMANENT GROUND COVER IS ESTABLISHED.
- AFTER CURBING, GRADED AGGREGATE BASE, AND PAVEMENT HAS BEEN INSTALLED, ALL INLET SEDIMENT TRAPS ON A SINGLE AND DOUBLE WING CATCH BASINS ALONG WITH ANY CURB INLETS SHALL BE REMOVED AND REPLACED WITH CURB FILTER INLET PROTECTION.
- ALL ROADWAY AND PARKING SHOULDERS SHOULD BE GRASSED AS SOON AS FINAL GRADE IS ACHIEVED BEHIND CURBS.
- SEDIMENT AND EROSION CONTROL MEASURES SHOULD BE CHECKED AFTER EACH RAIN EVENT. EACH DEVICE IS TO BE MAINTAINED OR REPLACED IF SEDIMENT ACCUMULATION HAS REACHED 1/3 THE CAPACITY OF THE DEVICE. ADDITIONAL DEVICES MUST BE INSTALLED IF NEW CHANNELS HAVE DEVELOPED.
- THE CONTRACTOR SHALL INSPECT CONTROL MEASURES AT THE END OF EACH WORKING DAY TO ENSURE MEASURES ARE FUNCTIONING PROPERLY.
- FAILURE TO INSTALL, OPERATE, OR MAINTAIN ALL EROSION CONTROL MEASURES WILL RESULT IN ALL CONSTRUCTION BEING STOPPED ON JOB UNTIL SUCH MEASURES ARE FUNCTIONING PROPERLY.
- ALL DISTURBED AREAS WILL BE GRASSED AND STABILIZED IMMEDIATELY AFTER GRADING ACCORDING TO THE GRASSING, FERTILIZATION AND MULCH SPECIFICATION. STABILIZATION MEASURES SHALL BE INITIATED ON DISTURBED AREAS WITHIN 14 DAYS OF CESSATION OF CONSTRUCTION ACTIVITY IN THAT AREA, OR WHEN CONSTRUCTION ACTIVITY WILL NOT RESUME WITHIN TWENTY-ONE (21) DAYS OR WITHIN FOURTEEN (14) DAYS OF COMPLETION OF CONSTRUCTION ACTIVITY.
- FINAL GRADING WILL CONSIST OF SPREADING TOPSOIL (4" MIN. TO FINISHED GRADES INDICATED) ON ALL DISTURBED AREAS NOT PAVED.
- ALL SLOPES STEEPER THAN 4:1 WILL BE HYDROSEEDDED UNLESS OTHERWISE SODDED.

## NPDES GAR 100001 CERTIFICATION

"I CERTIFY THAT THE PERMITTEE'S EROSION, SEDIMENTATION AND POLLUTION CONTROL PLAN PROVIDES FOR AN APPROPRIATE AND COMPREHENSIVE SYSTEM OF BEST MANAGEMENT PRACTICES REQUIRED BY THE GEORGIA WATER QUALITY CONTROL ACT AND THE DOCUMENT "MANUAL FOR EROSION AND SEDIMENT CONTROL IN GEORGIA" (MANUAL) PUBLISHED BY THE STATE SOIL AND WATER CONSERVATION COMMISSION AS OF JANUARY 1 OF THE YEAR IN WHICH THE LAND-DISTURBING ACTIVITY WAS PERMITTED, PROVIDES FOR THE SAMPLING OF THE RECEIVING WATER(S) OR THE SAMPLING OF STORM WATER OUTFALLS AND THAT THE DESIGNED SYSTEM OF BEST MANAGEMENT PRACTICES AND SAMPLING METHODS IS EXPECTED TO MEET THE REQUIREMENTS CONTAINED IN THE GENERAL NPDES PERMIT NO. GAR100001."

"I CERTIFY UNDER PENALTY OF LAW THAT THIS PLAN WAS PREPARED AFTER A SITE VISIT TO THE LOCATIONS DESCRIBED HEREIN BY MYSELF OR MY AUTHORIZED AGENT, UNDER MY SUPERVISION."

MERIDETH K. POOL, P.E.  
LEVEL II CERT. NO. 0000037897

11/15/2022  
DATE

**Mead  
Hunt**

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AUGUSTA, GA 30906-9620

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ISSUED FOR BID

GEORGIA  
REGISTERED  
No. 32385  
PROFESSIONAL  
ENGINEER  
MERIDETH K. POOL

ISSH NO: 0119700-170868.01  
DATE: NOVEMBER 15, 2022  
DESIGNED BY: MKP  
DRAWN BY: MKP  
CHECKED BY: ALL  
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SHEET CONTENTS  
EROSION,  
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NOTES & LEGEND

SHEET NO. of

C-032

**GSWCC** GEORGIA SOIL AND WATER  
CONSERVATION COMMISSION  
Merideth K. Pool  
Level II Certified Design Professional  
CERTIFICATION NUMBER: 0000037897  
ISSUED: 02/28/2022 EXPIRES: 02/28/2025



CONSTRUCTION SCHEDULE

START DATE 06/15/2023

END DATE 06/15/2024

Table with 5 columns: ITEM, DESCRIPTION, MONTHS OF CONSTRUCTION ACTIVITY (1-12), and checkboxes for 'MAINTAIN AS REQUIRED!'.

NOTES:

- 1. OWNERS OR OPERATORS OR BOTH SHALL SUBMIT A NOTICE OF INTENT (NOI) IN ACCORDANCE WITH NPDES REQUIREMENTS AT LEAST 14 DAYS PRIOR TO THE COMMENCEMENT OF CONSTRUCTION ACTIVITIES.



Rr Roanoke loam, DgA Dogue fine sandy loam, 0 to 3 percent slopes, WvB Wickham-Urban land complex, 2 to 6 percent slopes

SOILS MAP

DESCRIPTION:

PREVENT OR REDUCE THE DISCHARGE OF POLLUTANTS TO STORMWATER FROM CONCRETE WASTE BY CONDUCTING OFF-SITE WASHOUT, PERFORMING ON-SITE WASHOUT IN A DESIGNATED AREA, AND TRAINING EMPLOYEES AND SUBCONTRACTORS.

TARGETED POLLUTANTS: CONCRETE WASTE

CONSTRUCTION GUIDELINES:

- 1. AVOID MIXING EXCESS AMOUNTS OF FRESH CONCRETE OR CEMENT ON SITE. 2. PERFORM WASHOUT OF CONCRETE TRUCKS OFF SITE OR IN DESIGNATED AREAS ONLY.

FOR ON-SITE WASHOUT:

- 1. LOCATE WASHOUT AREA AT LEAST 50 FT FROM STORM DRAINS, OPEN DITCHES, OR WATER BODIES. CONSTRUCT A TEMPORARY PIT OR BERMED/DIked AREA WITH A PAVED OR GRAVEL APPROACH TO CAPTURE LIQUID AND SOLID WASTE.

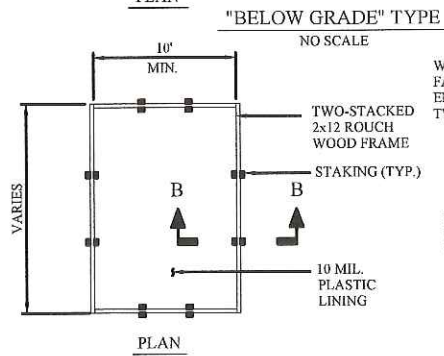
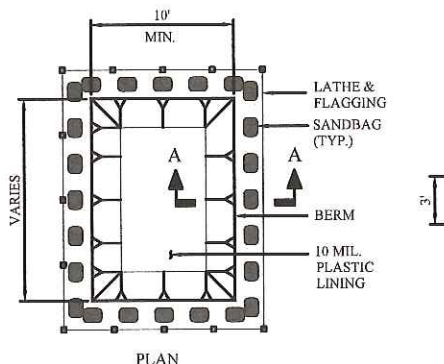
MAINTENANCE:

- 1. INSPECT SUBCONTRACTORS TO ENSURE THAT CONCRETE WASTES ARE BEING PROPERLY MANAGED. TEMPORARY WASHOUT FACILITIES MUST BE CLEANED, OR NEW FACILITY MUST BE CONSTRUCTED AND READY FOR USE ONCE THE WASHOUT IS 75% FULL.

CW

CONCRETE WASTE MANAGEMENT

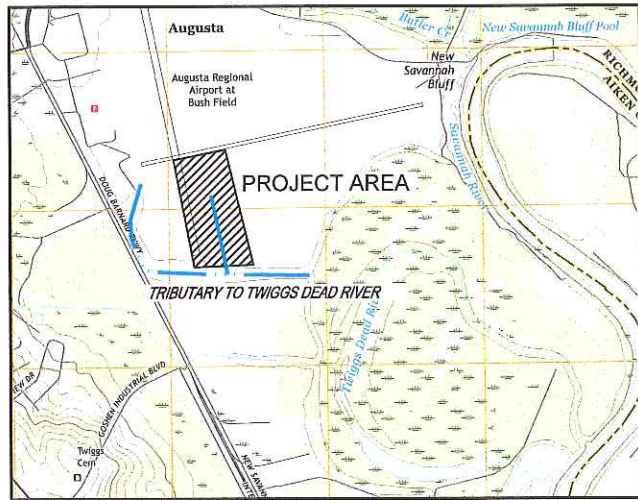
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NOTES:

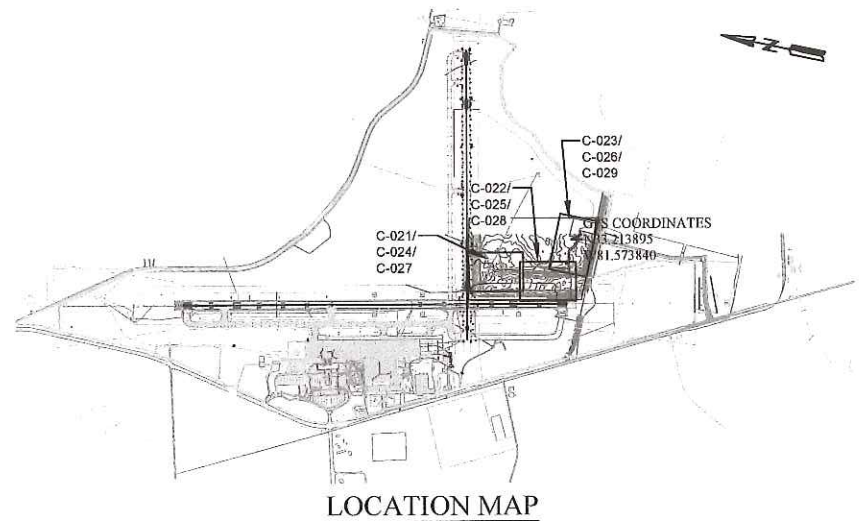
- 1. CONTRACTOR MUST PROVIDE A DESIGNATED AREA FOR CONCRETE WASHDOWN OF TOOLS, CONCRETE MIXER CHUTES, AND THE REAR OF THE VEHICLES. THIS AREA MUST HAVE A CONCRETE WASHOUT FACILITY AND SHALL BE CONSTRUCTED PER THE DETAIL SHOWN ABOVE.

USGS MAP - DRAINAGE AREA MAP



GPS LOCATION: N 33.42833° W 82.59055° DATUM: NAD 83 USGS MAP NAME: MECHANIC HILL, GA-SC. MAP SCALE: 1" = 2,000'

SURFACE DRAINAGE AREA MAP: 1.67 SQ. MILES



LOCATION MAP SCALE: 1"=2000'

EROSION, SEDIMENTATION & POLLUTION CONTROL PLAN CHECKLIST STAND ALONE CONSTRUCTION PROJECTS

Project Name: TAXIWAY G - PHASE I Address: 1501 AVIATION WAY City/County: AUGUSTA-RICHMOND Date on Plans: 11/15/2022 Name & email of person filling out checklist: MERIDETH POOL MKPOOL@ISMLLC-ENGR.COM

- 1. The applicable Erosion, Sedimentation and Pollution Control Plan Checklist established by the Commission as of January 1 of the year in which the land-disturbing activity was permitted. 2. Level II certification number issued by the Commission, signature and seal of the certified design professional.

- 27 Description of practices to provide cover for building materials and building products on site. 28 Description of the practices that will be used to reduce the pollutants in storm water discharges. 29 Description and chart or timeline of the intended sequence of major activities which disturb soils for the major portions of the site.

Mead & Hunt, Inc. 878 South Lake Drive Lexington, SC 29072 phone: 803-996-2900 meadhunt.com



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AUGUSTA REGIONAL AIRPORT CONSTRUCT TAXIWAY G

1501 AVIATION WAY AUGUSTA, GA 30906-9620



MSH NO: 0119700-170869.01 DATE: NOVEMBER 15, 2022 DESIGNED BY: MKP DRAWN BY: MKP CHECKED BY: ALL

SHEET CONTENTS EROSION, SEDIMENTATION & POLLUTION CONTROL NOTES & DETAILS

Effective January 1, 2022 C-033



Species	Rates per Acre	Rates per 1,000 sq. ft.	Planting Dates by Region			
			M-1	P	C	
Barley	3 bu.	3.3 lb.	8/15-11/15	8/15-12/15	9/1-12/31	
Oats	4 bu.	3.3 lb.	9/1-11/30	9/1-11/30	9/1-11/30	
Triticale	3 bu.	3.3 lb.	9/1-11/30	9/1-11/30	9/1-11/30	
Ryegrass, Annual	40 lbs.	0.9 lb.	8/1-1/30	8/1-1/15	8/15-3/31	
Rye Grain (alone)	3.0 bu.	3.9 lb.	7/15-11/30	8/15-12/31	9/1-2/28	
Rye Grain (in mixture)	0.5 bu.	0.6 lb.	7/15-11/30	8/15-12/31	9/1-2/28	
Lepesosa, Annual	40 lbs.	0.9 lb.	2/1-4/30	2/15-4/30	1/15-3/15	
Weeping Lovegrass	4 lbs.	0.1 lb.	3/15-6/15	3/15-6/15	2/15-6/15	
Sudangrass	60 lbs.	1.4 lb.	4/1-8/31	4/1-8/31	3/1-7/31	
Millet, Browntop	40 lbs.	0.9 lbs.	4/1-6/30	4/1-7/15	4/1-7/15	
Millet, Pearl	50 lbs.	1.1 lbs.	5/1-7/31	4/15-8/31	4/1-8/31	
Wheat	3 bu.	4.1 lbs.	9/1-12/31	9/1-12/31	9/15-1/31	

1. Temporary cover crops are very competitive and will crowd out perennials if seeded too heavily.
2. Reduce seeding rates by 50% when drilled.
3. Unusual site conditions may require heavier seeding rates.
4. Seeding rates may need to be altered to fit temperature variations and local conditions.

Species	Rate per Acre	Rates per 1,000 sq. ft.	Planting Dates by Region				Remarks
			M-1	P	C		
Bermuda, Common (Hulled seed) Alone With Perennials	10 lbs.	0.2 lb.	3/1-6/30	2/15-6/30		Quick cover, low growing, sod forming; needs full sun.	
Bermuda, Common (Hulled seed) With temporary cover With other perennials	10 lbs.	0.2 lb.	10/1-2/28	1/1-1/31		Plant with winter annuals.	
Bermuda Sprigs With temporary cover	40 cu. ft.	0.5 cu. ft.	3/15-7/15	3/15-7/31	1/1-12/31	1 cu. ft. = 650 sprigs; 1 bu. = 1.25 cu. ft. or 800 sprigs.	

Types of Species	Planting Year	Fertilizer (N-P-K)	Rate (lbs./acre)	N Top Dressing Rate (lbs./acre)
Cool season grasses	First	6-12-12	1500	50-100
	Second Maintenance	6-12-12 10-10-10	1000 400	30
Cool season grasses & legumes	First	6-12-12 0-10-10	1500 1000	0-50
	Second Maintenance	6-12-12 10-10-10	1000 400	30
Ground covers	First	10-10-10	1300	30
	Second Maintenance	10-10-10	1300 1100	30
Pine seedlings	First	20-10-5	one 21-gram pellet per seedling placed in the closing hole	30
	Maintenance	0-10-10	700 700	30
Shrub Lespedeza	First	0-10-10	700	30
	Maintenance	0-10-10	700	30
Temporary cover crops seeded alone	First	10-10-10	500	30
	Maintenance	6-12-12 6-12-12 10-10-10	1500 800 400	50-100 30-100 30
Warm season grasses	First	6-12-12	1500	50
	Second Maintenance	6-12-12 10-10-10	800 400	50-100 30
Warm season grasses & legumes	First	6-12-12	1500	50
	Second Maintenance	6-12-12 10-10-10	1000 400	50-100 30

\* APPLY AGRICULTURAL LIME AS PRESCRIBED BY SOIL TESTS OR AT A RATE OF 1 TO 2 TONS PER ACRE

Material	Depth
Dry Straw Or Hay	2" To 4"
Wood Waste (sawdust, Bark, Chips)	2" To 3"
Cracked Asphalt (slow Curing)	1200 Gal./acre (1/4 Gal./sq.yd.)
Black Polyethylene Film	Completely Cover Area; Hold in Place With Soil On Outer Edge

Types of Species	Planting Year	Fertilizer (N-P-K)	Rate (lbs./acre)	N Top Dressing Rate (lbs./acre)
Cool season grasses	First	6-12-12	1500	50-100
	Second Maintenance	6-12-12 10-10-10	1000 400	30
Cool season grasses & legumes	First	6-12-12 0-10-10	1500 1000	0-50
	Second Maintenance	6-12-12 10-10-10	1000 400	30
Temporary cover crops seeded alone	First	10/10/10	500	30
	Maintenance	6-12-12 6-12-12 10-10-10	1500 800 400	50-100 30-100 30
Warm season grasses	First	6-12-12	1500	50-100
	Second Maintenance	6-12-12 10-10-10	800 400	50-100 30

M-1 represents the Mountain, Blue Ridge, and Ridge and Valleys MLRAs  
P represents the Southern Piedmont Region MLRA  
C represents the Southern Coastal Plain, Sand Hills, Black Land, and Atlantic Coast Flatwoods MLRAs

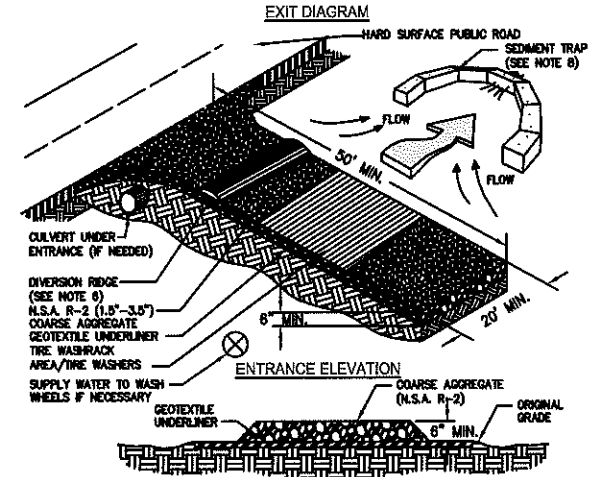
- Ds2 TEMPORARY GRASSING**
1. ALL SEEDED AREAS WILL BE MULCHED WITH HAY OR STRAW AT A RATE OF 2-2 1/2 TONS PER ACRE WITHIN 24 HRS. AFTER SEEDING. USE OF A TACKIFIER IS REQUIRED ON ALL SLOPES EXCEEDING 3:1.
  2. GRASSING WILL BE ACCEPTED WHEN A 95% COVER BY PERMANENT GRASSING IS OBTAINED AND WEEDS ARE NOT DOMINANT.
  3. GRASSING OF DISTURBED AREAS WILL COMMENCE AT COMPLETION OF EACH PHASE OF CONSTRUCTION OR IN THE SEQUENCE AS INDICATED. IN ANY CASE, GRASSING OF ANY DISTURBED AREA WILL BEGIN AT EARLIEST POSSIBLE DATE.

- Ds3 PERMANENT GRASSING**
4. IRRIGATION WILL BE APPLIED AT A RATE THAT WILL NOT CAUSE RUNOFF.
  5. TOPDRESSING WILL BE APPLIED ON ALL TEMPORARY & PERMANENT SPECIES PLANTED ALONE OR IN MIXTURES WITH OTHER SPECIES.
  6. PERMANENT GRASSING SHALL BE IRRIGATED AND MAINTAINED. BERMUDA GRASS MAY BE MOWED AS DESIRED MAINTAINING AT LEAST 4 TO 6 INCHES OF TOP GROWTH.

- Ds1 MULCHING**

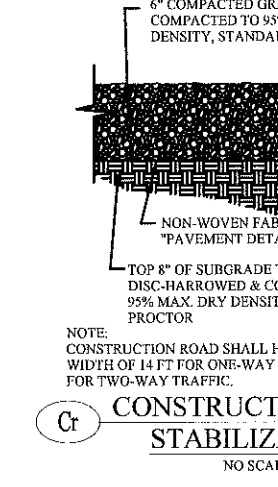
- Du DUST CONTROL BY IRRIGATION**
1. THE CONTRACTOR SHALL CONDUCT OPERATIONS AND MAINTAIN THE PROJECT SITE SO AS TO MINIMIZE THE CREATION AND DISPERSION OF DUST. DUST CONTROL SHALL BE USED THROUGHOUT THE WORK AT THE SITE.
  2. THE CONTRACTOR MUST PROVIDE CLEAN WATER, FREE FROM SALT, OIL AND OTHER DELETERIOUS MATERIALS TO BE USED FOR ON-SITE DUST CONTROL.
  3. THE CONTRACTOR SHALL SUPPLY WATER SPRAYING EQUIPMENT CAPABLE OF ACCESSING ALL WORK AREAS.
  4. THE CONTRACTOR SHALL IMPLEMENT STRICT DUST CONTROL MEASURES DURING ACTIVE CONSTRUCTION PERIODS ON-SITE. THESE CONTROL MEASURES WILL GENERALLY CONSIST OF WATER APPLICATIONS THAT SHALL BE APPLIED A MINIMUM OF ONCE PER DAY DURING DRY WEATHER OR MORE OFTEN AS REQUIRED TO PREVENT DUST EMISSIONS.
  5. FOR WATER APPLICATION TO UNDISTURBED OIL SURFACES, THE CONTRACTOR SHALL:
    - A. APPLY WATER WITH EQUIPMENT CONSISTING OF TANK, SPRAY BAR, PUMP DISCHARGE PRESSURE GAUGE.
    - B. ADJUST SPRAY BAR HEIGHT, NOZZLE SPACING AND SPRAY PATTERN TO PROVIDE COMPLETE COVERAGE OF GROUND WATER.
    - C. DISPERSE WATER THROUGH NOZZLES ON SPRAY BAR AT 20 PSI MINIMUM. KEEP AREAS DAMP WITHOUT CREATING NUISANCE CONDITIONS SUCH AS PONDING.
  6. FOR WATER APPLICATION TO SOIL SURFACES DURING DEMOLITION AND/OR EXCAVATION, THE CONTRACTOR SHALL:
    - A. APPLY WATER WITH EQUIPMENT CONSISTING OF A TANK, PUMP WITH DISCHARGE GAUGE, HOSES AND MIST NOZZLES.
    - B. LOCATE TANK AND SPRAYING EQUIPMENT SO THAT THE ENTIRE EXCAVATION AREA CAN BE MISTED WITHOUT INTERFERING WITH DEMOLITION AND/OR EXCAVATION EQUIPMENT OR OPERATIONS.
    - C. KEEP AREAS DAMP WITHOUT CREATING NUISANCE CONDITIONS SUCH AS PONDING.
    - D. APPLY WATER SPRAY IN A MANNER TO PREVENT MOVEMENT OF SPRAY BEYOND CONSTRUCTION BOUNDARIES.
  7. CONTRACTOR SHALL REFER TO "MANUAL FOR EROSION AND SEDIMENT CONTROL IN GEORGIA" LATEST EDITION FOR ADDITIONAL INFORMATION ON TEMPORARY AND PERMANENT DUST CONTROL BMPs.

**GRASSING, FERTILIZATION & MULCH**  
NO SCALE



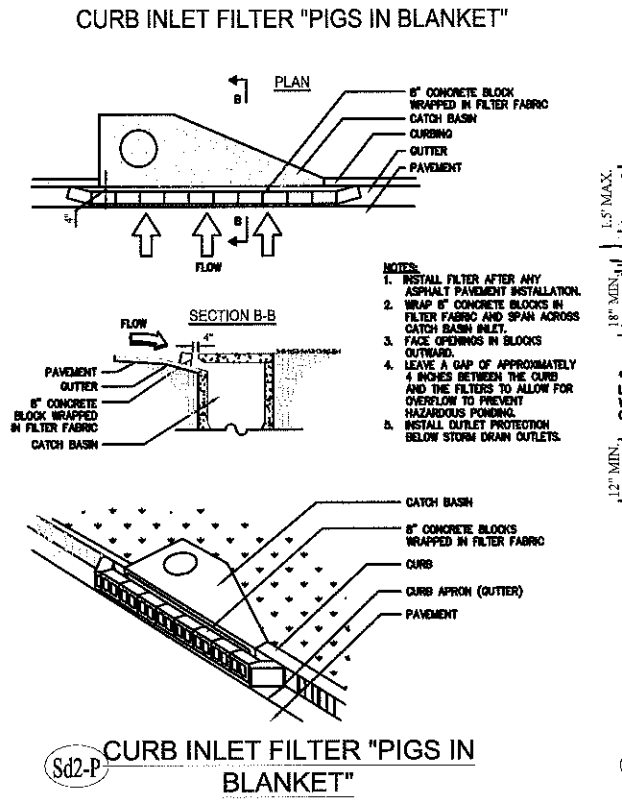
- NOTES:**
1. AVOID LOCATING ON STEEP SLOPES OR AT CURVES ON PUBLIC ROADS.
  2. REMOVE ALL VEGETATION AND OTHER UNSUITABLE MATERIAL FROM THE FOUNDATION AREA, GRADE, AND CROWN FOR POSITIVE DRAINAGE.
  3. AGGREGATE SIZE SHALL BE IN ACCORDANCE WITH NATIONAL STONE ASSOCIATION R-2 (1.5"-3.5" STONE).
  4. GRAVEL PAD SHALL HAVE A MINIMUM THICKNESS OF 6".
  5. PAD WIDTH SHALL BE EQUAL FULL WIDTH AT ALL POINTS OF VEHICULAR EGRESS, BUT NO LESS THAN 20'.
  6. A DIVERSION RIDGE SHOULD BE CONSTRUCTED WHEN GRADE TOWARD PAVED AREA IS GREATER THAN 2%.
  7. INSTALL PIPE UNDER THE ENTRANCE IF NEEDED TO MAINTAIN DRAINAGE DITCHES.
  8. WHEN WASHING IS REQUIRED, IT SHOULD BE DONE ON AN AREA STABILIZED WITH CRUSHED STONE THAT DRAINS INTO AN APPROVED SEDIMENT TRAP OR SEDIMENT BASIN (DIVERT ALL SURFACE RUNOFF AND DRAINAGE FROM THE ENTRANCE TO A SEDIMENT CONTROL DEVICE).
  9. WASHRACKS AND/OR TIRE WASHERS MAY BE REQUIRED DEPENDING ON SCALE AND CIRCUMSTANCE. IF NECESSARY, WASHRACK DESIGN MAY CONSIST OF ANY MATERIAL SUITABLE FOR TRUCK TRAFFIC THAT REMOVE MUD AND DIRT.
  10. MAINTAIN AREA IN A WAY THAT PREVENTS TRACKING AND/OR FLOW OF MUD ONTO PUBLIC RIGHTS-OF-WAYS. THIS MAY REQUIRE TOP DRESSING, REPAIR AND/OR CLEANOUT OF ANY MEASURES USED TO TRAP SEDIMENT.

**Co CRUSHED STONE CONSTRUCTION EXIT**

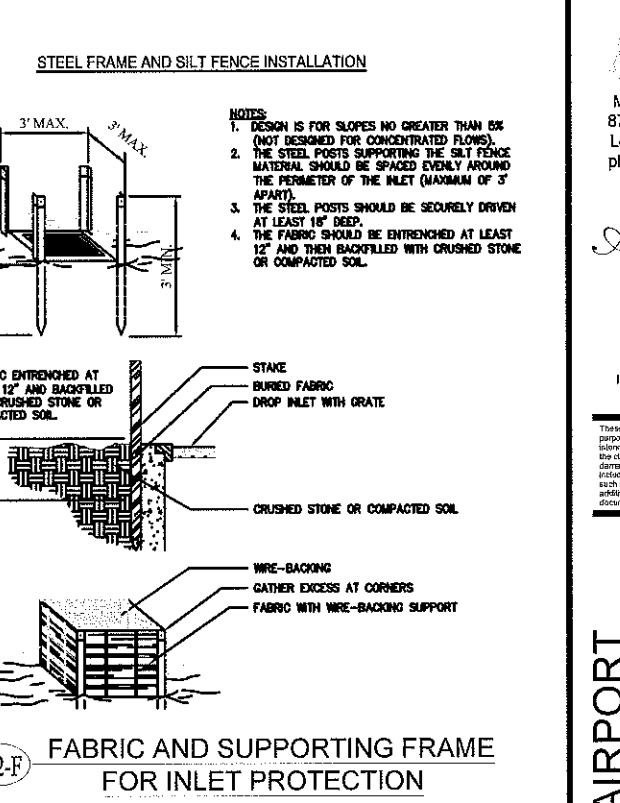


- NOTE:** CONSTRUCTION ROAD SHALL HAVE A MINIMUM WIDTH OF 14 FT FOR ONE-WAY TRAFFIC AND 20 FT FOR TWO-WAY TRAFFIC.
- Ct CONSTRUCTION ROAD STABILIZATION**  
NO SCALE

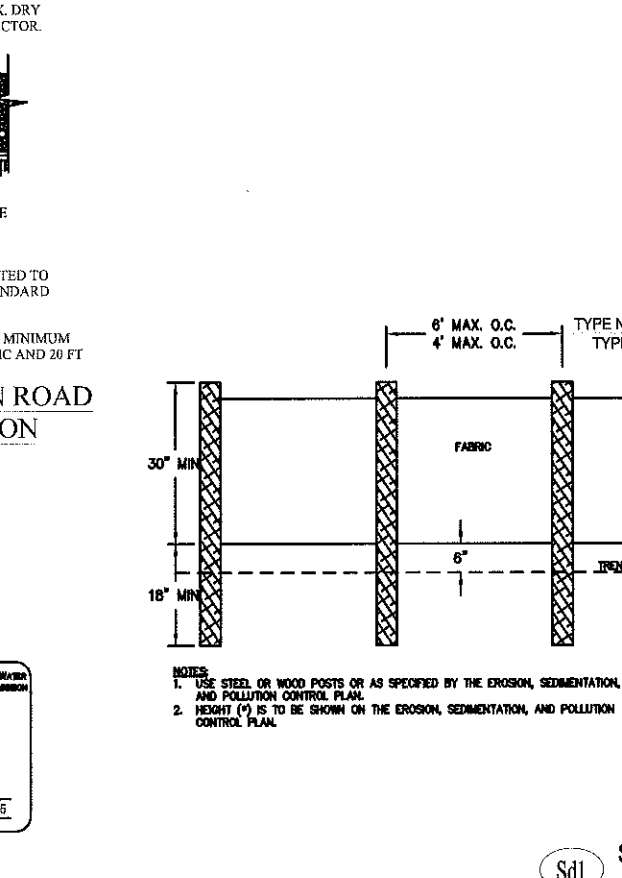
**GSWCC** GEORGIA SOIL AND WATER CONSERVATION COMMISSION  
Merideth K. Pool  
Level II Certified Design Professional  
Certification Number 000037897  
Issued: 02/28/2022 Expires: 02/28/2025



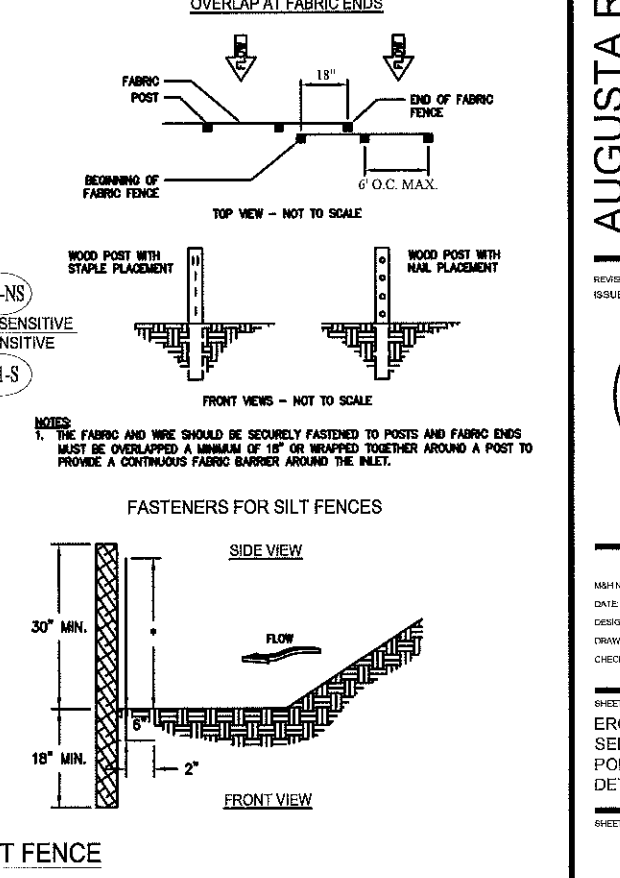
**Sd2-P CURB INLET FILTER "PIGS IN BLANKET"**



**Sd2-F FABRIC AND SUPPORTING FRAME FOR INLET PROTECTION**



**Sd1 SILT FENCE**



**Mead Hunt**  
Mead and Hunt, Inc.  
878 South Lake Drive  
Lexington, SC 29072  
phone: 803-996-2900  
meadhunt.com

Augusta Regional Airport  
**ISM**  
Infrastructure Systems  
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www.ismcorp.com

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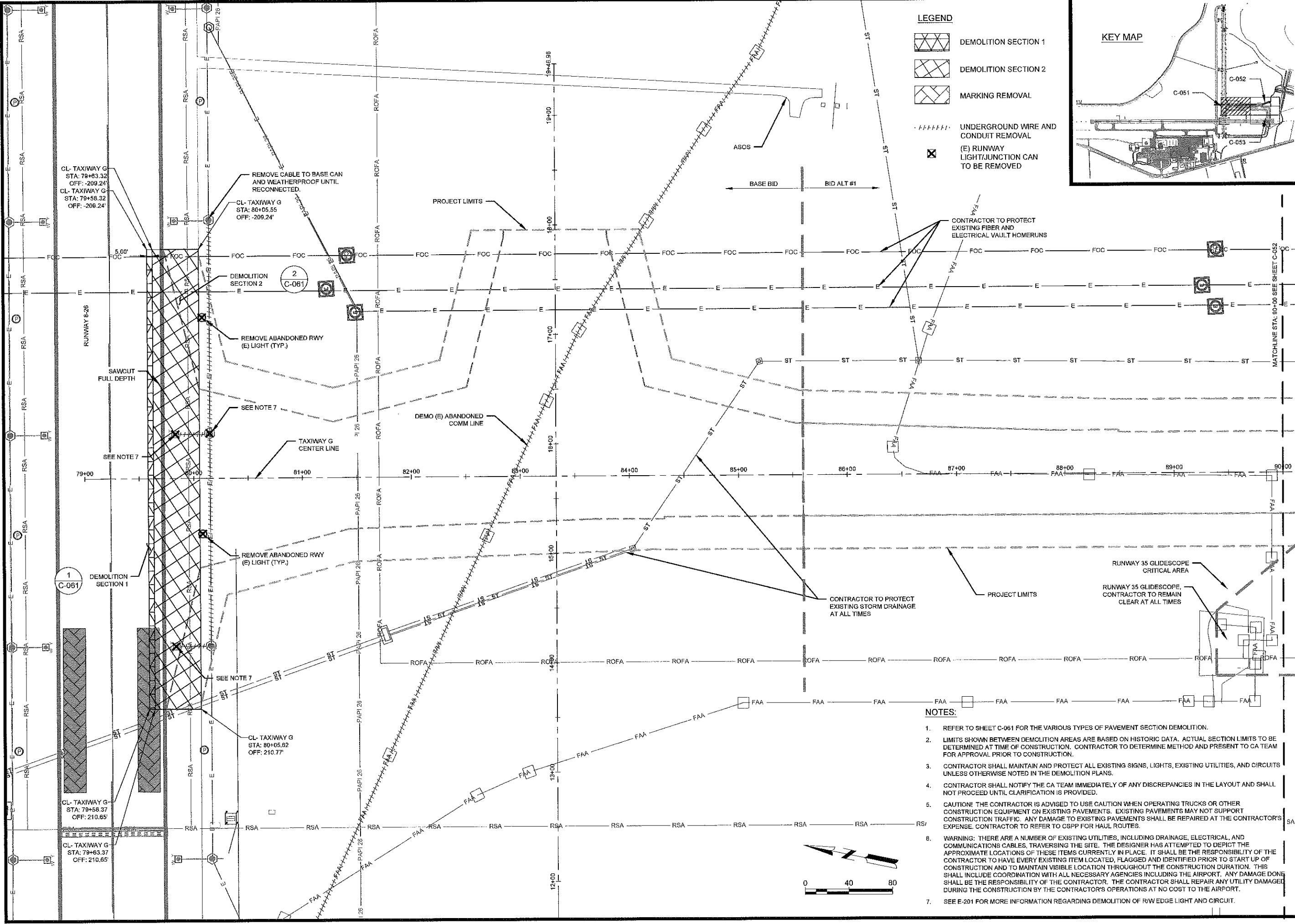
**GEORGIA REGISTERED ENGINEER**  
No. 32385  
MERIDETH K. POOL

MSH No.: 0119700-170889.01  
DATE: NOVEMBER 15, 2022  
DESIGNED BY: MKP  
DRAWN BY: MKP  
CHECKED BY: ALL  
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SHEET CONTENTS  
EROSION, SEDIMENTATION & POLLUTION CONTROL DETAILS

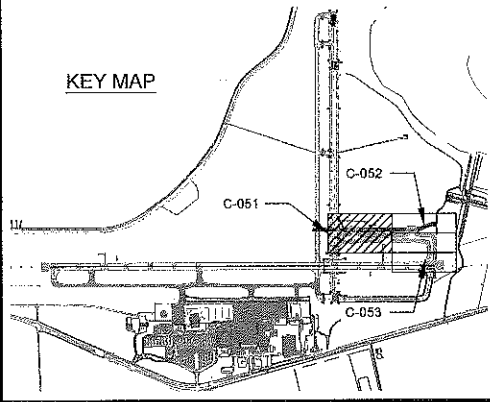
SHEET No. of  
**C-034**

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**LEGEND**

- DEMOLITION SECTION 1
- DEMOLITION SECTION 2
- MARKING REMOVAL
- UNDERGROUND WIRE AND CONDUIT REMOVAL
- (E) RUNWAY LIGHT/JUNCTION CAN TO BE REMOVED



**Mead & Hunt**  
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878 South Lake Drive  
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meadhunt.com

**Augusta Regional Airport**

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DATE: DECEMBER 6, 2022  
DESIGNED BY: EJS  
DRAWN BY: NJH  
CHECKED BY: DES  
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SHEET CONTENTS  
**DEMOLITION PLANS**

SHEET NO.

**C-051**

- NOTES:**
1. REFER TO SHEET C-061 FOR THE VARIOUS TYPES OF PAVEMENT SECTION DEMOLITION.
  2. LIMITS SHOWN BETWEEN DEMOLITION AREAS ARE BASED ON HISTORIC DATA. ACTUAL SECTION LIMITS TO BE DETERMINED AT TIME OF CONSTRUCTION. CONTRACTOR TO DETERMINE METHOD AND PRESENT TO CA TEAM FOR APPROVAL PRIOR TO CONSTRUCTION.
  3. CONTRACTOR SHALL MAINTAIN AND PROTECT ALL EXISTING SIGNS, LIGHTS, EXISTING UTILITIES, AND CIRCUITS UNLESS OTHERWISE NOTED IN THE DEMOLITION PLANS.
  4. CONTRACTOR SHALL NOTIFY THE CA TEAM IMMEDIATELY OF ANY DISCREPANCIES IN THE LAYOUT AND SHALL NOT PROCEED UNTIL CLARIFICATION IS PROVIDED.
  5. CAUTION! THE CONTRACTOR IS ADVISED TO USE CAUTION WHEN OPERATING TRUCKS OR OTHER CONSTRUCTION EQUIPMENT ON EXISTING PAVEMENTS. EXISTING PAVEMENTS MAY NOT SUPPORT CONSTRUCTION TRAFFIC. ANY DAMAGE TO EXISTING PAVEMENTS SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. CONTRACTOR TO REFER TO CSPP FOR HAUL ROUTES.
  6. WARNING! THERE ARE A NUMBER OF EXISTING UTILITIES, INCLUDING DRAINAGE, ELECTRICAL, AND COMMUNICATIONS CABLES, TRAVERSING THE SITE. THE DESIGNER HAS ATTEMPTED TO DEPICT THE APPROXIMATE LOCATIONS OF THESE ITEMS CURRENTLY IN PLACE. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO HAVE EVERY EXISTING ITEM LOCATED, FLAGGED AND IDENTIFIED PRIOR TO START UP OF CONSTRUCTION AND TO MAINTAIN VISIBLE LOCATION THROUGHOUT THE CONSTRUCTION DURATION. THIS SHALL INCLUDE COORDINATION WITH ALL NECESSARY AGENCIES INCLUDING THE AIRPORT. ANY DAMAGE DONE SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR SHALL REPAIR ANY UTILITY DAMAGED DURING THE CONSTRUCTION BY THE CONTRACTOR'S OPERATIONS AT NO COST TO THE AIRPORT.
  7. SEE E-201 FOR MORE INFORMATION REGARDING DEMOLITION OF RW EDGE LIGHT AND CIRCUIT.



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 DRAWN BY: NJH  
 CHECKED BY: DES  
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SHEET CONTENTS  
 DEMOLITION PLANS

SHEET NO.

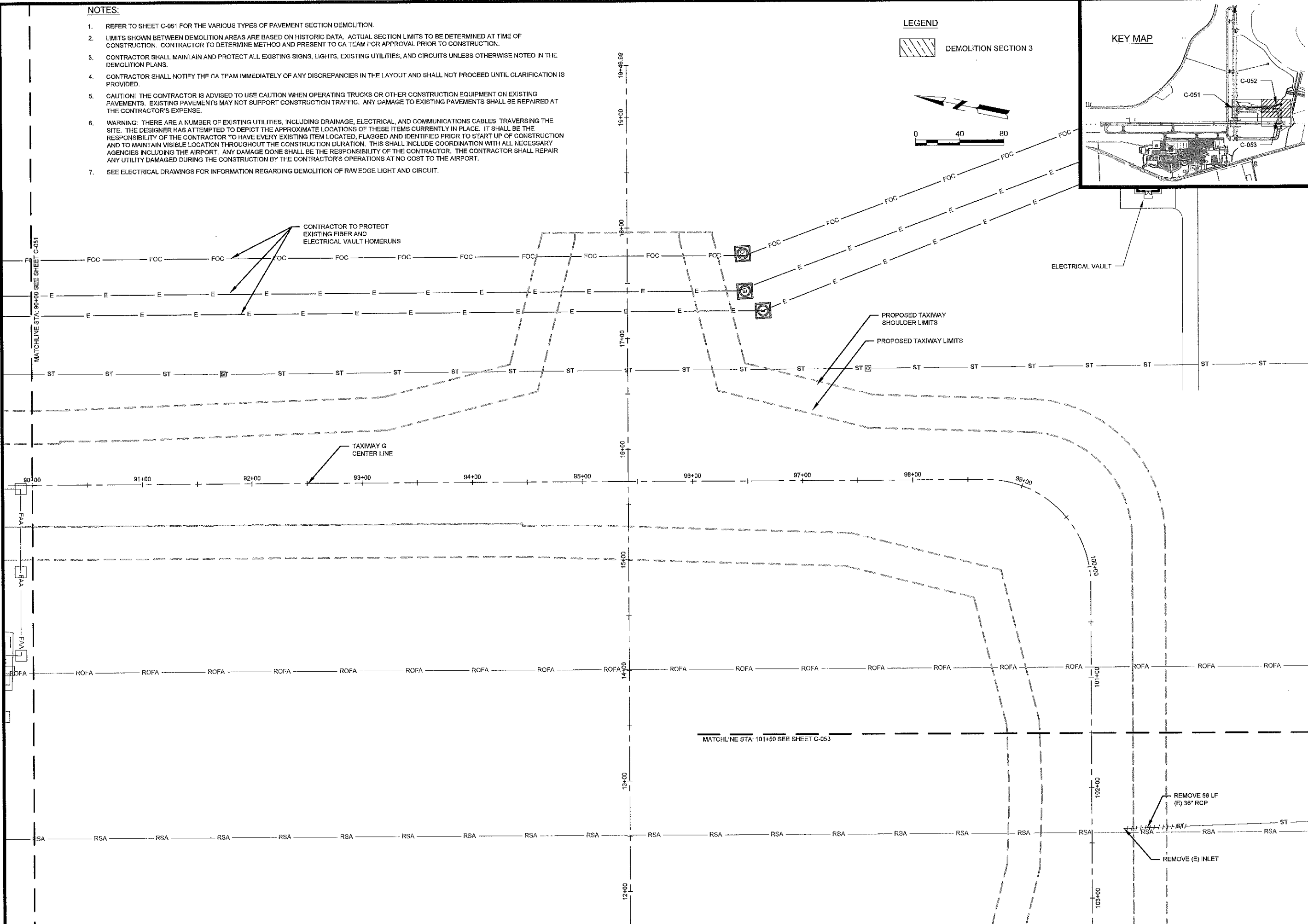
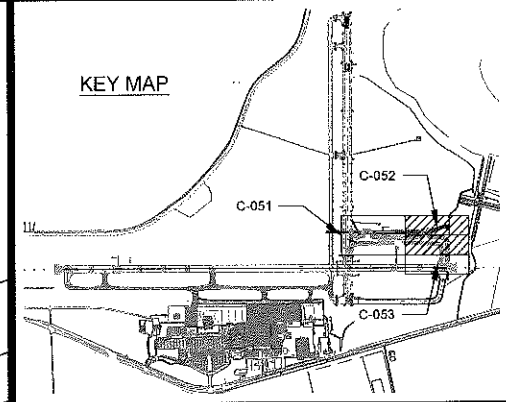
**C-052**

**NOTES:**

- REFER TO SHEET C-051 FOR THE VARIOUS TYPES OF PAVEMENT SECTION DEMOLITION.
- LIMITS SHOWN BETWEEN DEMOLITION AREAS ARE BASED ON HISTORIC DATA. ACTUAL SECTION LIMITS TO BE DETERMINED AT TIME OF CONSTRUCTION. CONTRACTOR TO DETERMINE METHOD AND PRESENT TO CA TEAM FOR APPROVAL PRIOR TO CONSTRUCTION.
- CONTRACTOR SHALL MAINTAIN AND PROTECT ALL EXISTING SIGNS, LIGHTS, EXISTING UTILITIES, AND CIRCUITS UNLESS OTHERWISE NOTED IN THE DEMOLITION PLANS.
- CONTRACTOR SHALL NOTIFY THE CA TEAM IMMEDIATELY OF ANY DISCREPANCIES IN THE LAYOUT AND SHALL NOT PROCEED UNTIL CLARIFICATION IS PROVIDED.
- CAUTION! THE CONTRACTOR IS ADVISED TO USE CAUTION WHEN OPERATING TRUCKS OR OTHER CONSTRUCTION EQUIPMENT ON EXISTING PAVEMENTS. EXISTING PAVEMENTS MAY NOT SUPPORT CONSTRUCTION TRAFFIC. ANY DAMAGE TO EXISTING PAVEMENTS SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
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- SEE ELECTRICAL DRAWINGS FOR INFORMATION REGARDING DEMOLITION OF RAW EDGE LIGHT AND CIRCUIT.

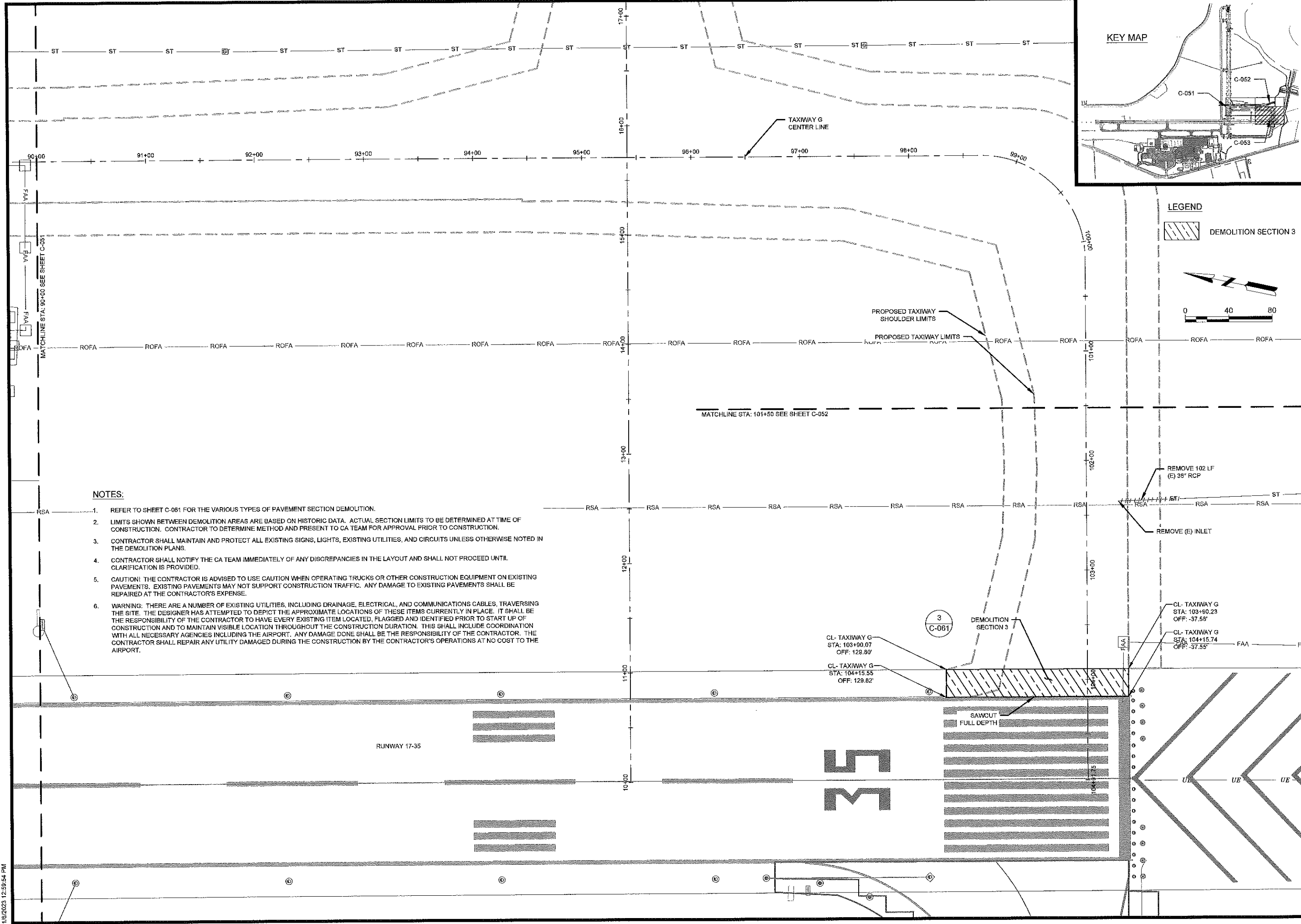
**LEGEND**

 DEMOLITION SECTION 3



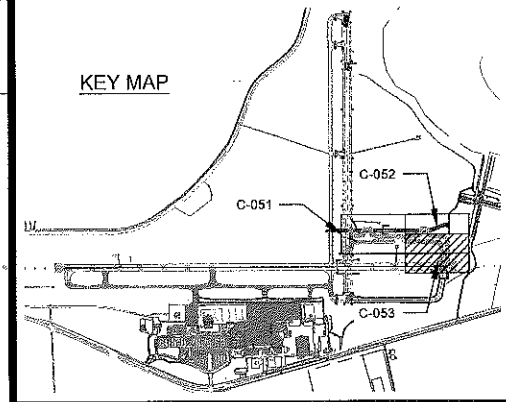
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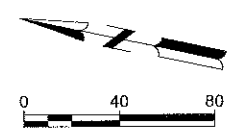
**NOTES:**

1. REFER TO SHEET C-061 FOR THE VARIOUS TYPES OF PAVEMENT SECTION DEMOLITION.
2. LIMITS SHOWN BETWEEN DEMOLITION AREAS ARE BASED ON HISTORIC DATA. ACTUAL SECTION LIMITS TO BE DETERMINED AT TIME OF CONSTRUCTION. CONTRACTOR TO DETERMINE METHOD AND PRESENT TO CA TEAM FOR APPROVAL PRIOR TO CONSTRUCTION.
3. CONTRACTOR SHALL MAINTAIN AND PROTECT ALL EXISTING SIGNS, LIGHTS, EXISTING UTILITIES, AND CIRCUITS UNLESS OTHERWISE NOTED IN THE DEMOLITION PLANS.
4. CONTRACTOR SHALL NOTIFY THE CA TEAM IMMEDIATELY OF ANY DISCREPANCIES IN THE LAYOUT AND SHALL NOT PROCEED UNTIL CLARIFICATION IS PROVIDED.
5. CAUTION! THE CONTRACTOR IS ADVISED TO USE CAUTION WHEN OPERATING TRUCKS OR OTHER CONSTRUCTION EQUIPMENT ON EXISTING PAVEMENTS. EXISTING PAVEMENTS MAY NOT SUPPORT CONSTRUCTION TRAFFIC. ANY DAMAGE TO EXISTING PAVEMENTS SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
6. WARNING: THERE ARE A NUMBER OF EXISTING UTILITIES, INCLUDING DRAINAGE, ELECTRICAL, AND COMMUNICATIONS CABLES, TRAVERSING THE SITE. THE DESIGNER HAS ATTEMPTED TO DEPICT THE APPROXIMATE LOCATIONS OF THESE ITEMS CURRENTLY IN PLACE. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO HAVE EVERY EXISTING ITEM LOCATED, FLAGGED AND IDENTIFIED PRIOR TO START UP OF CONSTRUCTION AND TO MAINTAIN VISIBLE LOCATION THROUGHOUT THE CONSTRUCTION DURATION. THIS SHALL INCLUDE COORDINATION WITH ALL NECESSARY AGENCIES INCLUDING THE AIRPORT. ANY DAMAGE DONE SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR SHALL REPAIR ANY UTILITY DAMAGED DURING THE CONSTRUCTION BY THE CONTRACTOR'S OPERATIONS AT NO COST TO THE AIRPORT.



**LEGEND**

DEMOLITION SECTION 3



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878 South Lake Drive  
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**AUGUSTA REGIONAL AIRPORT  
CONSTRUCT TAXIWAY G**

1501 AVIATION WAY  
AUGUSTA, GA 30906-9620

REVISION  
ISSUED FOR BID

**NOT FOR CONSTRUCTION**

MSH NO.: 0119700-170869.02  
DATE: DECEMBER 6, 2022  
DESIGNED BY: EJS  
DRAWN BY: NJH  
CHECKED BY: DES  
DO NOT SCALE DRAWINGS

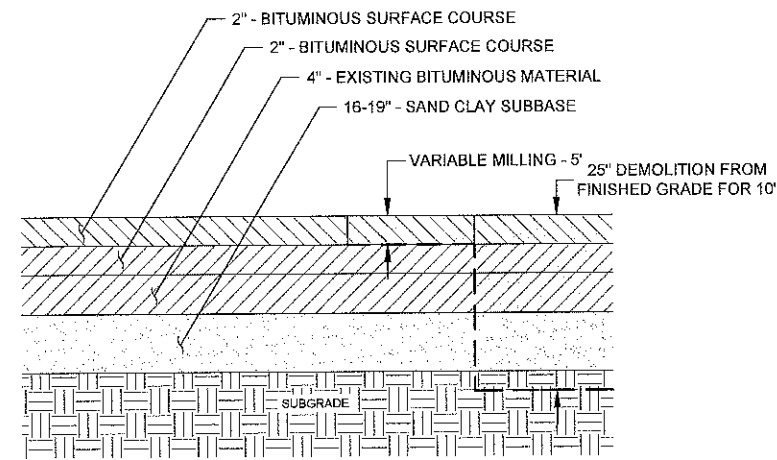
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DEMOLITION PLANS

SHEET NO.

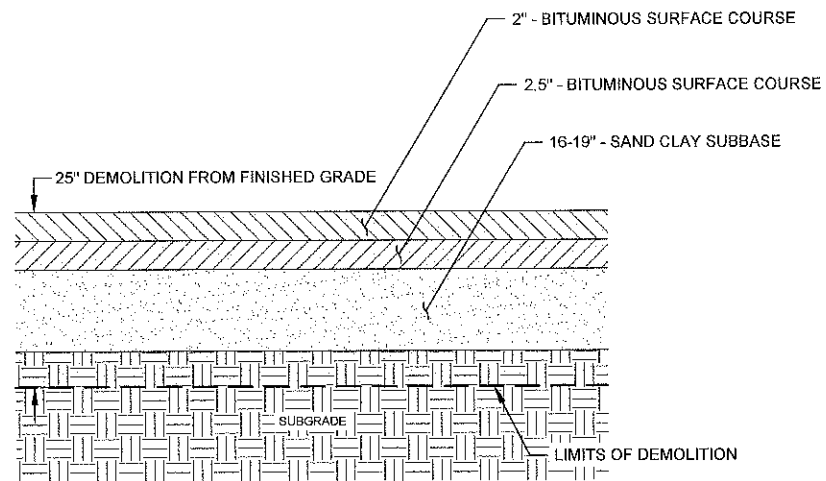
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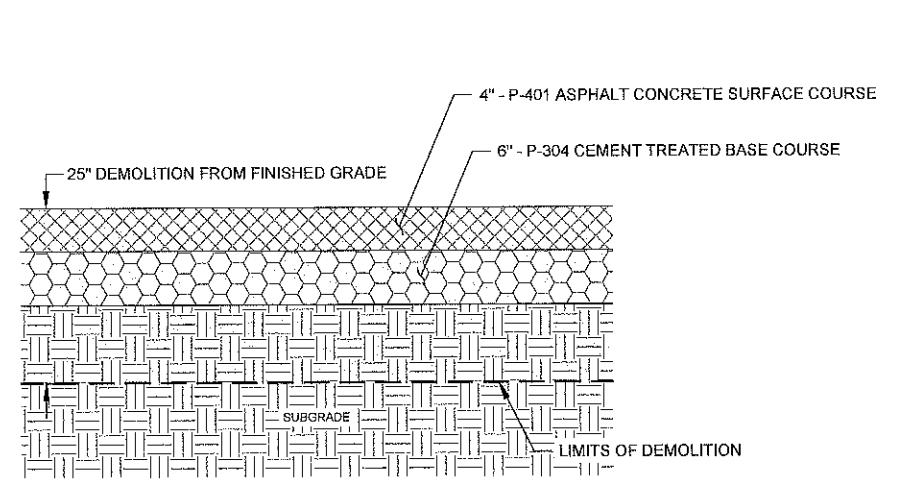
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**1** DEMOLITION SECTION  
NOT TO SCALE



**2** DEMOLITION SECTION  
NOT TO SCALE



**3** DEMOLITION SECTION  
NOT TO SCALE

**AUGUSTA REGIONAL AIRPORT  
CONSTRUCT TAXIWAY G**

1501 AVIATION WAY  
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SHEET CONTENTS  
TYPICAL DEMOLITION  
SECTIONS

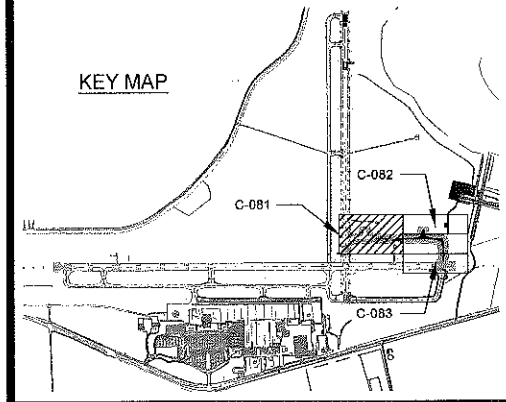
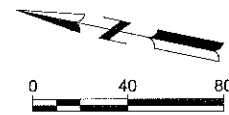
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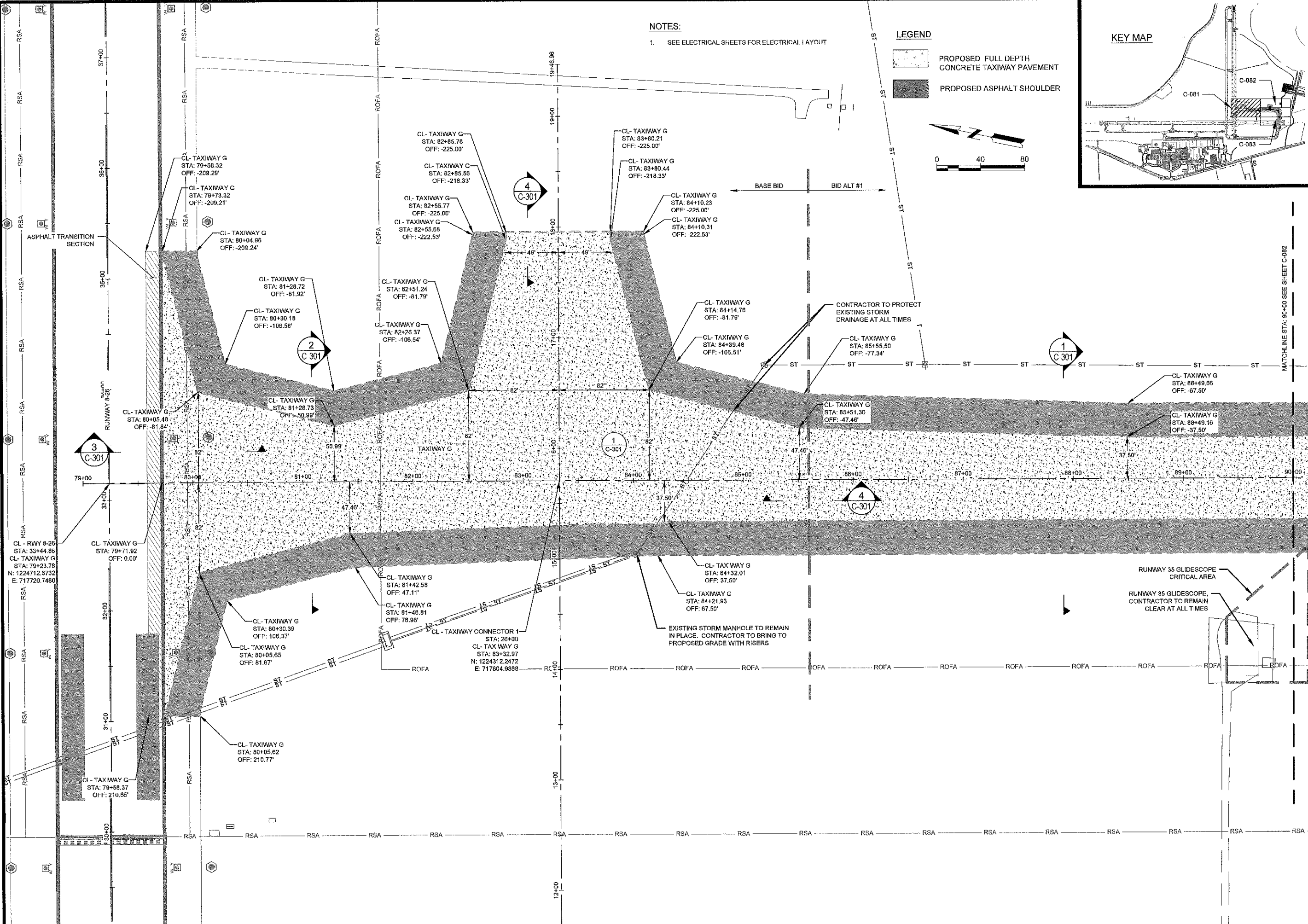
NOTES:  
1. SEE ELECTRICAL SHEETS FOR ELECTRICAL LAYOUT.

LEGEND  
[Pattern] PROPOSED FULL DEPTH CONCRETE TAXIWAY PAVEMENT  
[Solid Grey] PROPOSED ASPHALT SHOULDER



**Mead & Hunt**  
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SHEET CONTENTS  
PROJECT  
GEOMETRICS

SHEET NO.

**C-081**

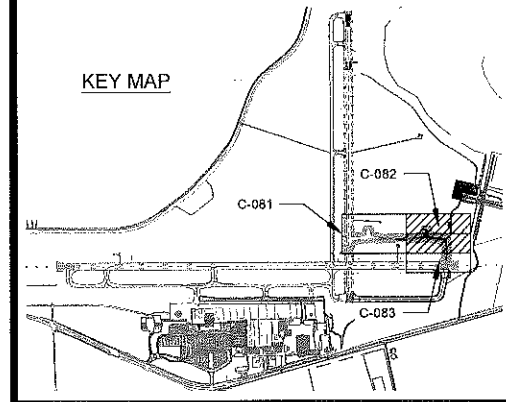
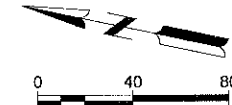


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NOTES:  
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LEGEND

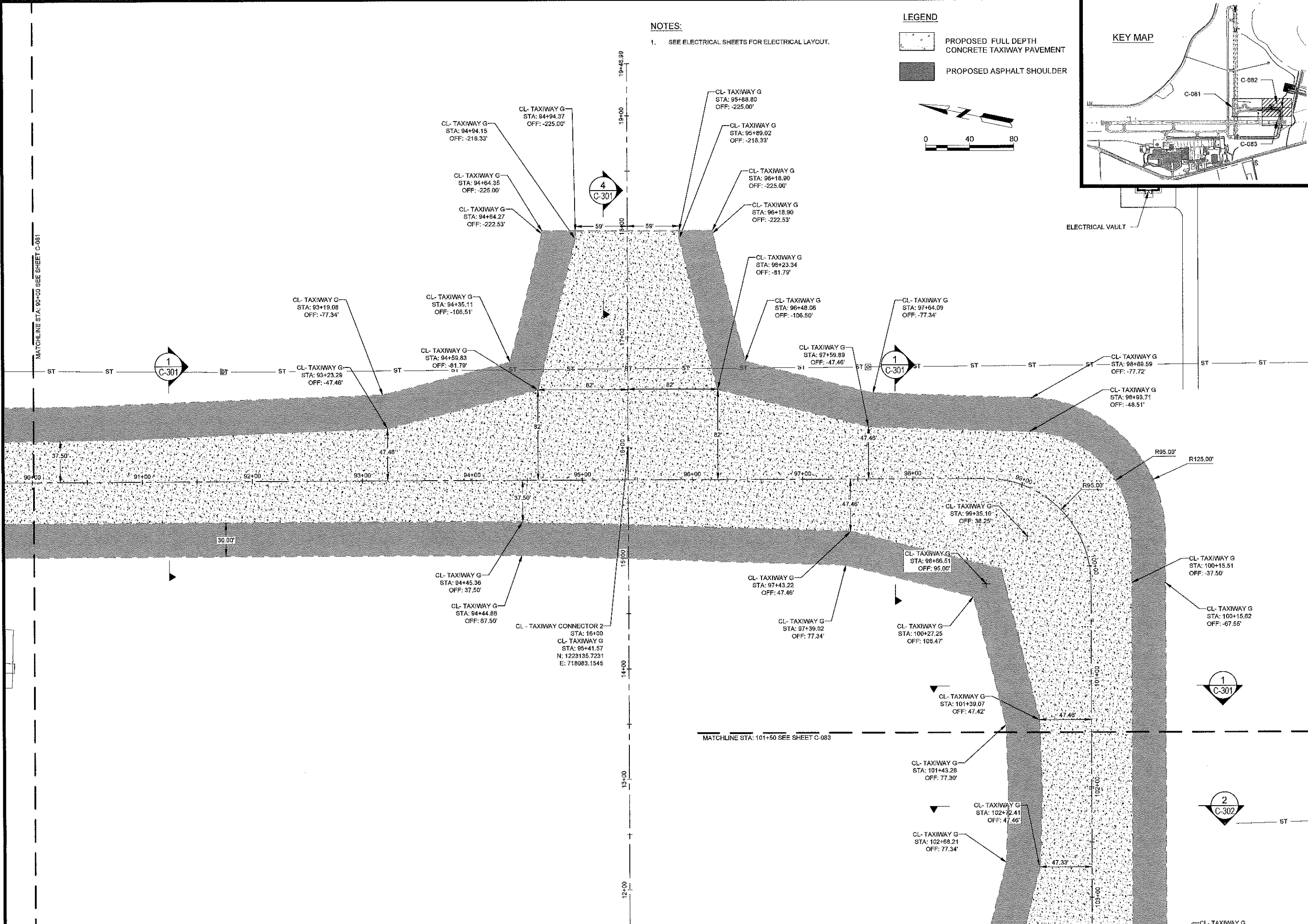
- PROPOSED FULL DEPTH CONCRETE TAXIWAY PAVEMENT
- PROPOSED ASPHALT SHOULDER



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CONSTRUCT TAXIWAY G**

1501 AVIATION WAY  
AUGUSTA, GA 30906-9620

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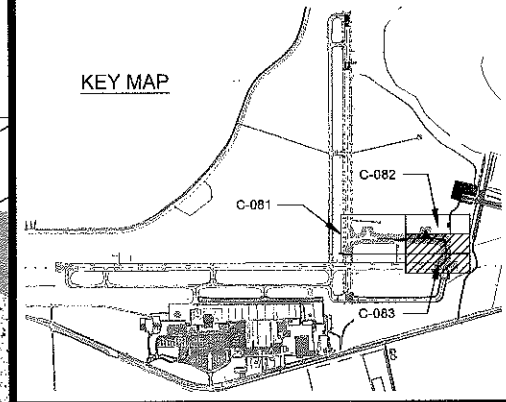
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DATE: DECEMBER 6, 2022  
DESIGNED BY: EJS  
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SHEET CONTENTS  
PROJECT  
GEOMETRICS

SHEET NO

**C-082**

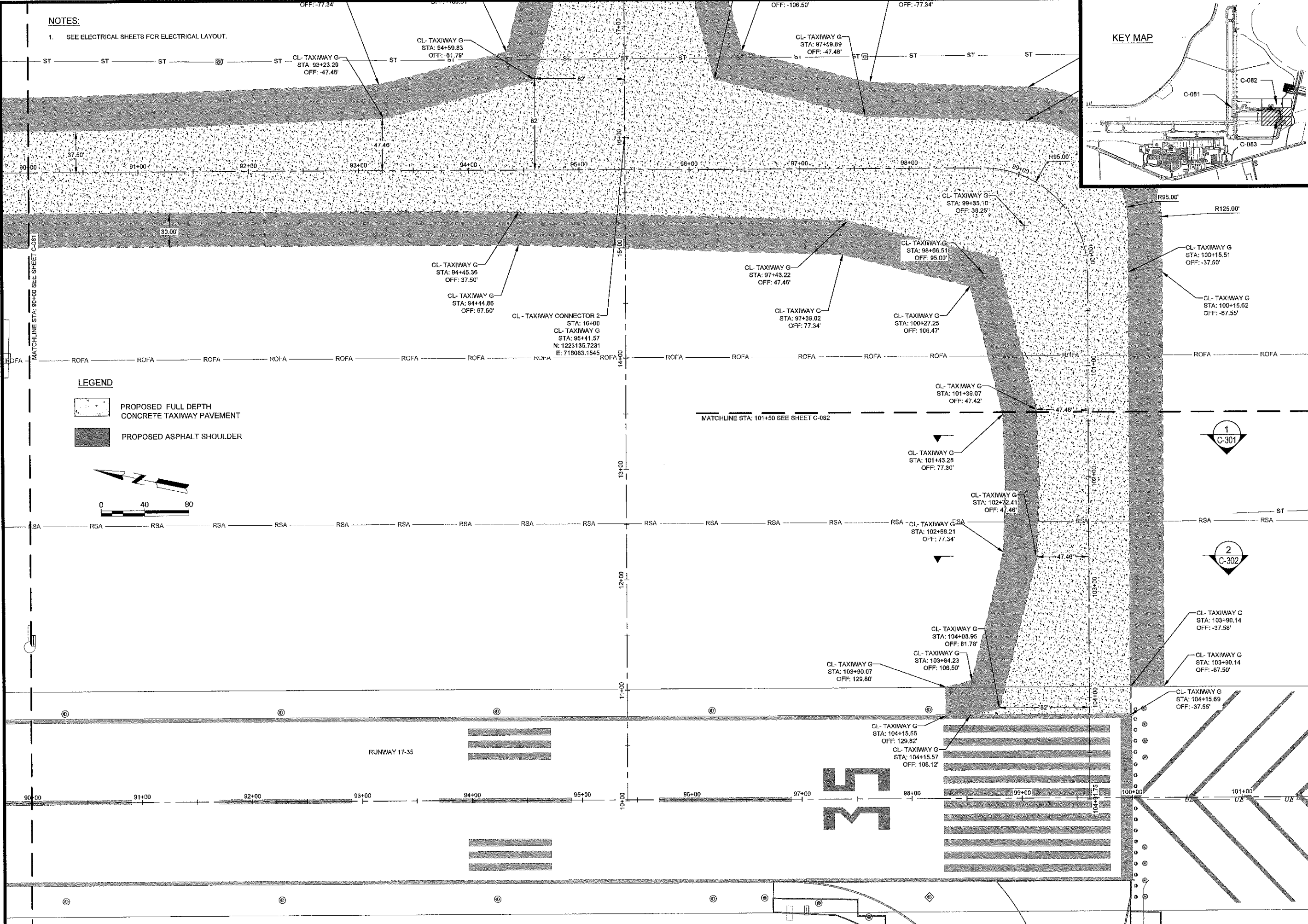
NOTES:  
1. SEE ELECTRICAL SHEETS FOR ELECTRICAL LAYOUT.



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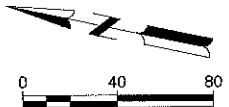
**Augusta Regional Airport**

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**LEGEND**

- PROPOSED FULL DEPTH CONCRETE TAXIWAY PAVEMENT
- PROPOSED ASPHALT SHOULDER



**AUGUSTA REGIONAL AIRPORT  
CONSTRUCT TAXIWAY G**

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SHEET CONTENTS  
PROJECT  
GEOMETRICS

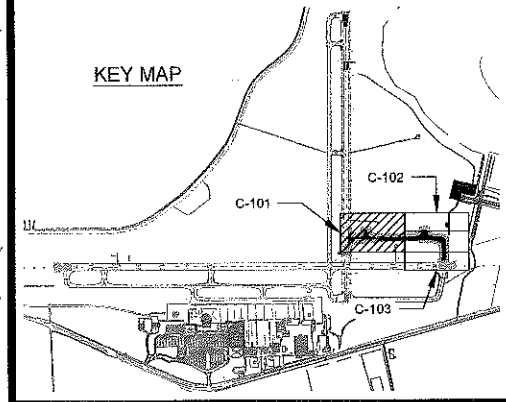
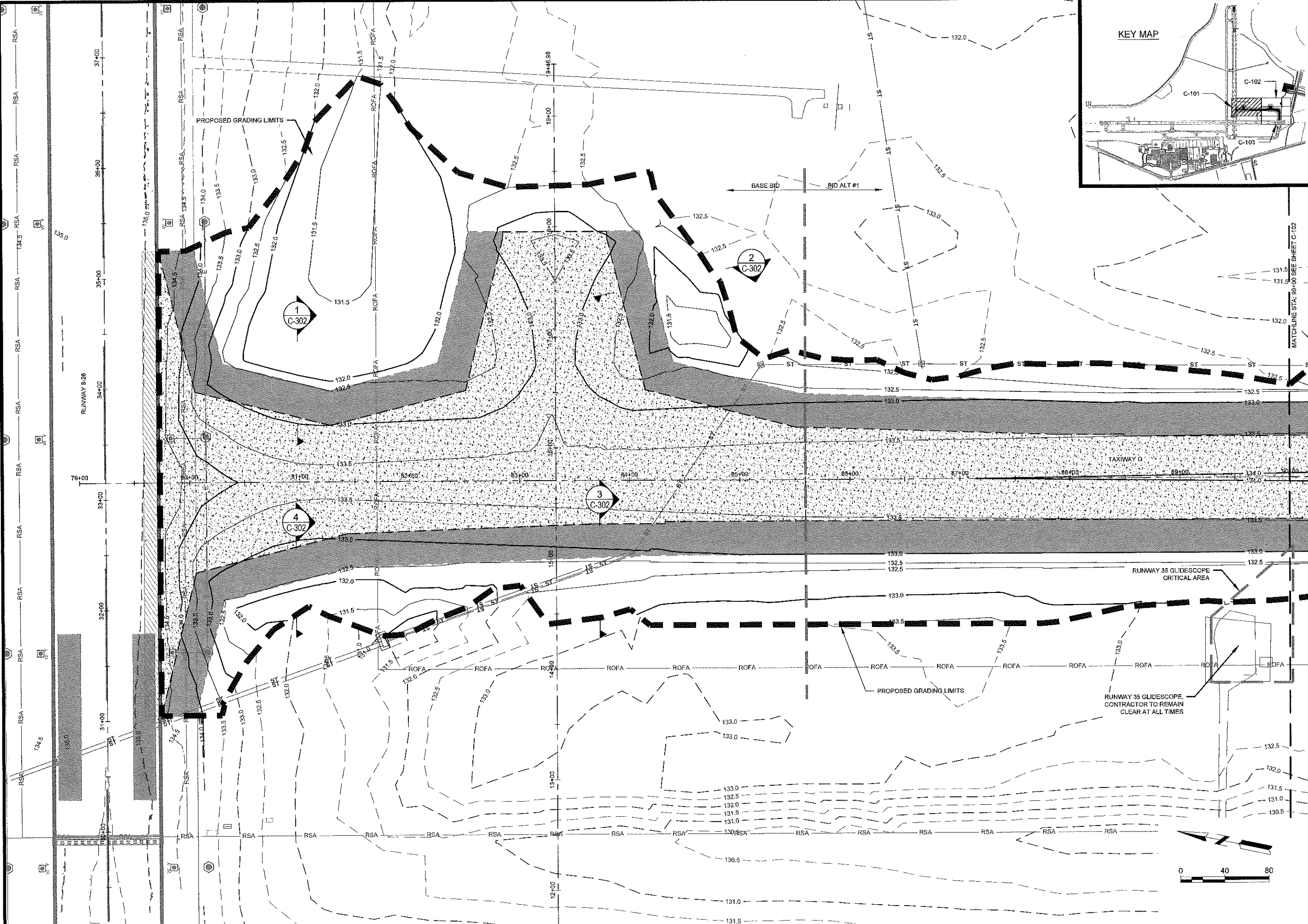
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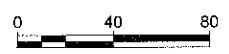
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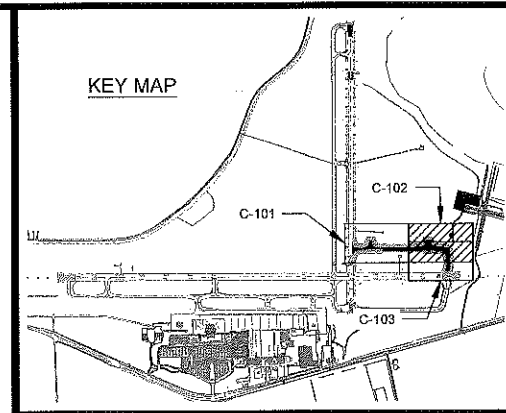
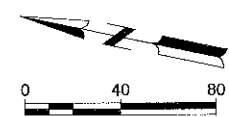
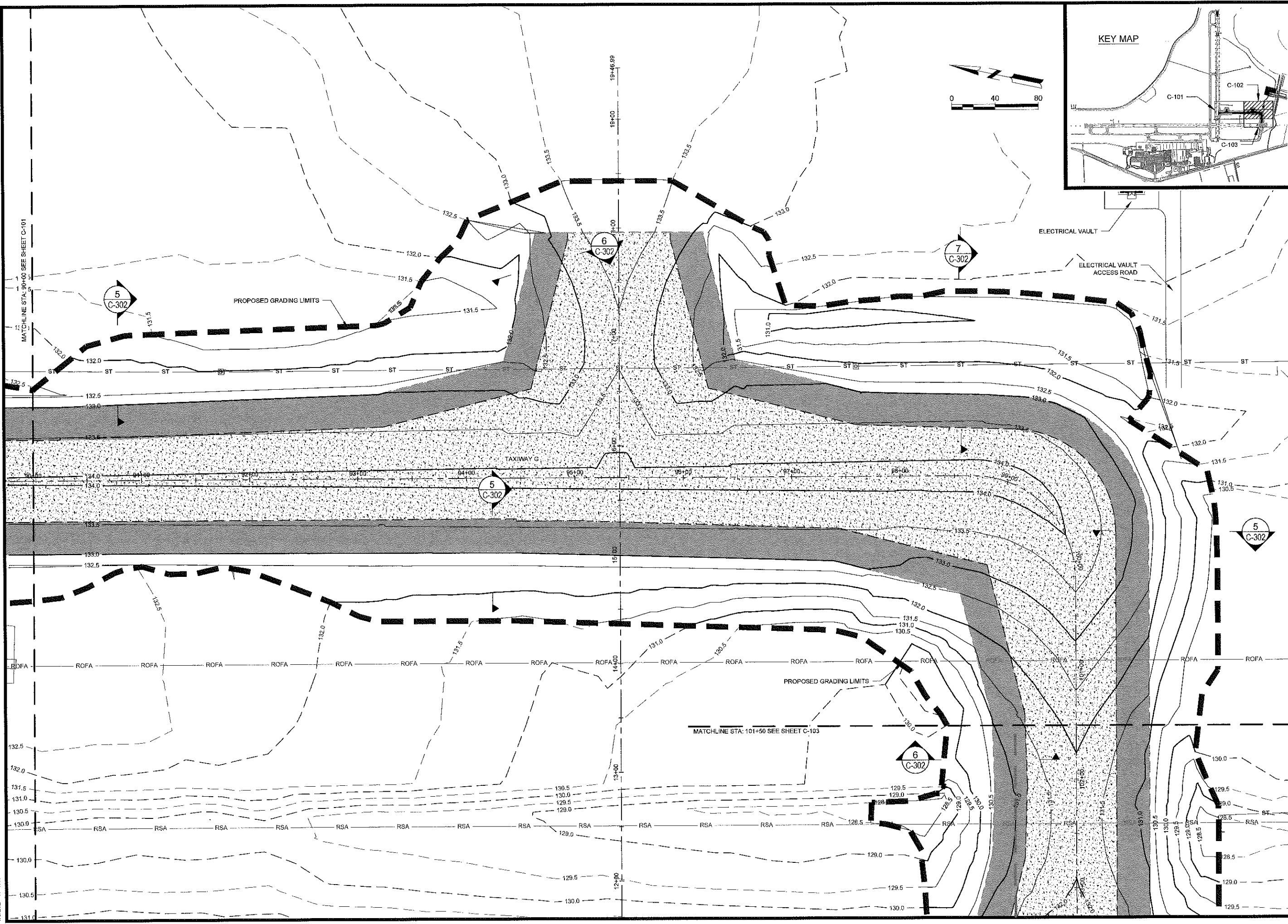
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SHEET NO.

**C-101**



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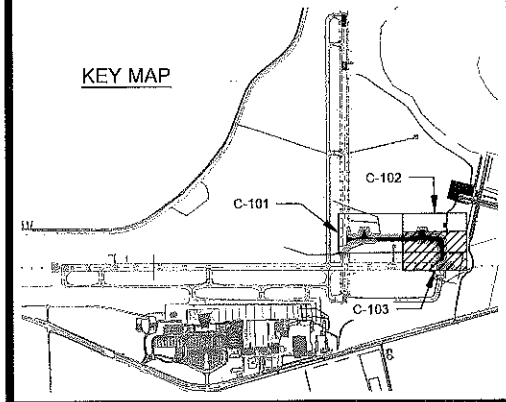
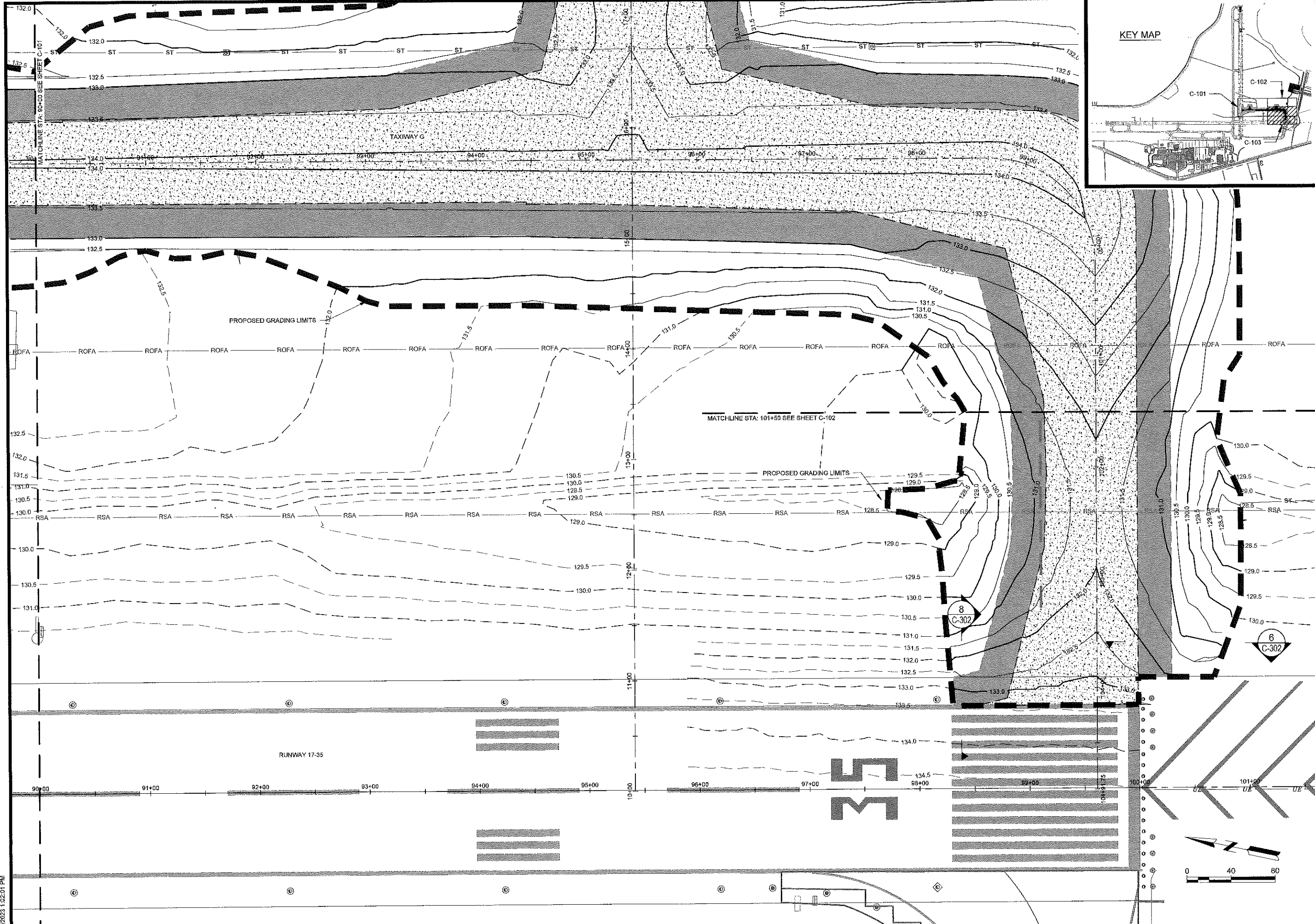
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SHEET NO.

**C-102**



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**AUGUSTA REGIONAL AIRPORT  
 CONSTRUCT TAXIWAY G**

1501 AVIATION WAY  
 AUGUSTA, GA 30906-9620

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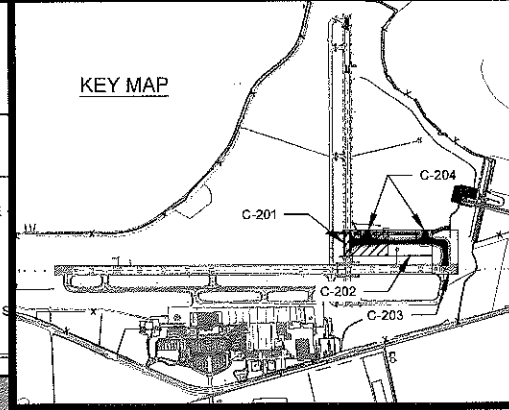
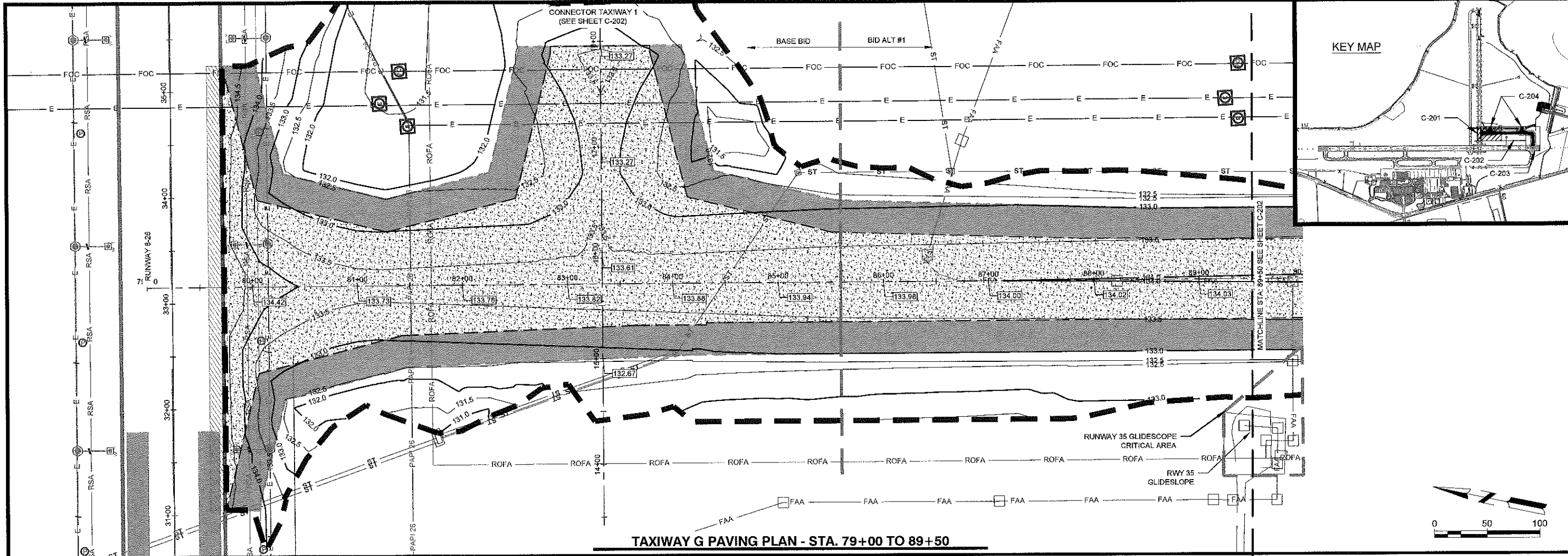
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MR# NO: 0119700-170869.02  
 DATE: DECEMBER 6, 2022  
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SHEET CONTENTS  
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SHEET NO.

**C-103**



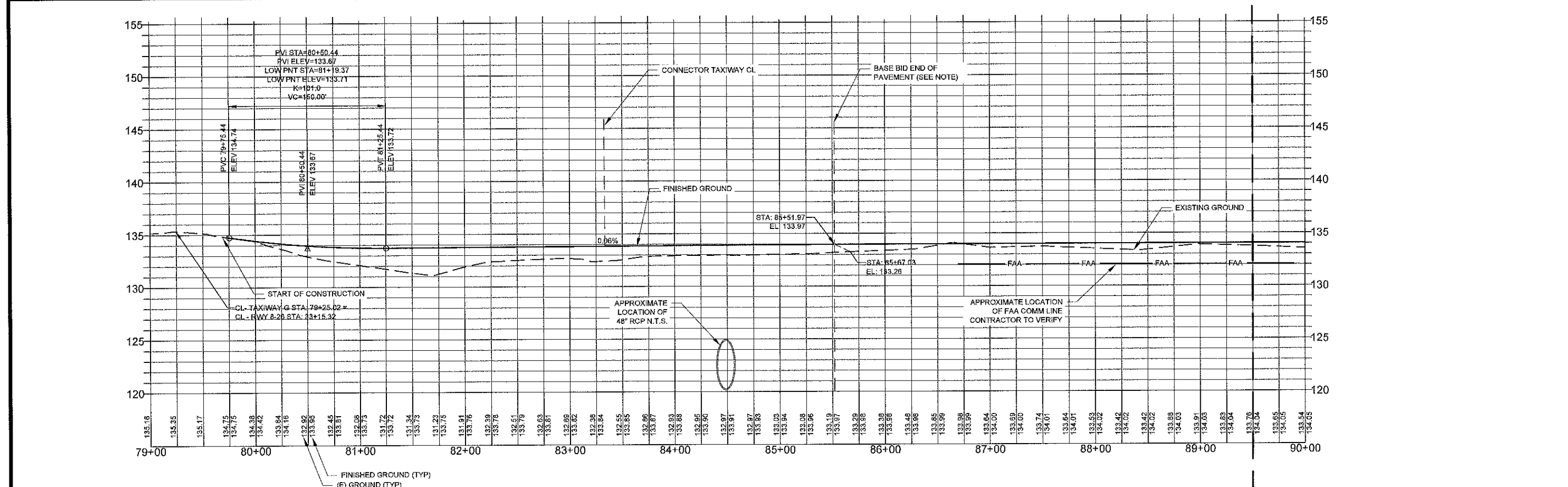
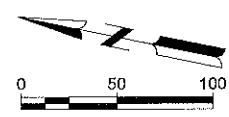
**Mead & Hunt**  
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**AUGUSTA REGIONAL AIRPORT  
 CONSTRUCT TAXIWAY G**

1501 AVIATION WAY  
 AUGUSTA, GA 30906-9620



**TAXIWAY G PAVING PROFILE - STA. 79+00 TO 89+50**

NOTE:  
 IF BID ALT 1 IS NOT AWARDED CONTRACTOR  
 SHALL GRADE FINISHED SURFACE TO DAYLIGHT  
 WITH EXISTING GRADE AT A 5% SLOPE

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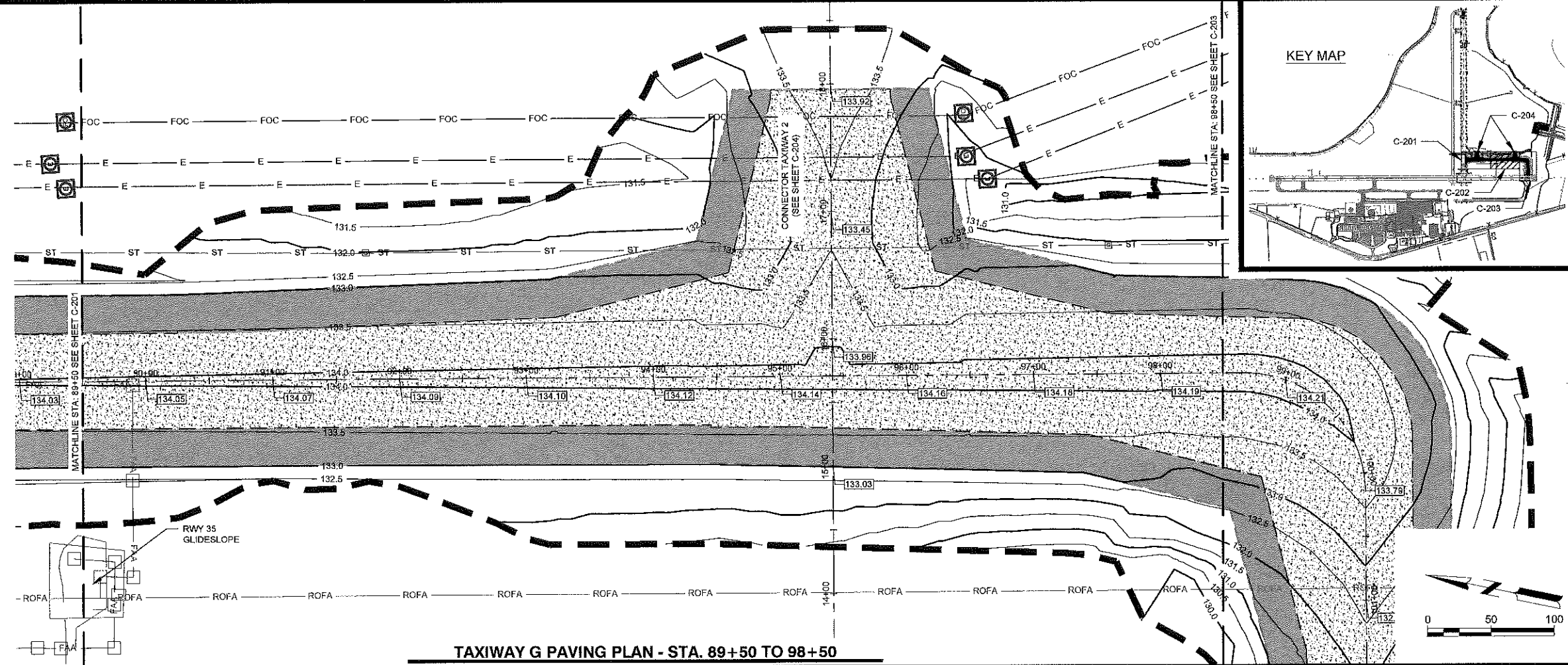
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 79+00-89+50

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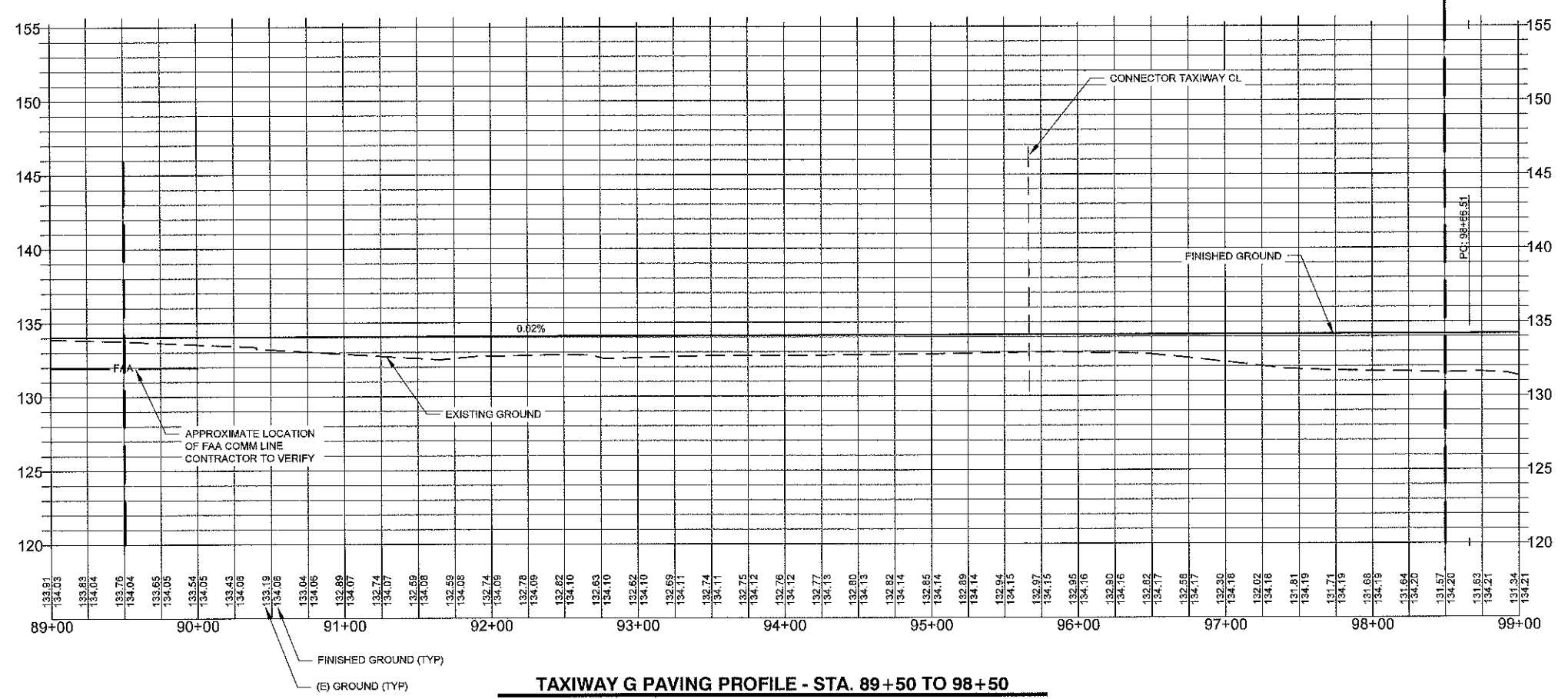
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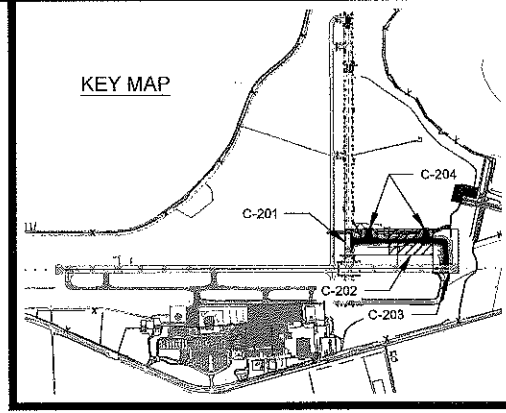




**TAXIWAY G PAVING PLAN - STA. 89+50 TO 98+50**



**TAXIWAY G PAVING PROFILE - STA. 89+50 TO 98+50**



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 CONSTRUCT TAXIWAY G**

1501 AVIATION WAY  
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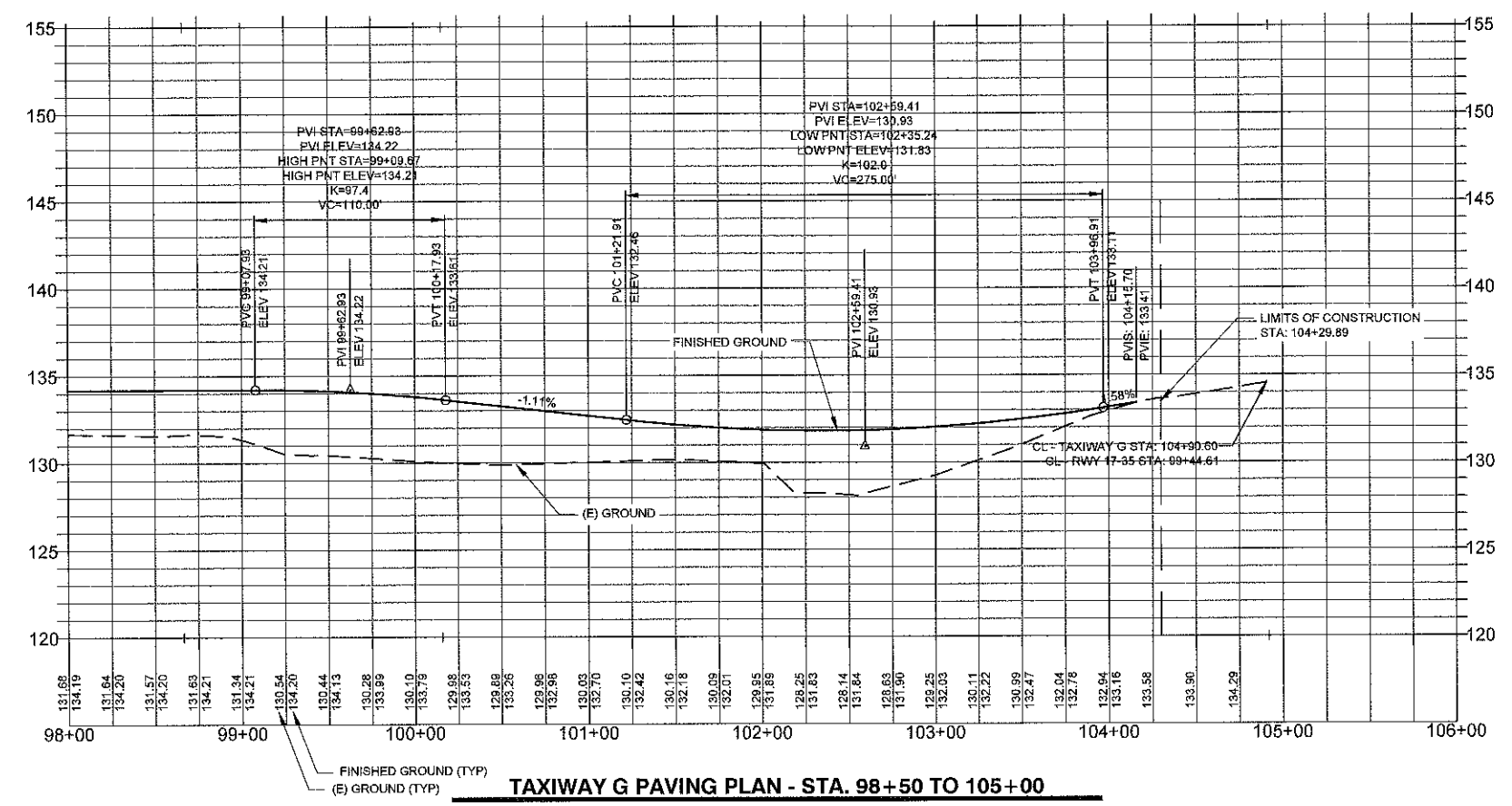
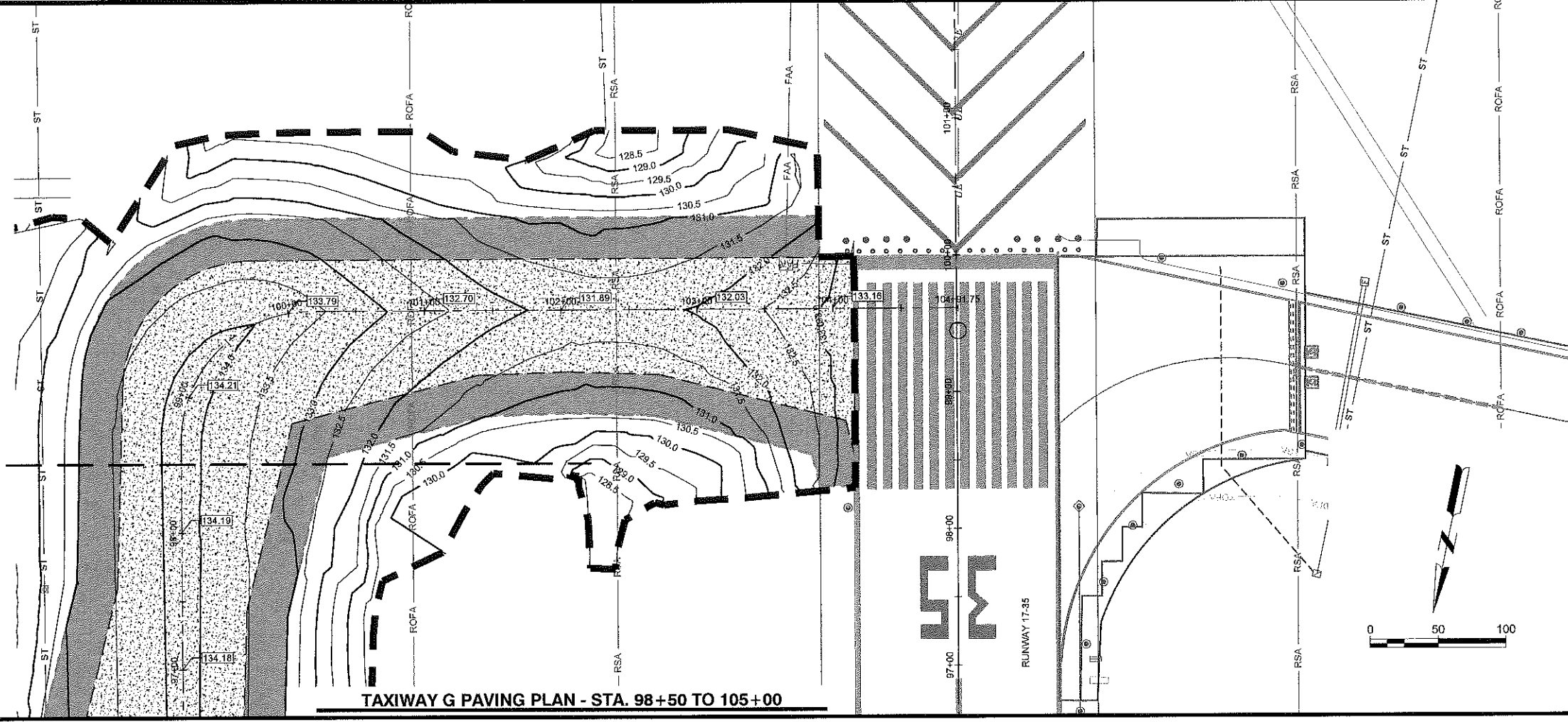
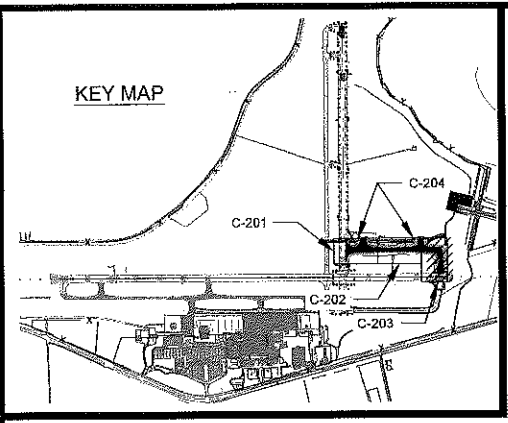
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 CHECKED BY: DES  
DO NOT SCALE DRAWINGS

SHEET CONTENTS  
**PAVING PLAN AND  
 PROFILE - STA  
 89+50-98+50**

SHEET NO.

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CONSTRUCT TAXIWAY G**

1501 AVIATION WAY  
AUGUSTA, GA 30906-9620

REVISION  
ISSUED FOR BID

NOT FOR CONSTRUCTION

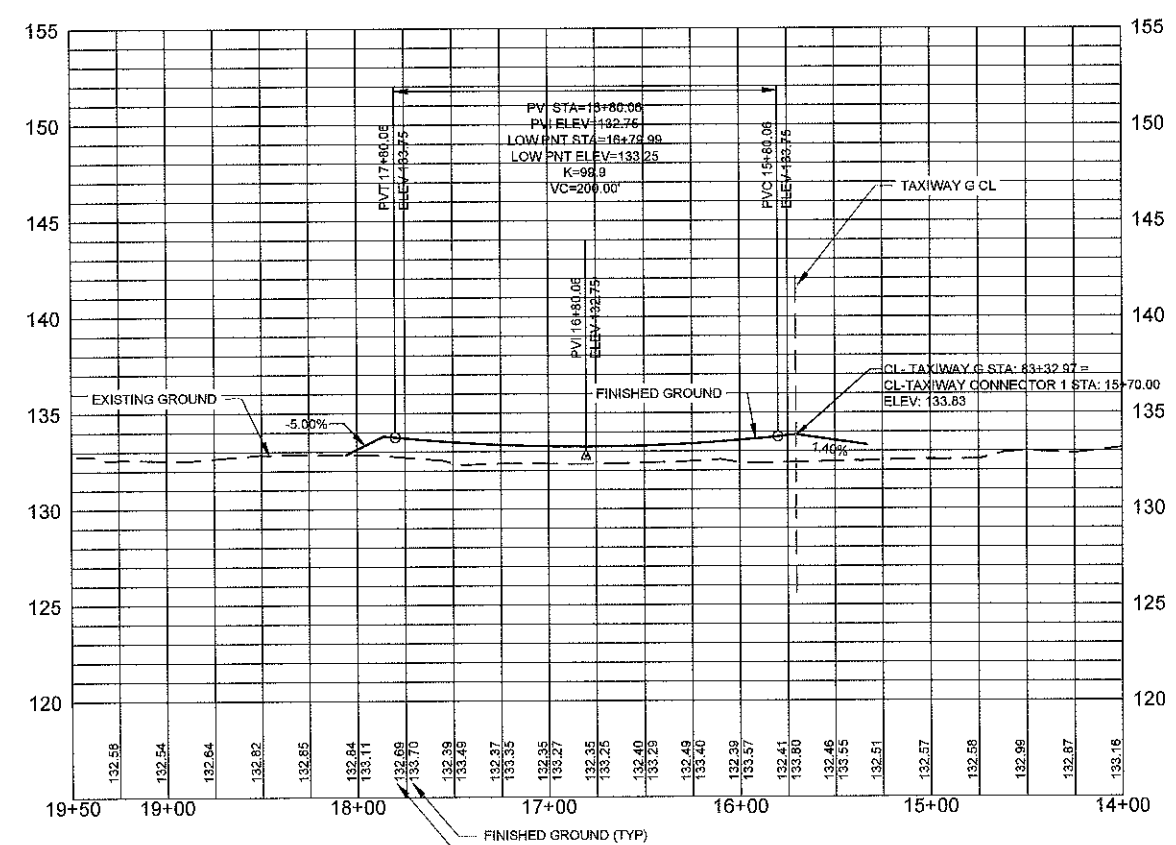
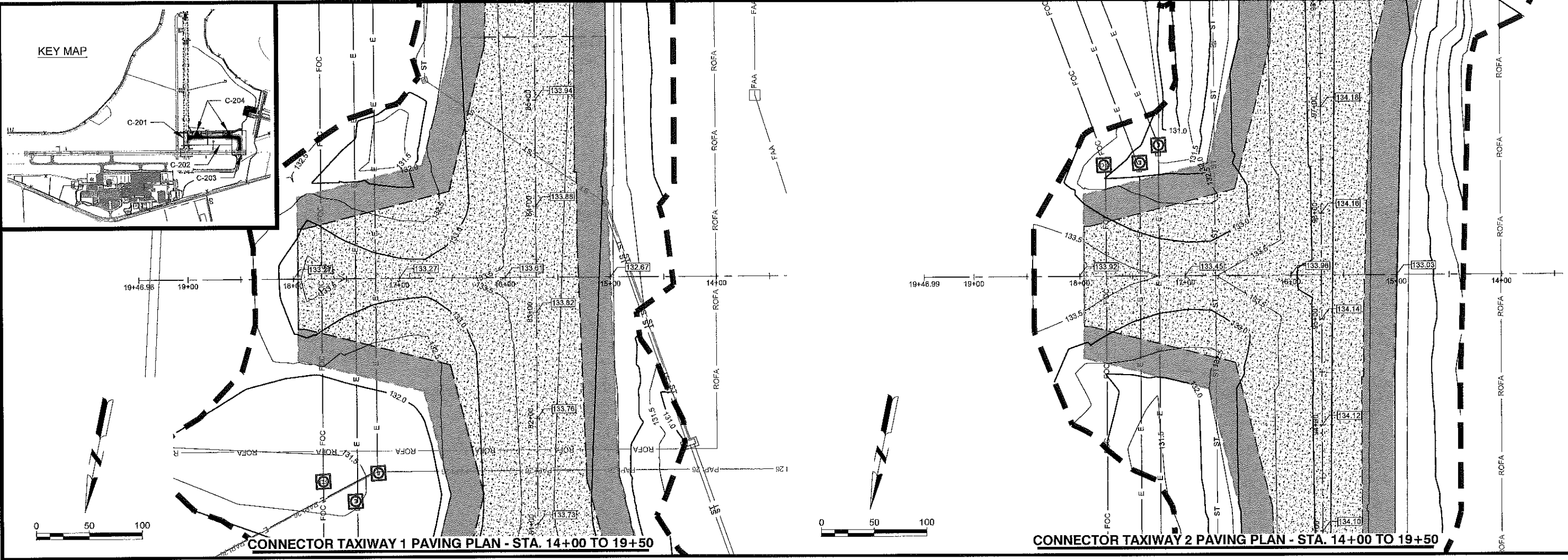
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SHEET CONTENTS  
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98+50-105+00

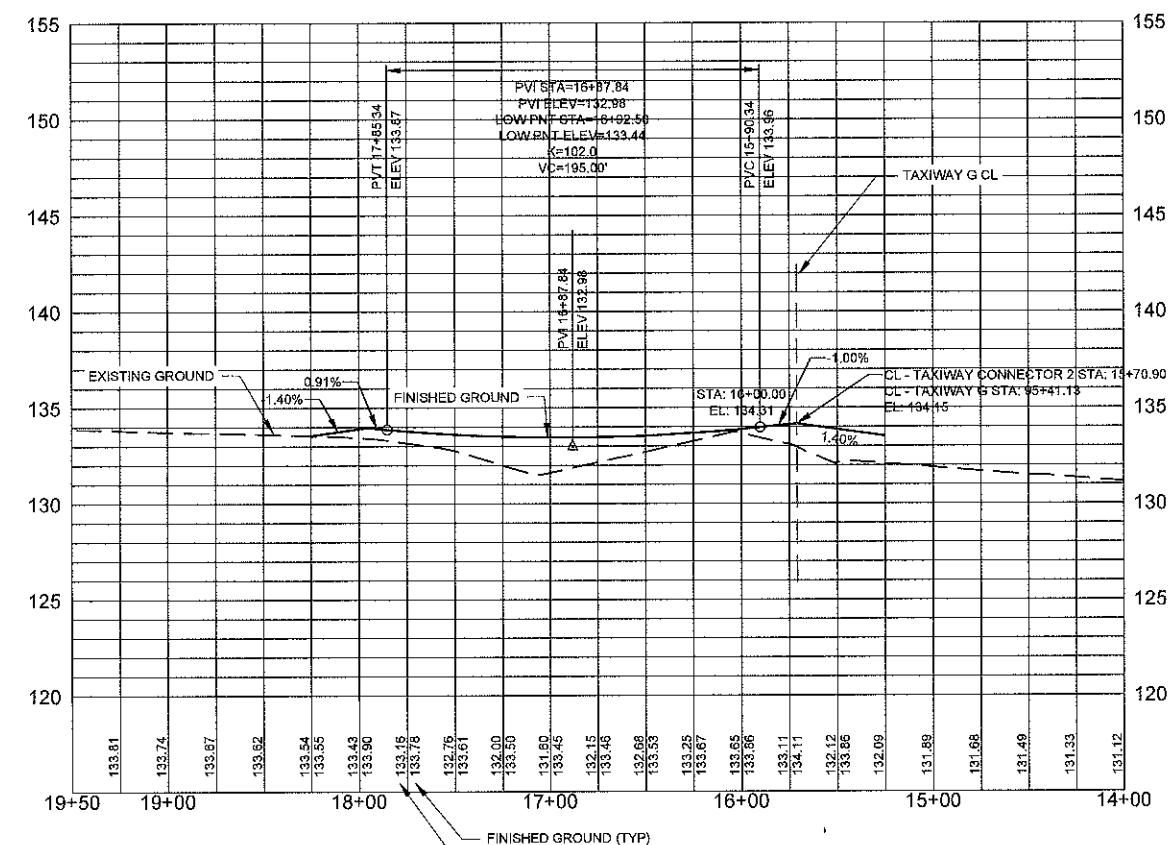
SHEET NO

**C-203**





CONNECTOR TAXIWAY 1 PAVING PROFILE - STA. 14+00 TO 19+50



CONNECTOR TAXIWAY 2 PAVING PROFILE - STA. 14+00 TO 19+50

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SHEET CONTENTS  
 PAVING PLAN AND  
 PROFILE -  
 CONNECTOR  
 TAXIWAYS

SHEET NO.

**C-204**

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1501 AVIATION WAY  
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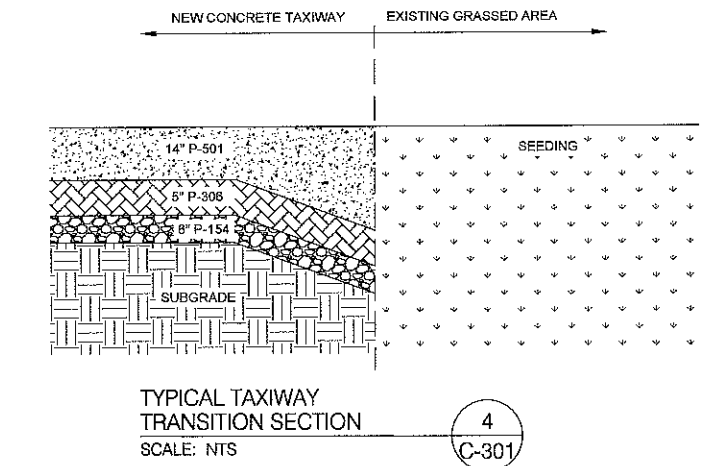
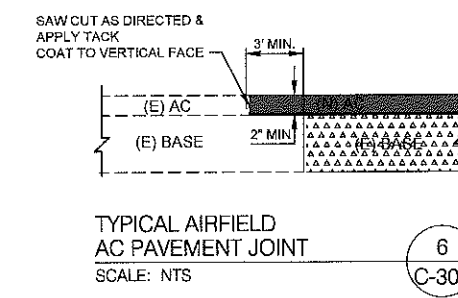
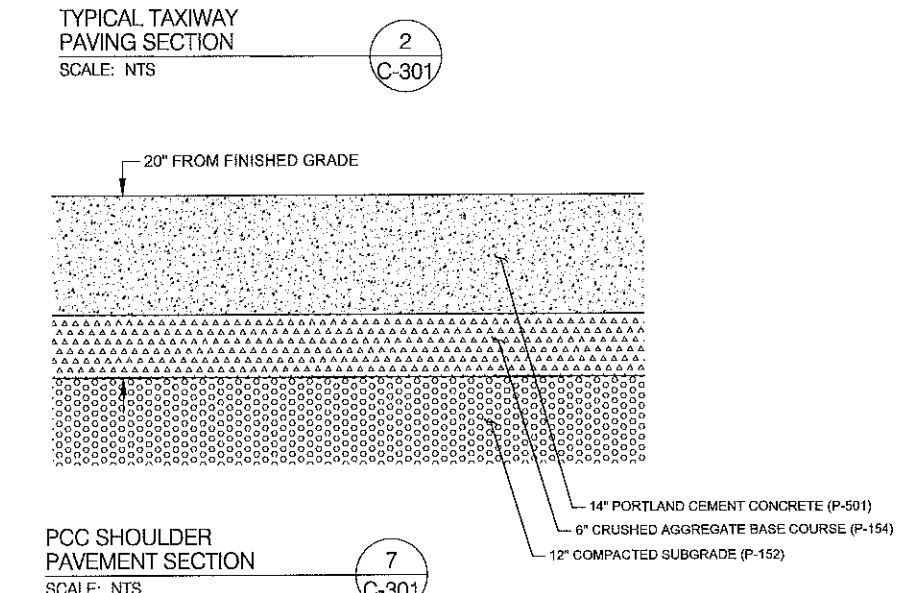
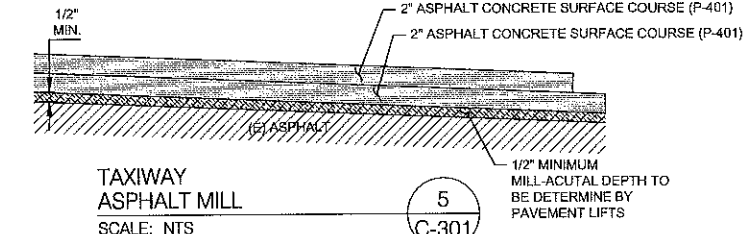
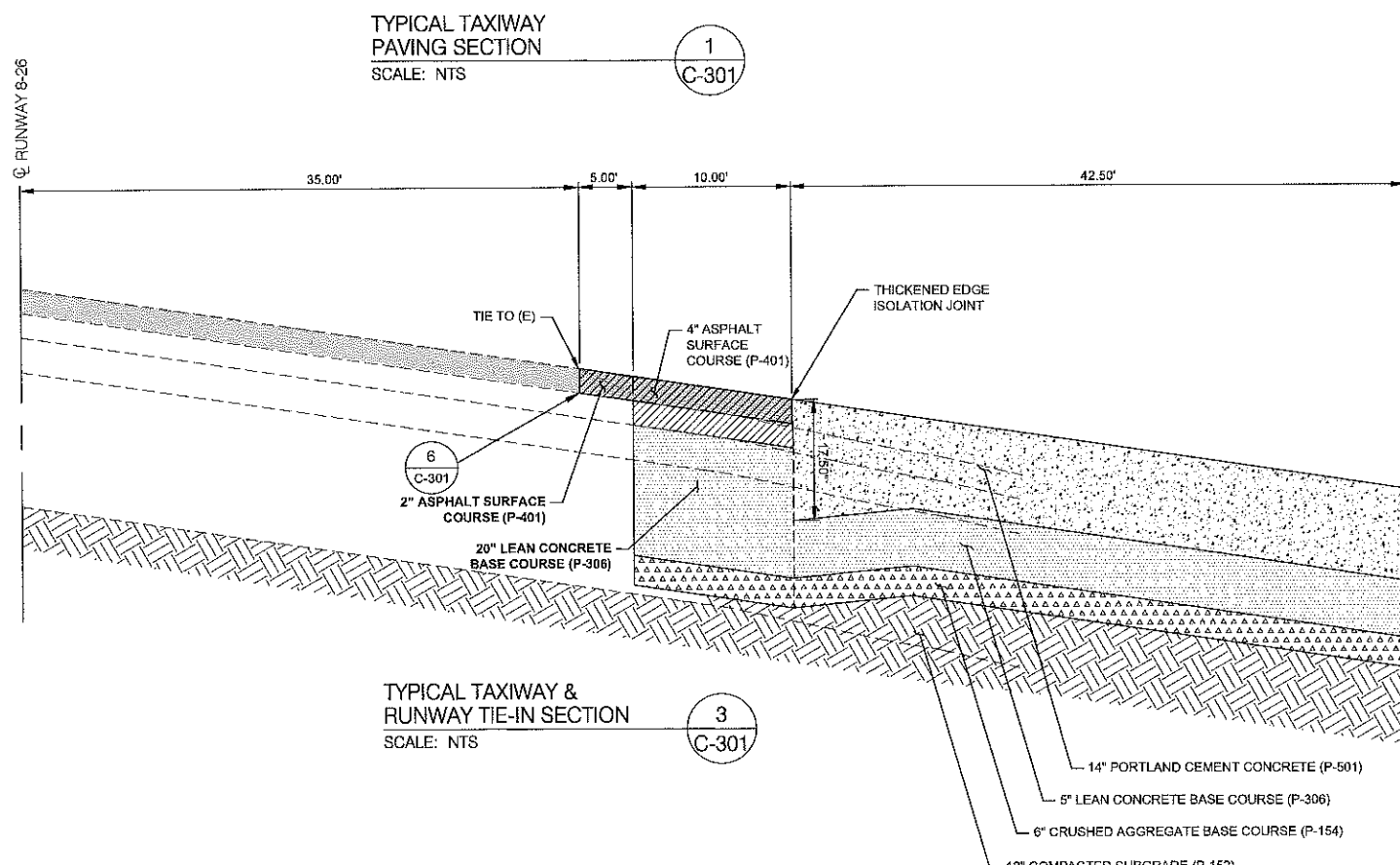
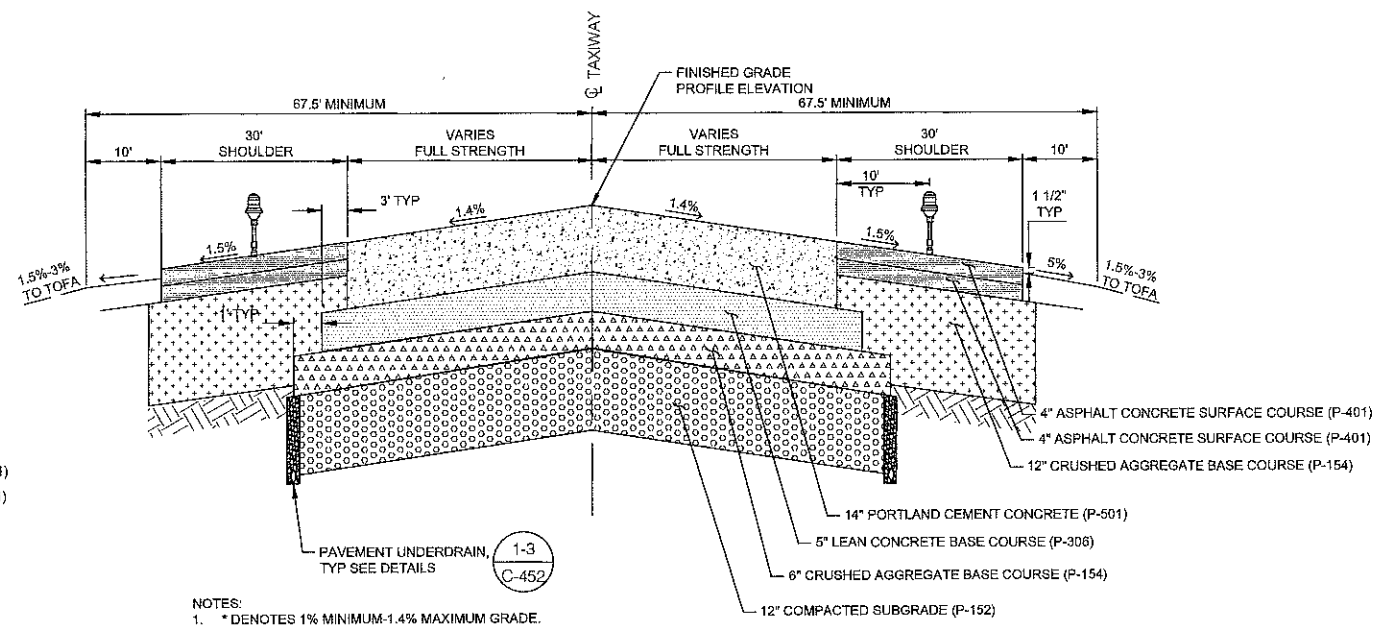
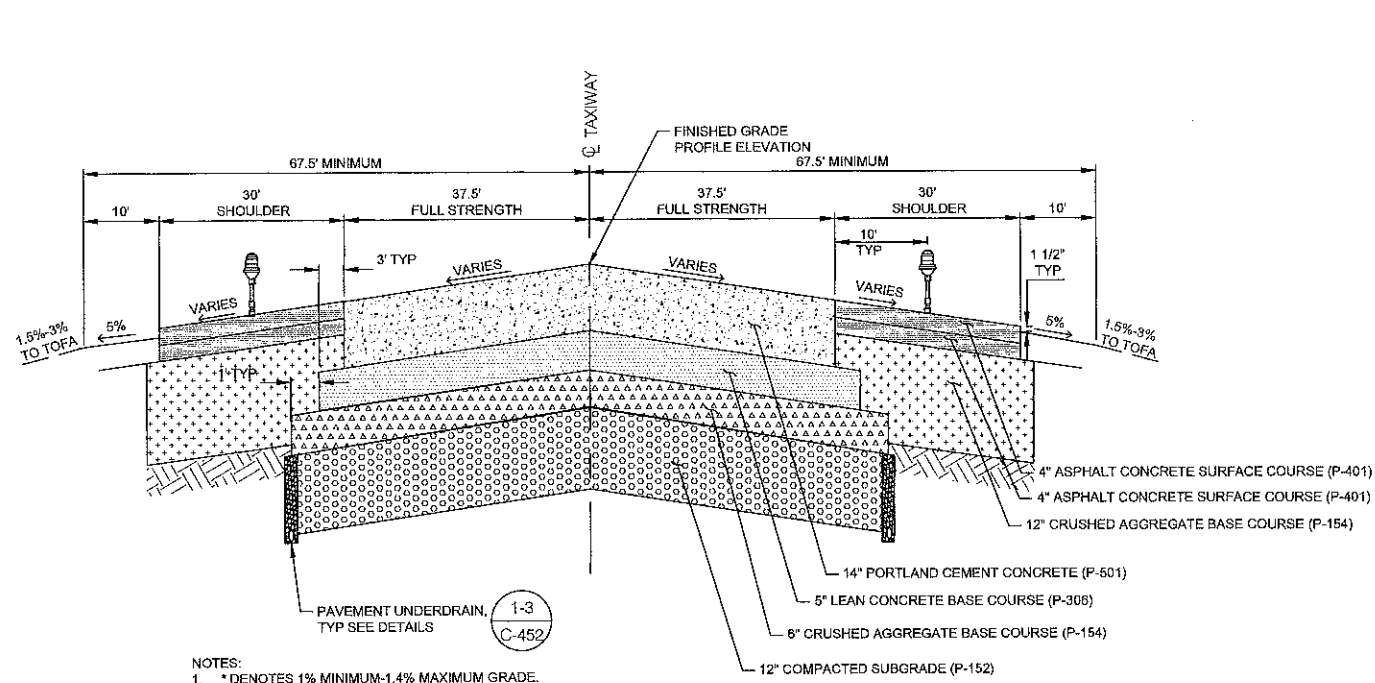
NOT FOR CONSTRUCTION

ME# NO. 0119700-170869.02  
DATE: DECEMBER 6, 2022  
DESIGNED BY: EJS  
DRAWN BY: NJH  
CHECKED BY: DES  
DO NOT SCALE DRAWINGS

SHEET CONTENTS  
TYPICAL SECTIONS

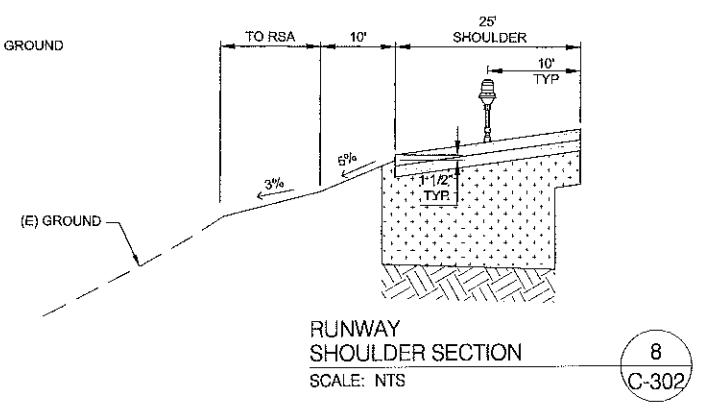
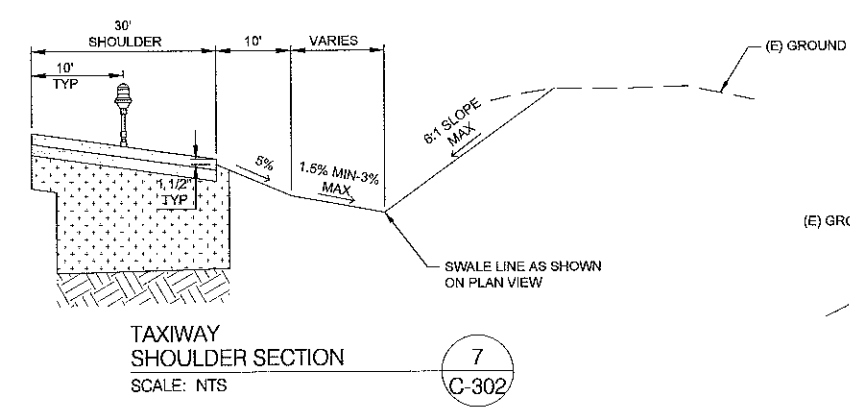
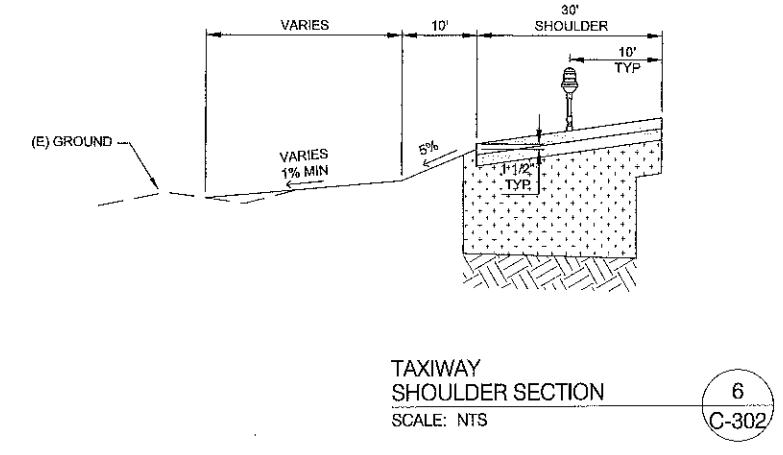
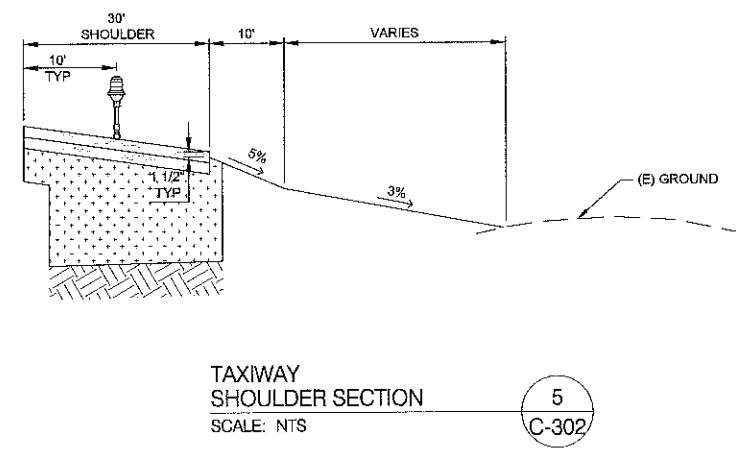
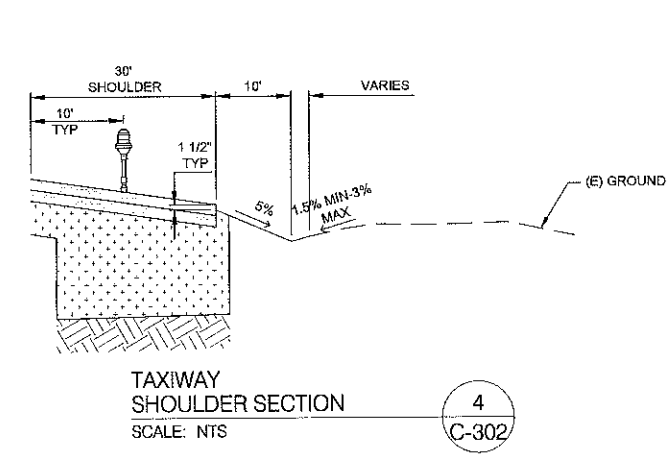
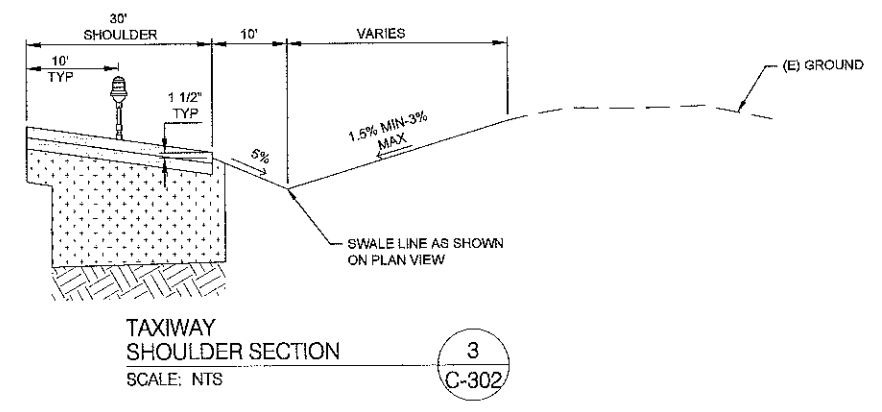
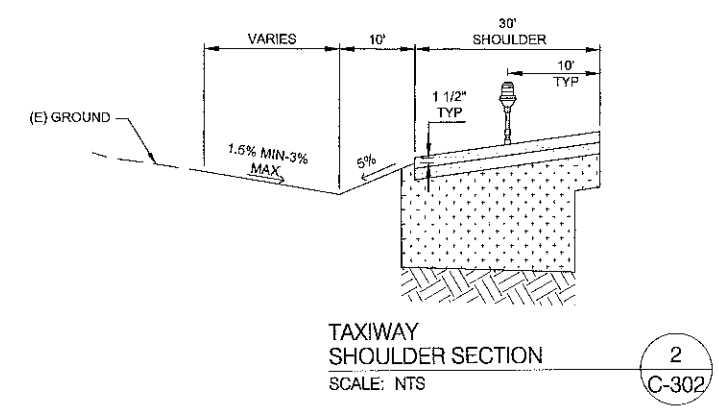
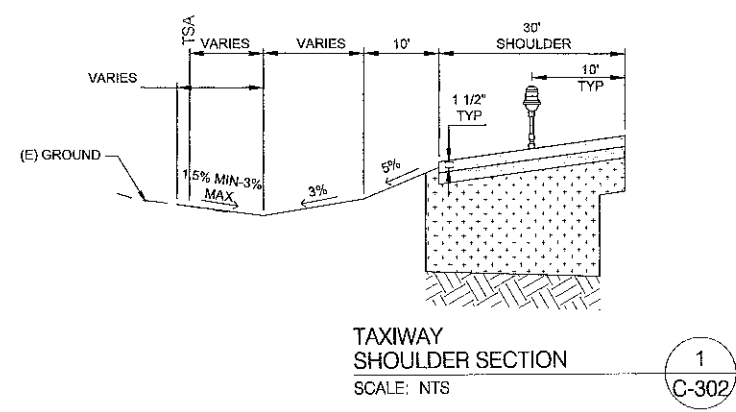
SHEET NO.

**C-301**





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**AUGUSTA REGIONAL AIRPORT  
 CONSTRUCT TAXIWAY G**

1501 AVIATION WAY  
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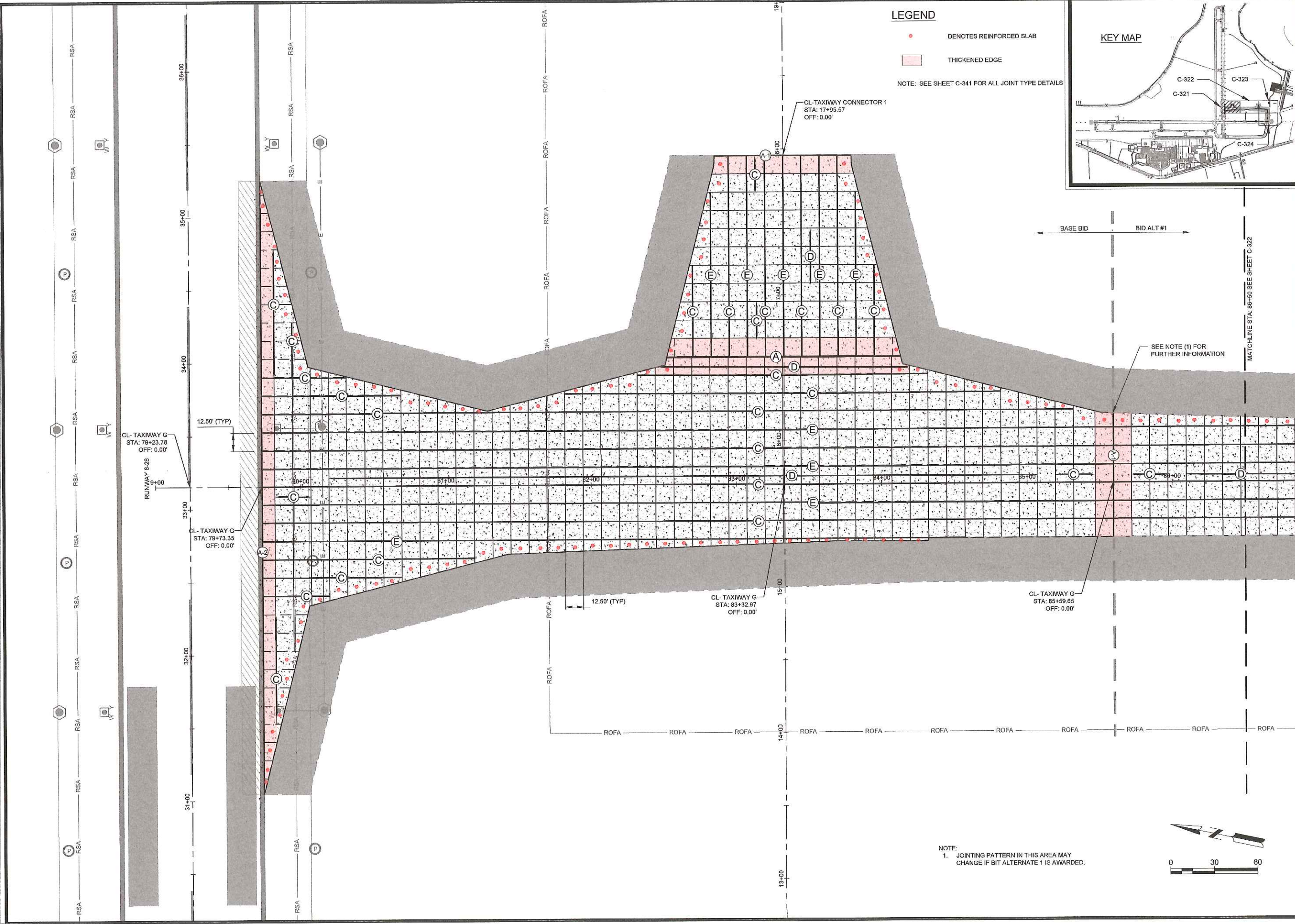
SHEET CONTENTS  
 TYPICAL SECTIONS

SHEET NO.

**C-302**



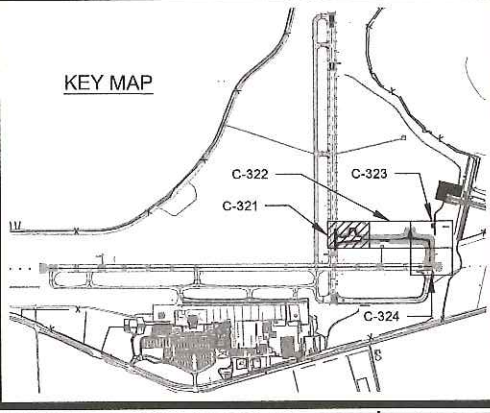
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1/6/2023 2:16:33 PM



**LEGEND**

- DENOTES REINFORCED SLAB
- THICKENED EDGE

NOTE: SEE SHEET C-341 FOR ALL JOINT TYPE DETAILS



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SHEET CONTENTS  
JOINTING PLAN

SHEET NO.

**C-321**

NOTE:  
1. JOINTING PATTERN IN THIS AREA MAY CHANGE IF BID ALTERNATE 1 IS AWARDED.



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**AUGUSTA REGIONAL AIRPORT  
CONSTRUCT TAXIWAY G**

1501 AVIATION WAY  
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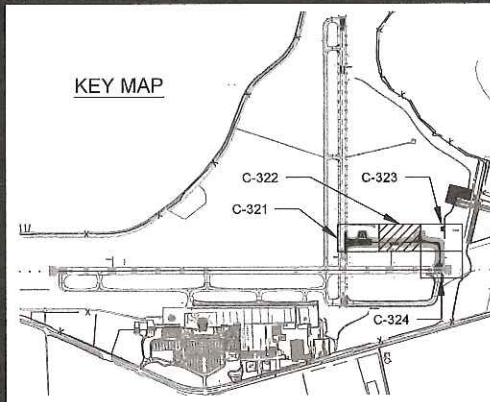
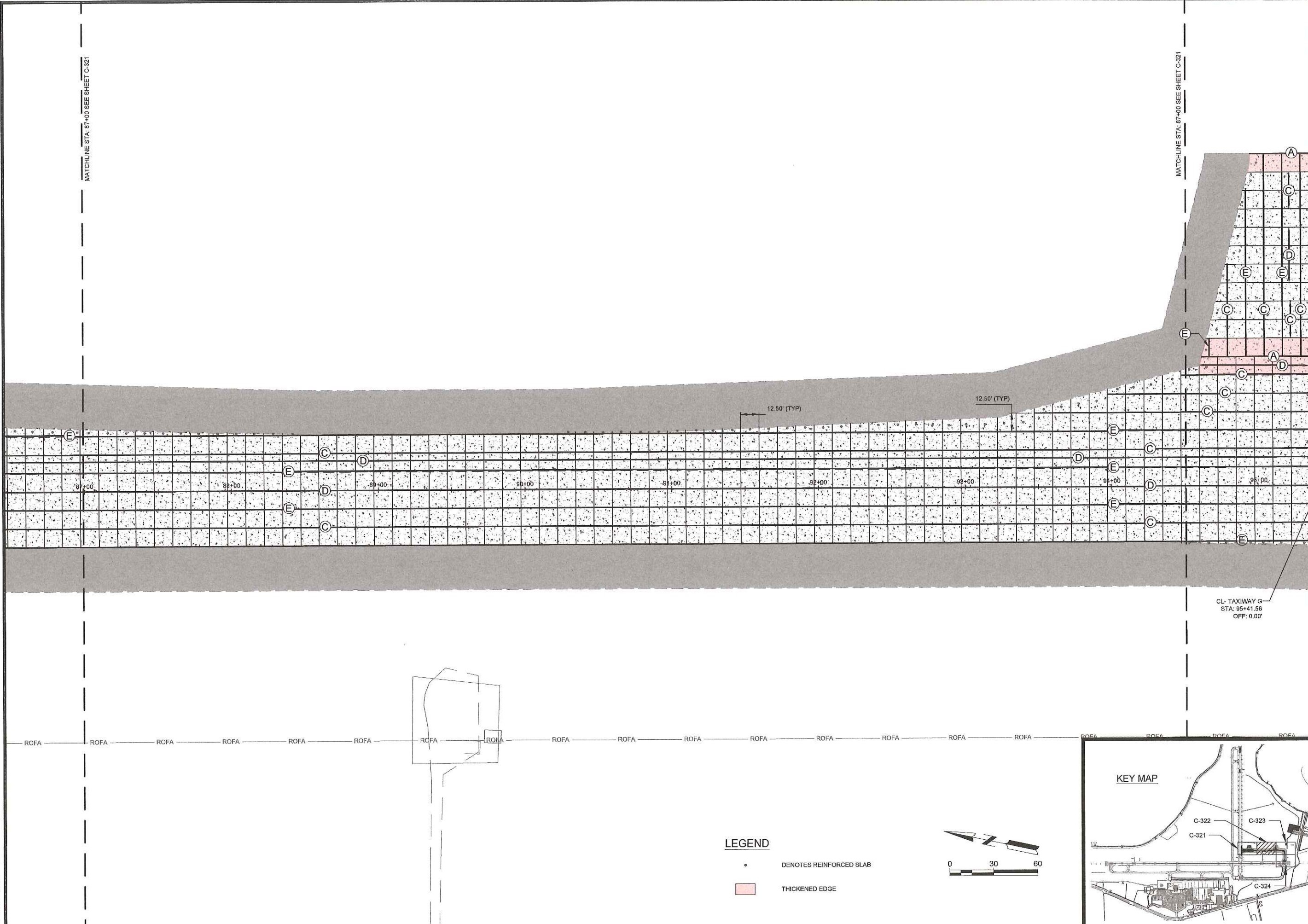
NOT FOR CONSTRUCTION

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SHEET CONTENTS:  
JOINTING PLAN

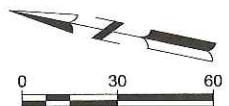
SHEET NO.

**C-322**



**LEGEND**

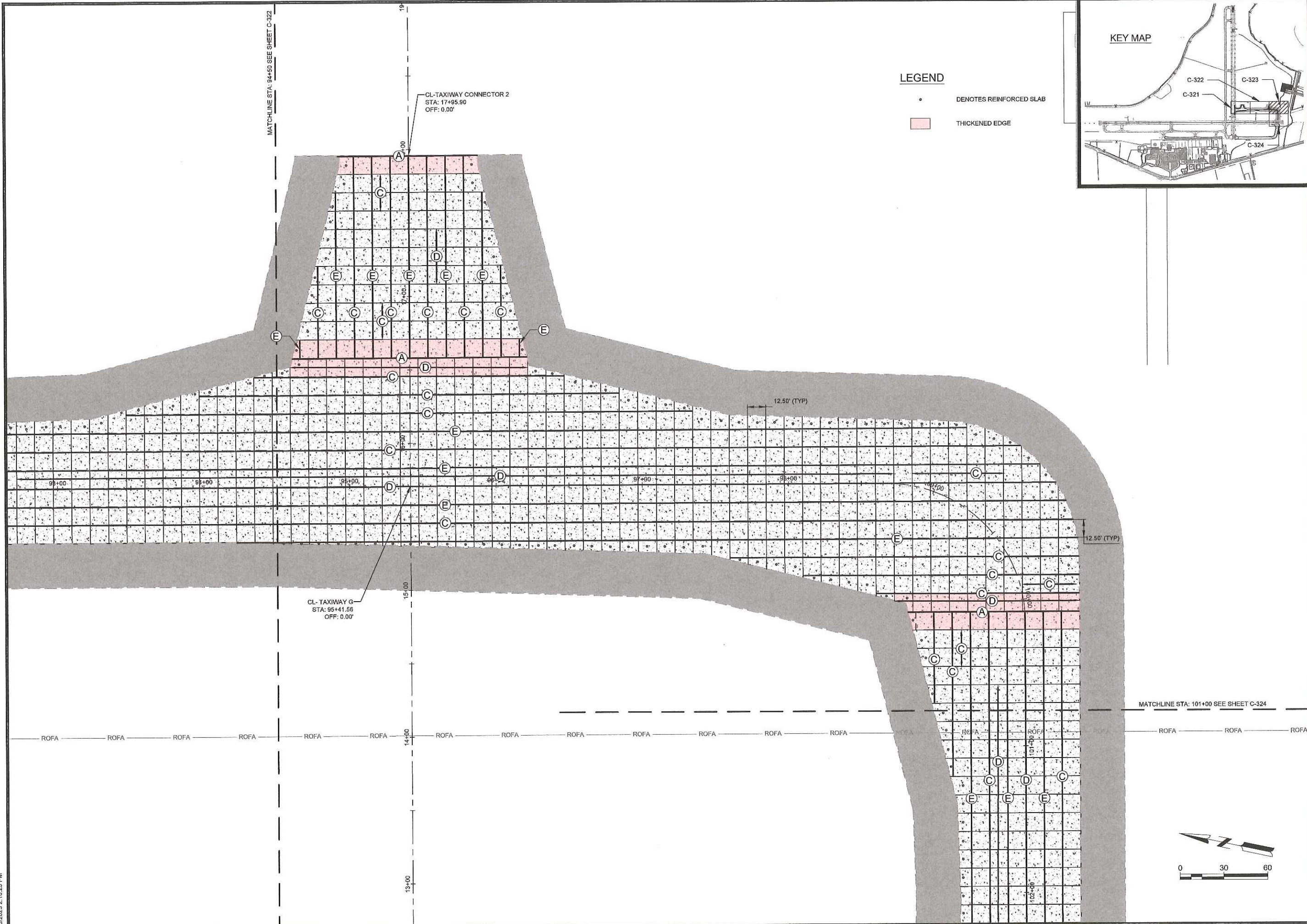
- DENOTES REINFORCED SLAB
- THICKENED EDGE



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1/6/2023 2:17:24 PM



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1/6/2023 2:18:23 PM



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CONSTRUCT TAXIWAY G**

1501 AVIATION WAY  
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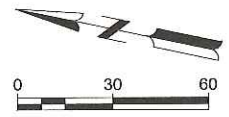
**NOT FOR CONSTRUCTION**

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DATE: DECEMBER 6, 2022  
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SHEET CONTENTS  
JOINTING PLAN

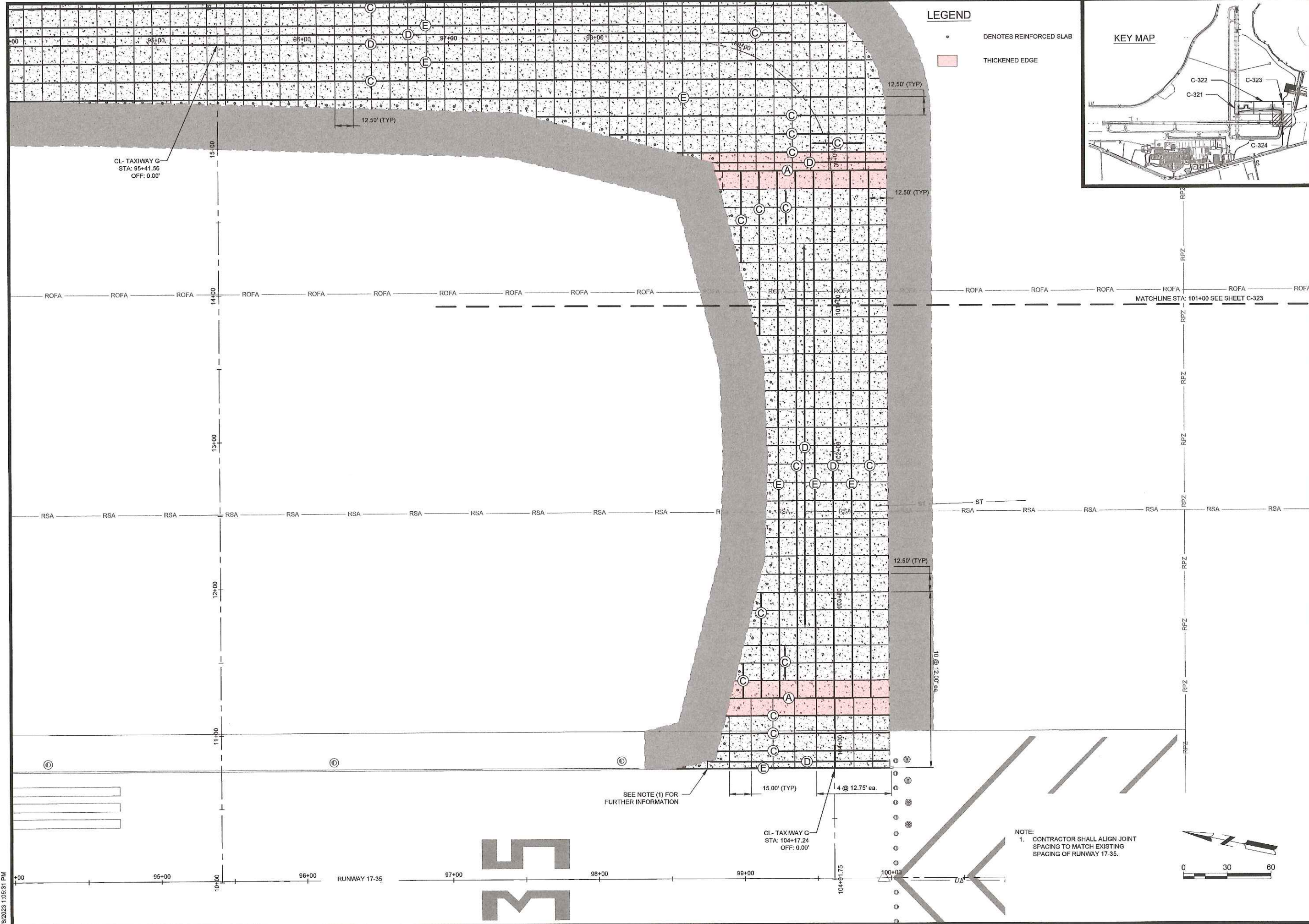
SHEET NO.

**C-323**

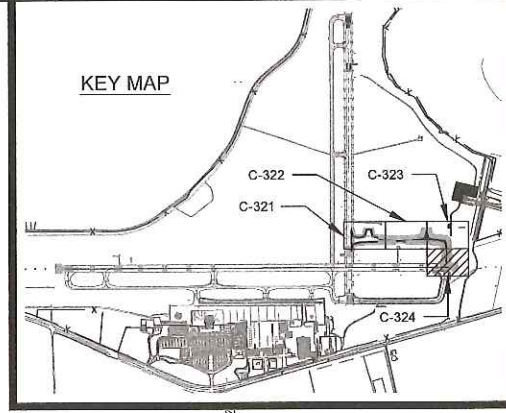




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**LEGEND**  
 • DENOTES REINFORCED SLAB  
 [Pink Box] THICKENED EDGE



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SHEET CONTENTS  
 JOINTING PLAN

SHEET NO.

**C-324**

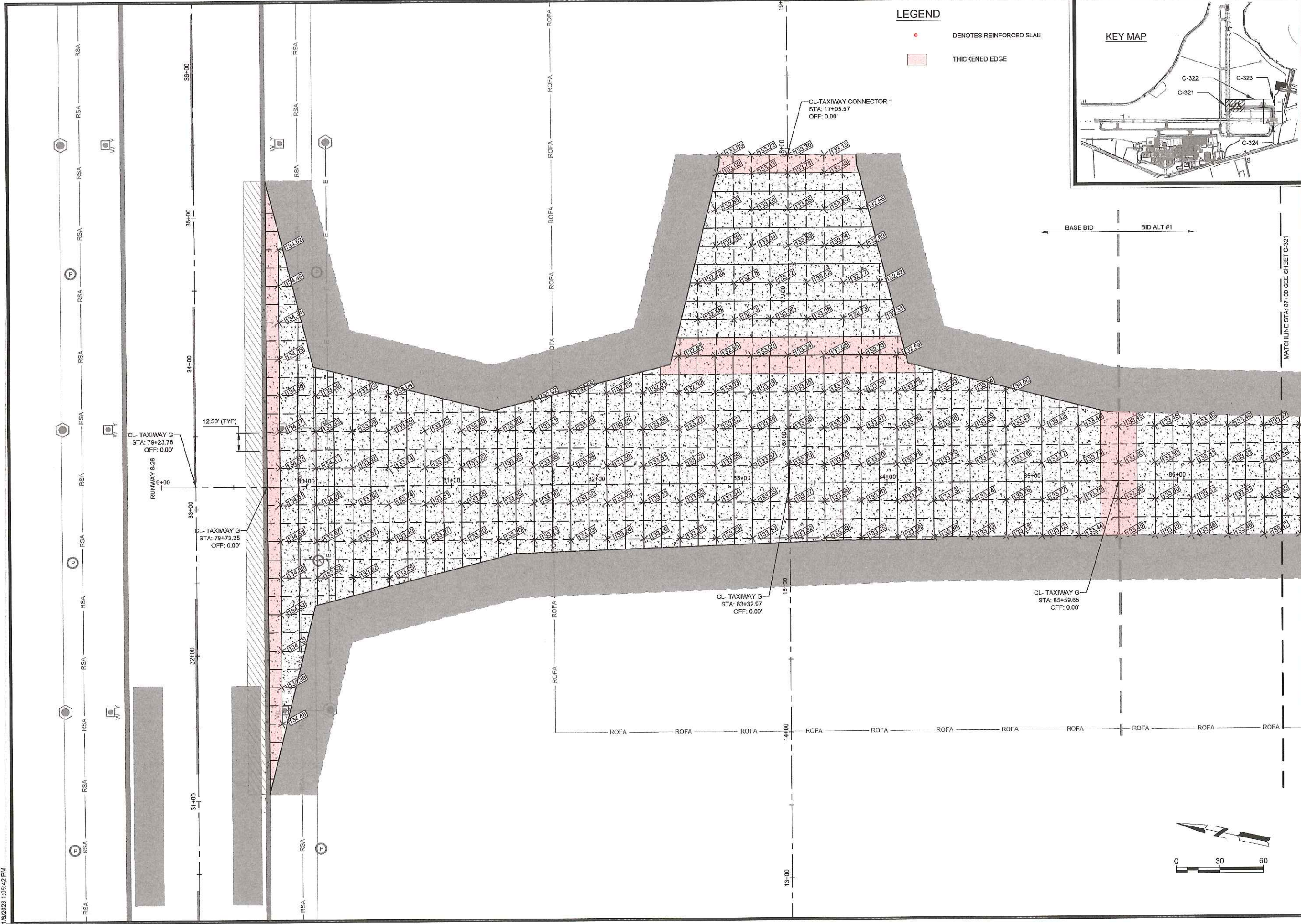
SEE NOTE (1) FOR FURTHER INFORMATION

CL- TAXIWAY G  
 STA: 104+17.24  
 OFF: 0.00'

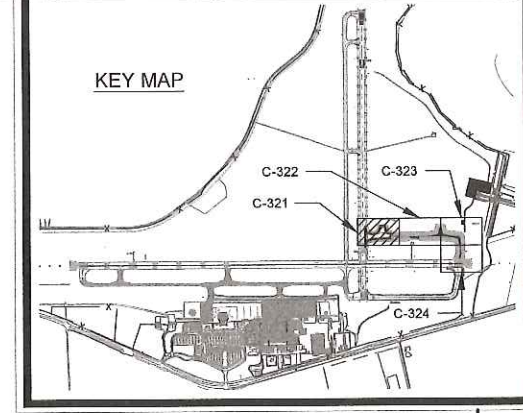
NOTE:  
 1. CONTRACTOR SHALL ALIGN JOINT SPACING TO MATCH EXISTING SPACING OF RUNWAY 17-35.



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1/6/2023 1:05:42 PM



**LEGEND**  
 • DENOTES REINFORCED SLAB  
 [Pink Box] THICKENED EDGE



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SHEET CONTENTS:  
 JOINTING PLAN -  
 SPOT ELEVATIONS

SHEET NO.

**C-325**



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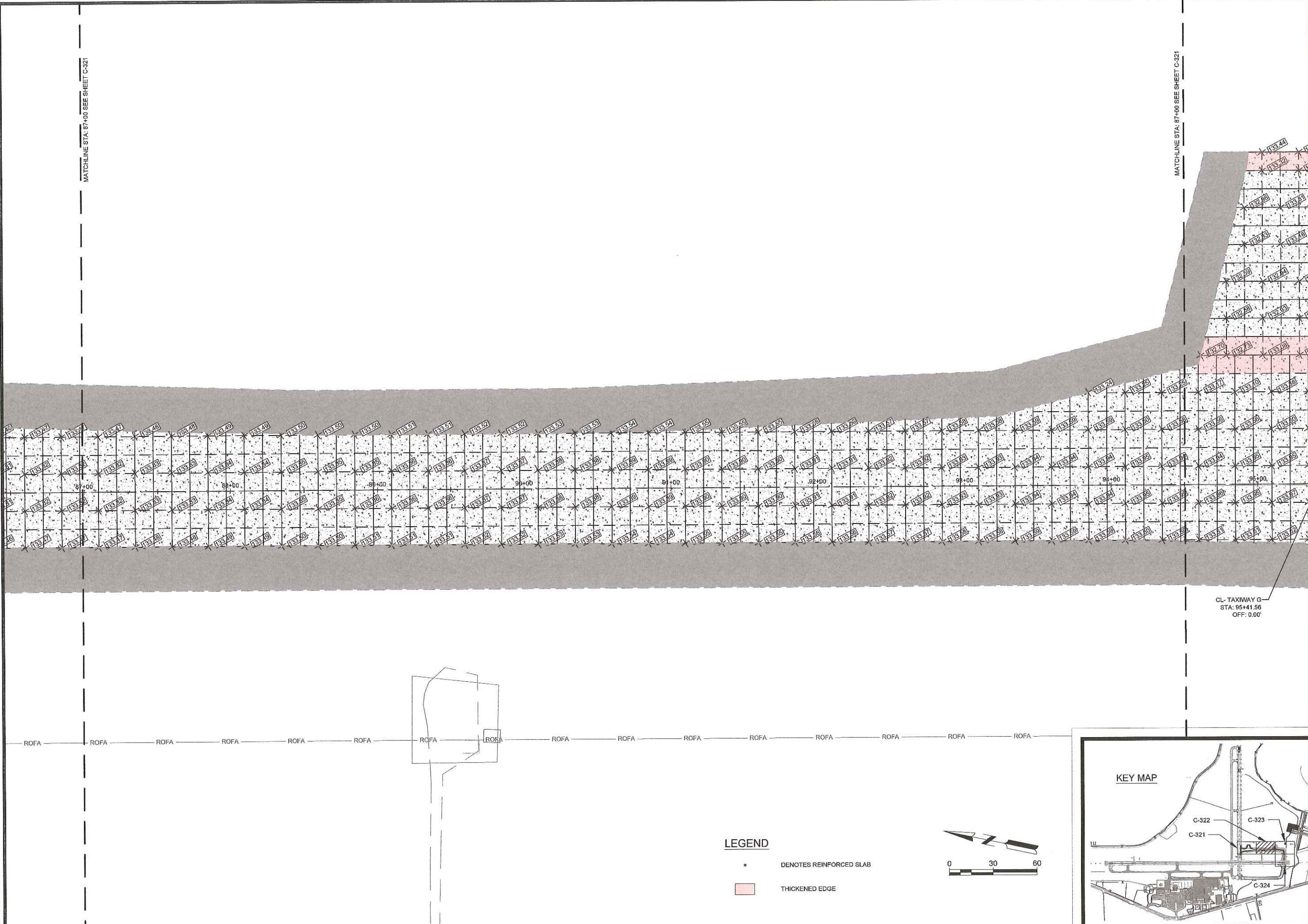
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SHEET CONTENTS  
 JOINTING PLAN -  
 SPOT ELEVATIONS

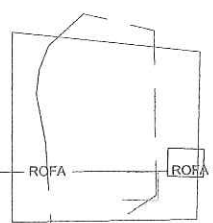
SHEET NO.

**C-326**

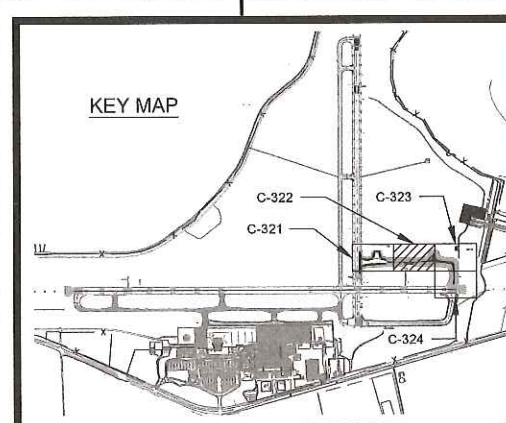
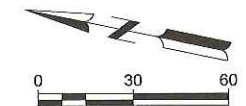


CL- TAXIWAY G  
 STA: 95+41.56  
 OFF: 0.00'

ROFA ROFA



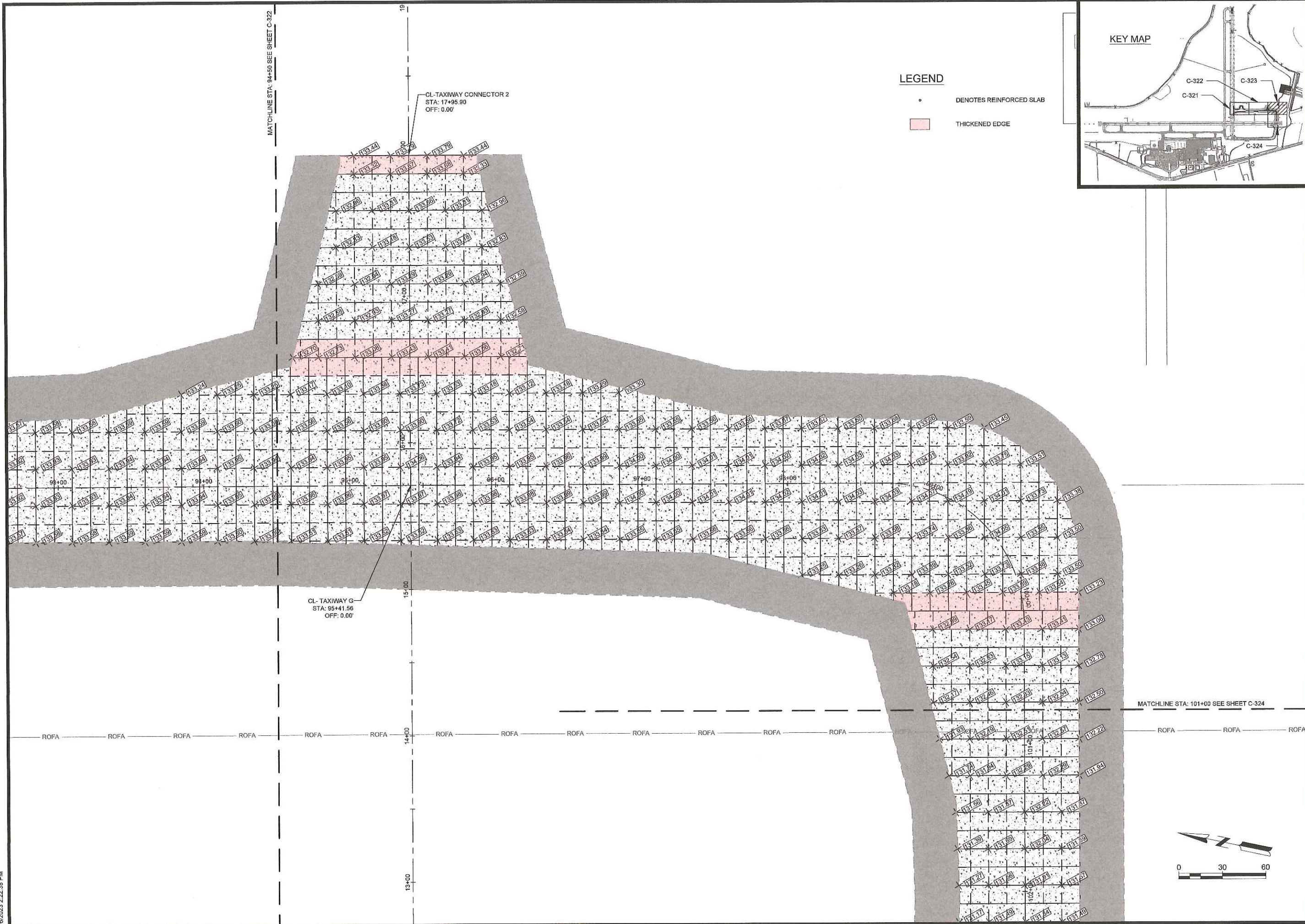
- LEGEND**
- DENOTES REINFORCED SLAB
  - THICKENED EDGE



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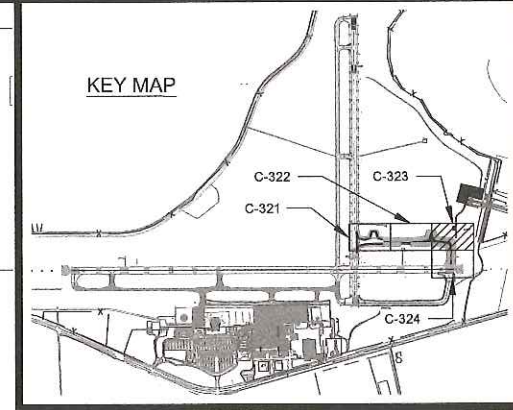


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**LEGEND**

- DENOTES REINFORCED SLAB
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CONSTRUCT TAXIWAY G**

1501 AVIATION WAY  
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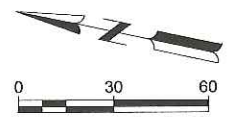
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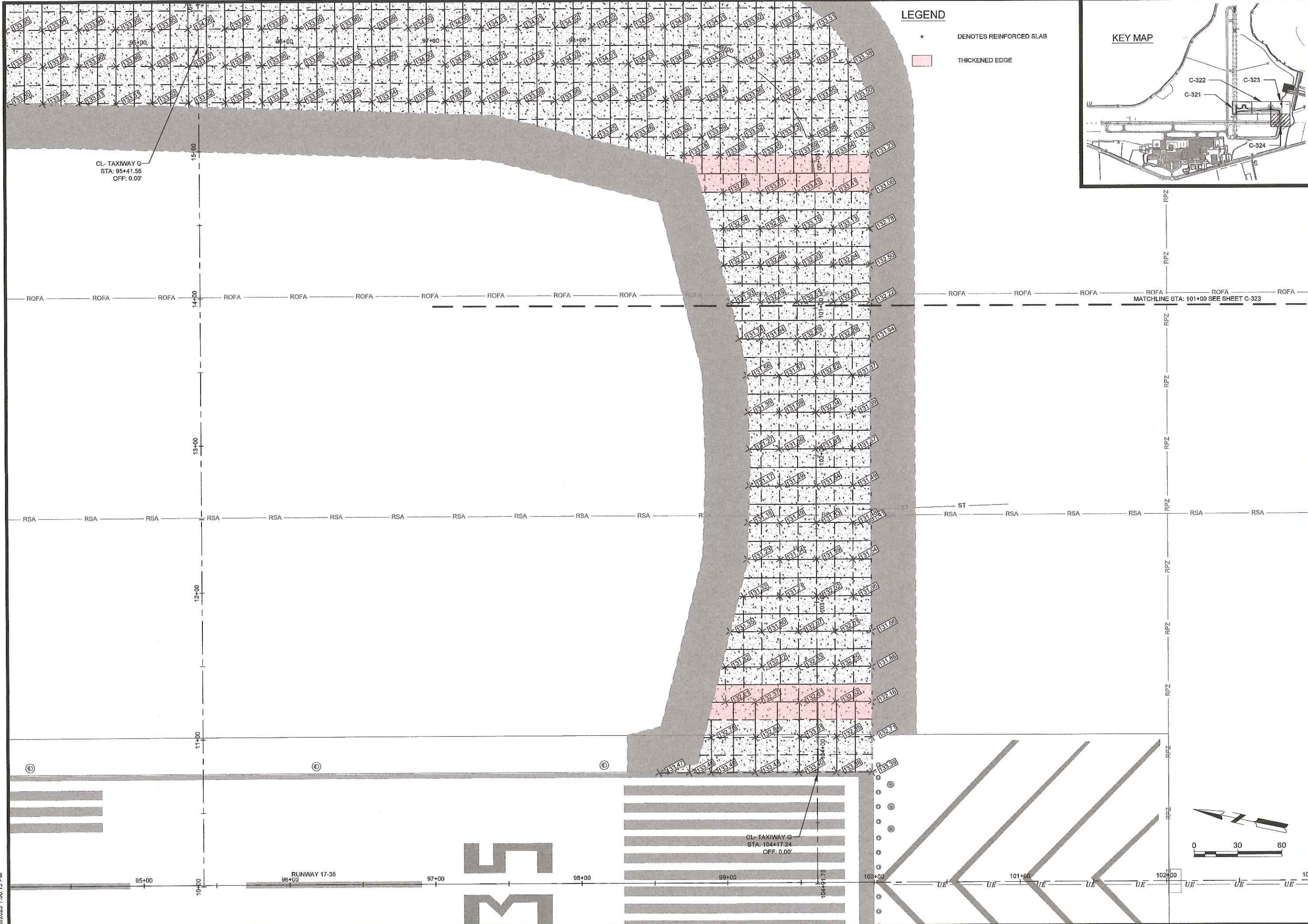
SHEET CONTENTS  
JOINTING PLAN -  
SPOT ELEVATIONS

SHEET NO.

**C-327**

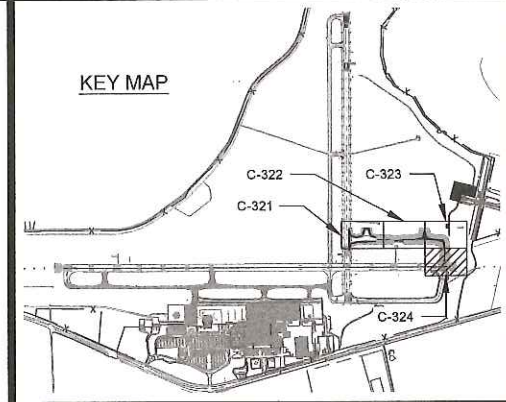






**LEGEND**

- DENOTES REINFORCED SLAB
- THICKENED EDGE



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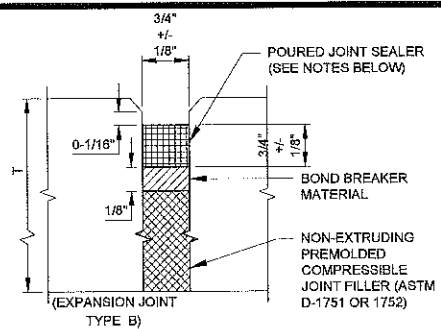
SHEET CONTENTS  
 JOINTING PLAN -  
 SPOT ELEVATIONS

SHEET NO.

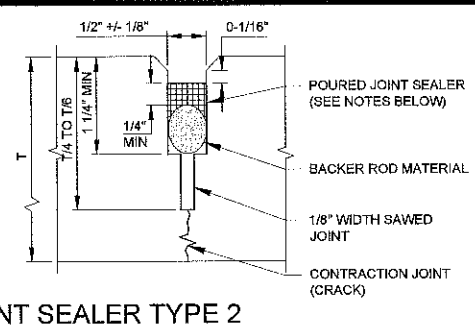
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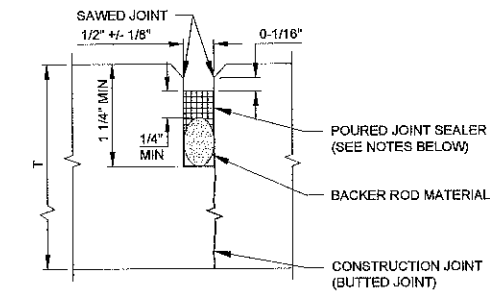




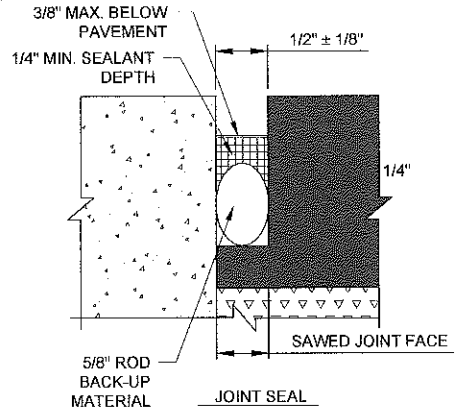
1 JOINT SEALER TYPE 1  
NOT TO SCALE



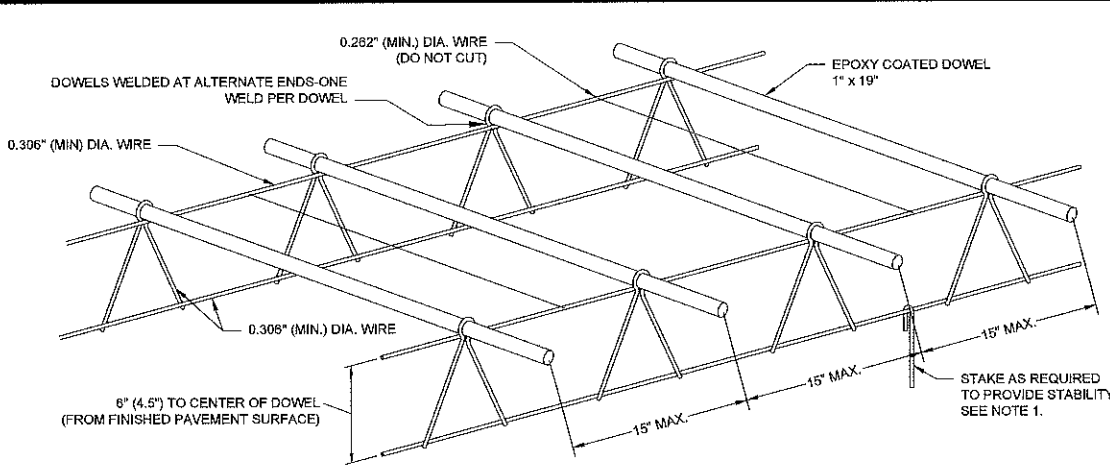
2 JOINT SEALER TYPE 2  
NOT TO SCALE



3 JOINT SEALER TYPE 3  
NOT TO SCALE



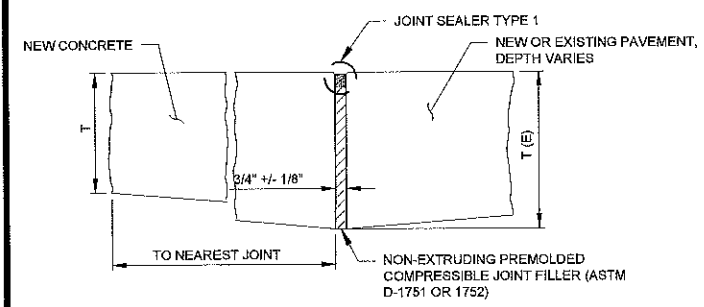
4 PCC/AC JOINT SEAL  
NOT TO SCALE



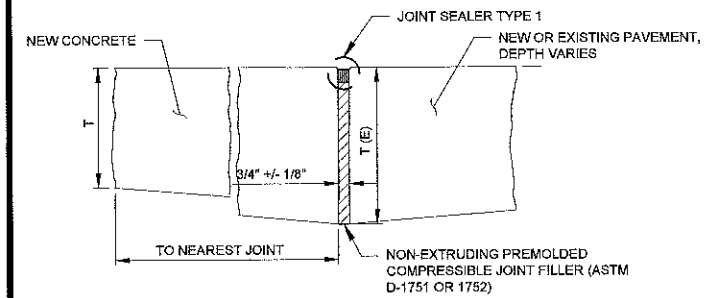
4 DOWEL BASKET ASSEMBLY  
NOT TO SCALE

- DOWEL NOTES:**
1. THE DOWEL BASKET ASSEMBLY SHOWN IS THE MINIMUM REQUIRED. THE REQUIREMENTS FOR THE DOWEL BASKETS ARE A PERFORMANCE SPECIFICATION AND IT WILL BE REQUIRED THAT THE CONTRACTOR PROVIDE SUFFICIENT SUPPORT, BRACING AND ANCHORAGE SO THAT THE AS CONSTRUCTED DOWELS WILL MEET THE TOLERANCE SPECIFICATIONS.
  2. DOWELS TO MEET ASTM A615-GR-60 ALL MATERIALS TO BE PROVIDED SHALL BE SMELTED AND MANUFACTURED IN THE UNITED STATES.
  3. DOWELS TO BE EPOXY COATED TO MEET AASHTO M-254-B 7 MIL MIN.
  4. ENDS OF DOWELS TO BE EPOXY COATED.
  5. DOWELS TO BE PRE-LUBRICATED.
  6. DOWEL ASSEMBLY LENGTH AS REQUIRED.

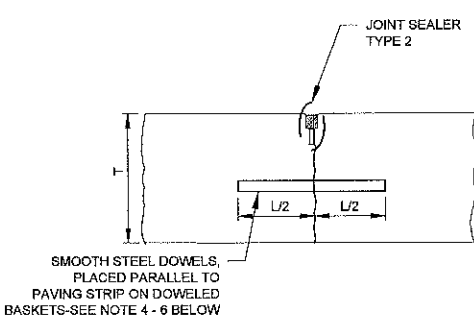
- SEALANT NOTES:**
1. SEALANT RESERVOIR SIZED TO PROVIDE PROPER SHAPE FACTOR. (WIDTH/DEPTH). DIFFERENT FIELD POURED SEALANTS REQUIRE DIFFERENT SHAPE FACTORS FOR OPTIMUM PERFORMANCE. USE MANUFACTURER'S RECOMMENDED SHAPE FACTOR.
  2. ROD BACK-UP MATERIAL MUST BE COMPATIBLE WITH THE TYPE OF POURED SEALANT USED AND SIZED TO PROVIDE THE DESIRED SHAPE FACTOR.
  3. SEE SPECIFICATION P-605 FOR SEALANT INFORMATION.
  4. TOP OF SEALANT SHALL BE 1/4" TO 3/8" BELOW TOP OF PAVEMENT.
  5. ALL JOINTS SHALL BE 1/4" - 45° BEVELED.
  6. ALL SEALANTS USED SHALL BE JET FUEL RESISTANT.
  7. THE INITIAL 1/4" DEPTH OF SAWCUT SHOWN MAY BE INCREASED BY 25% IF EACH SUCCESSIVE WEAKENED PLANE DOES NOT CRACK, CAUSING EXCESSIVE OPENING AT CRACKS THAT DO DEVELOP. APPROVAL TO DO SO WILL BE REQUIRED FROM THE CONTRACTING OFFICER.



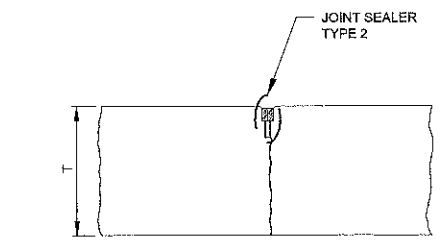
A THICKENED EDGE ISOLATION JOINT - TYPE "A"  
NOT TO SCALE



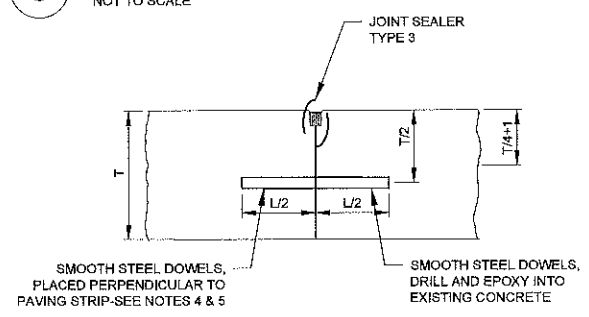
A-1 THICKENED EDGE ISOLATION JOINT - TYPE "A"  
NOT TO SCALE



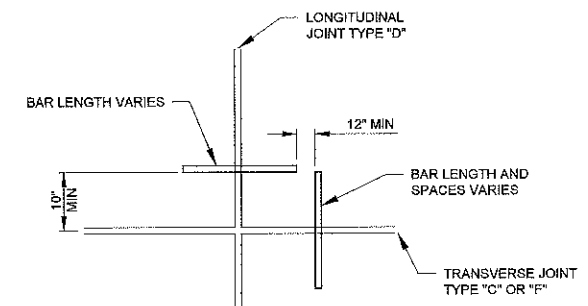
C DOWELED CONTRACTION JOINT - TYPE "C"  
NOT TO SCALE



D DUMMY CONTRACTION JOINT - TYPE "D"  
NOT TO SCALE

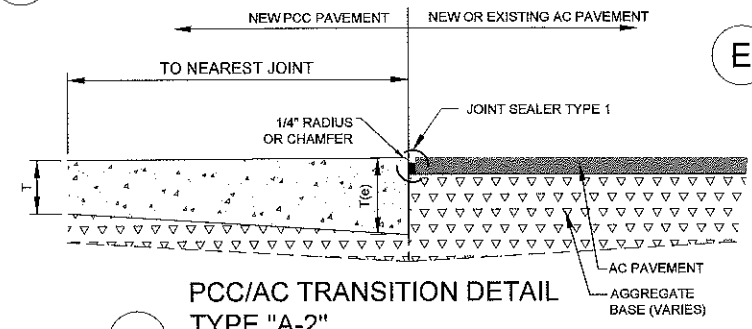


E DOWELED CONSTRUCTION JOINT - TYPE "E"  
NOT TO SCALE  
(ALSO USE WHERE PAVING OPERATIONS STOP)



5 POSITION OF DOWELS AT EDGE JOINT (TYPE C OR D)  
NOT TO SCALE

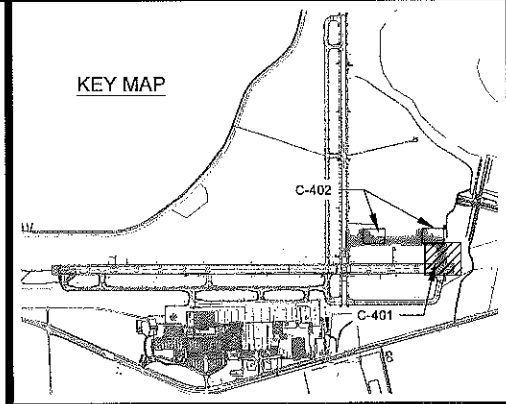
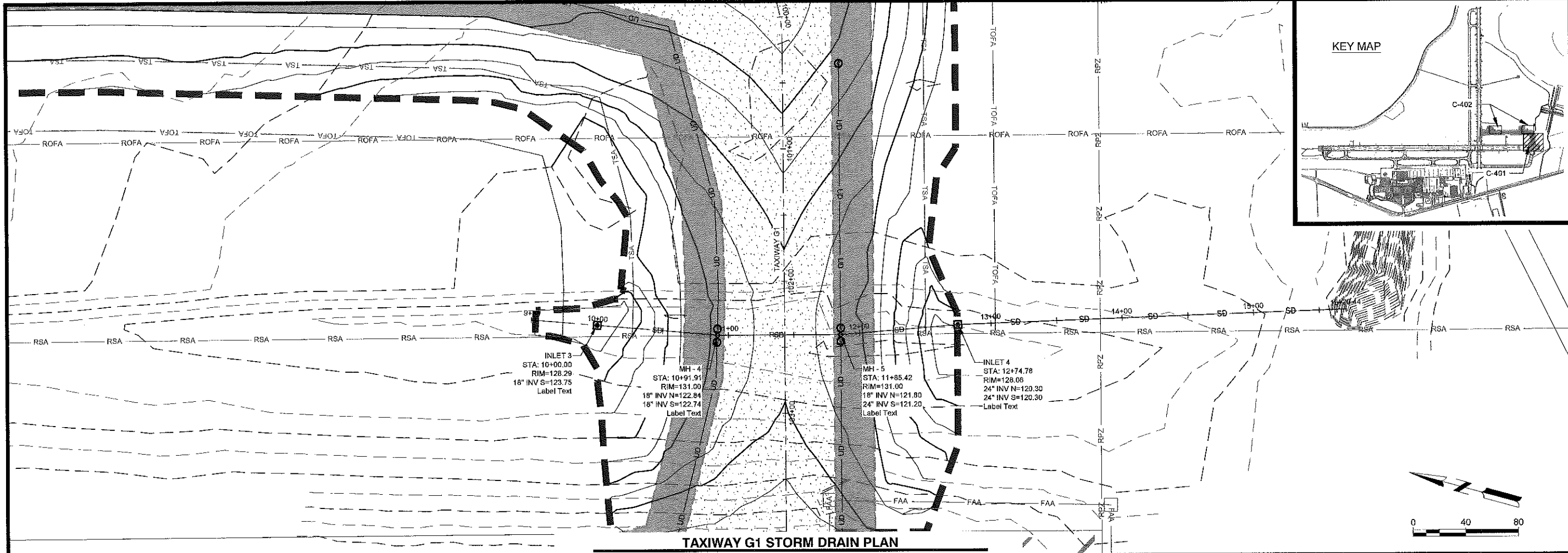
- JOINT NOTES:**
1. "T" = SLAB THICKNESS, "L" = DOWEL LENGTH
  2. "T" = 14" FOR TAXIWAY G PAVEMENT.
  3. FOR 14" P.C.C. USE 1 1/8" DIA. x 20" EPOXY COATED DOWELS @ 15" C-C SPACING. (OIL PRIOR TO PLACING CONC.)
  4. NO CHANGES IN THE JOINTING PATTERN SHALL BE MADE WITHOUT THE WRITTEN APPROVAL OF THE DESIGN ENGINEER.



A-2 PCC/AC TRANSITION DETAIL TYPE "A-2"  
NOT TO SCALE

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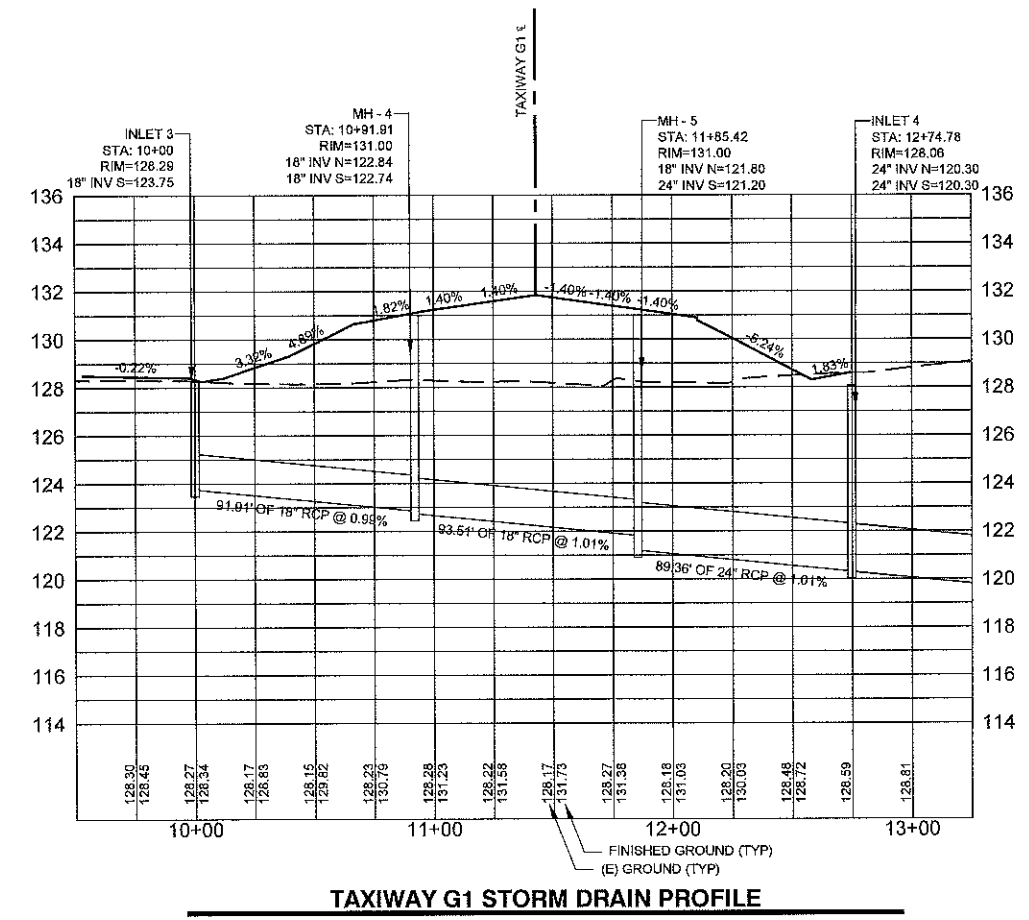
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 CONSTRUCT TAXIWAY G**

1501 AVIATION WAY  
 AUGUSTA, GA 30906-9620



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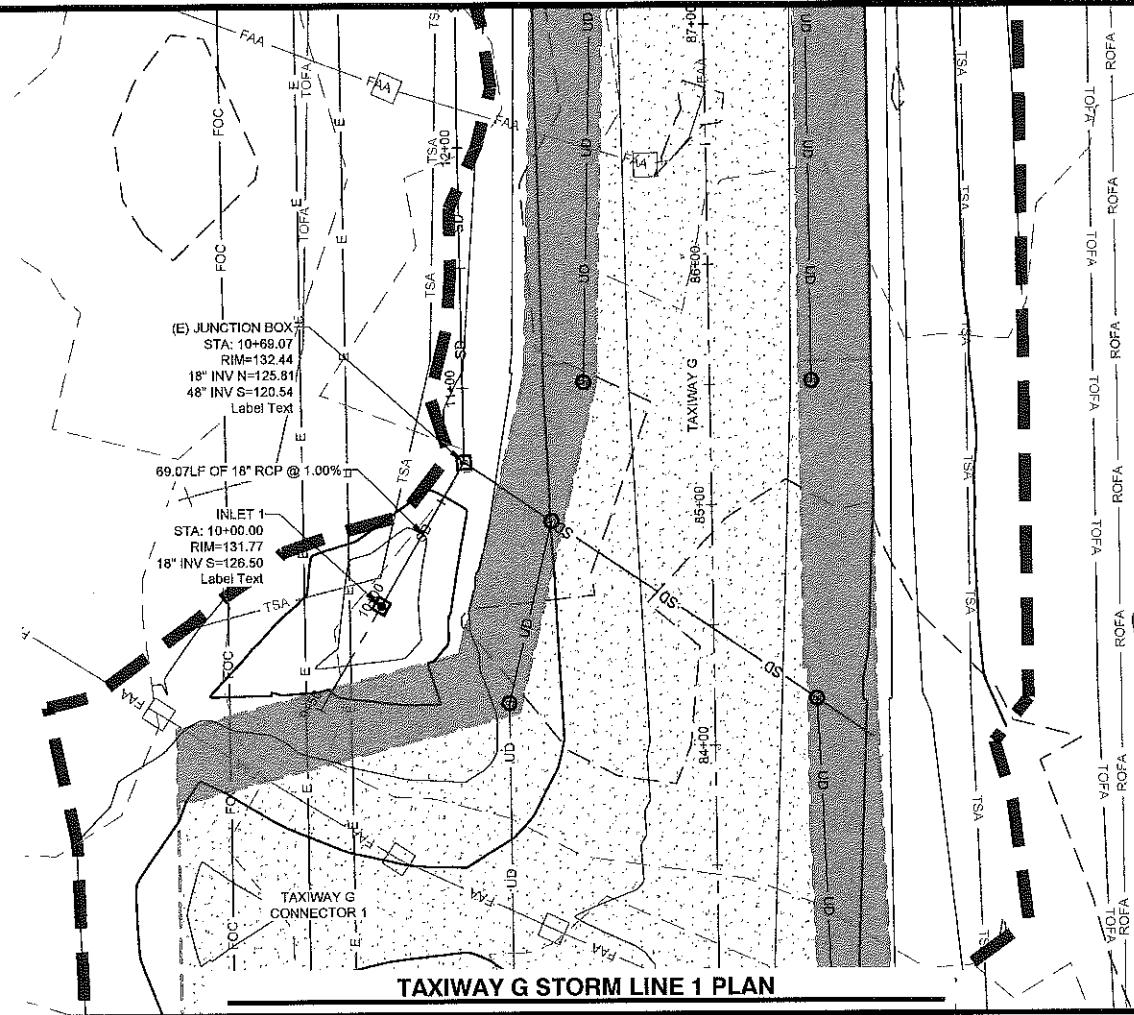
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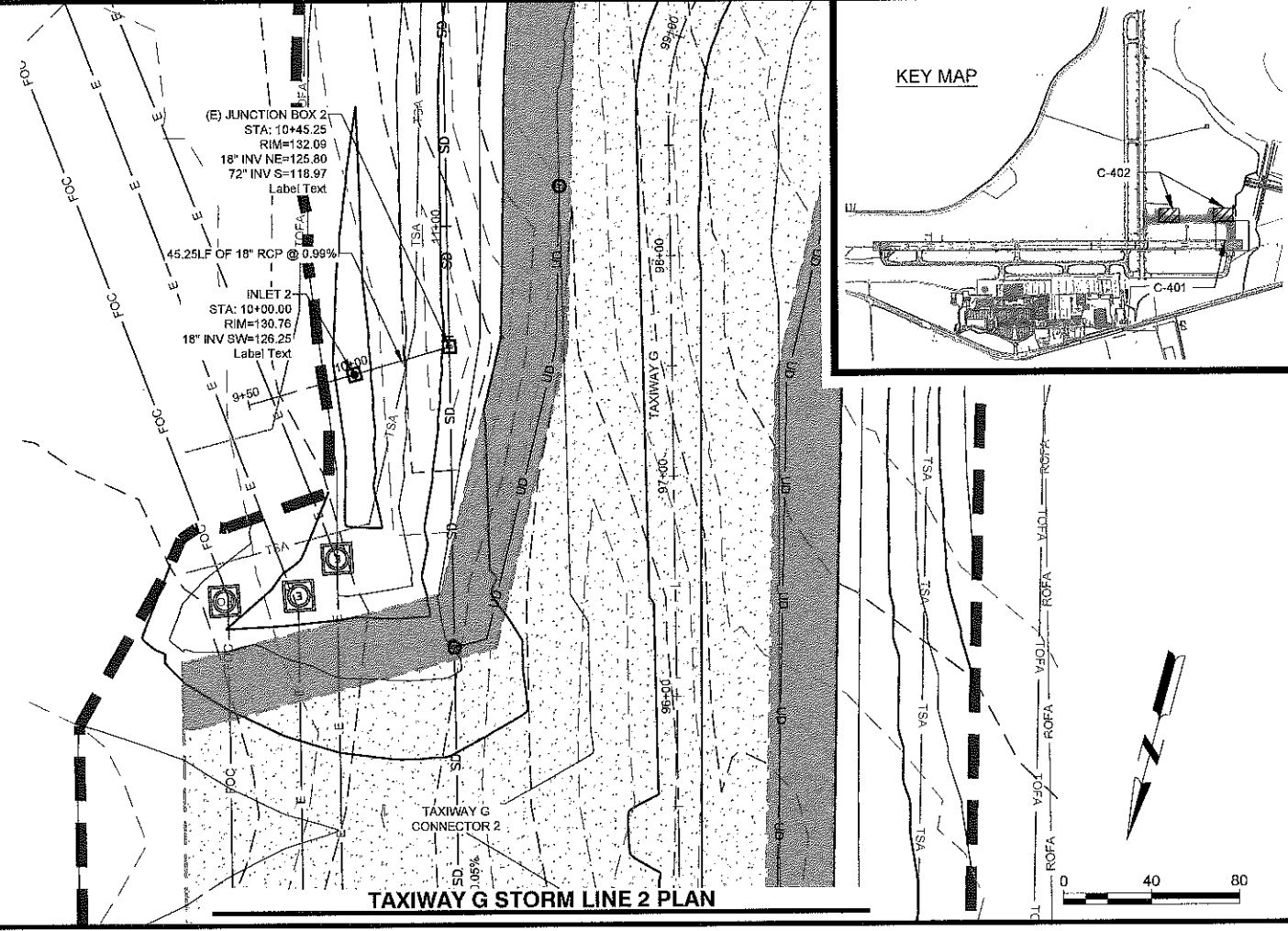
SHEET CONTENTS  
 STORM PLAN AND PROFILE

SHEET NO.

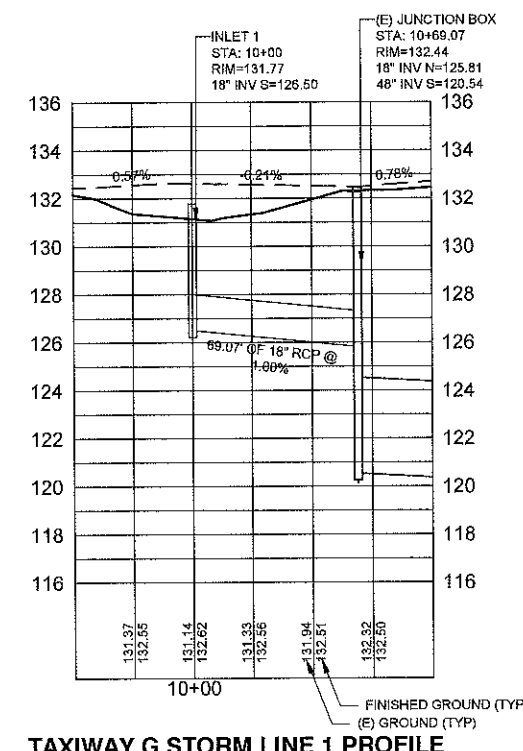
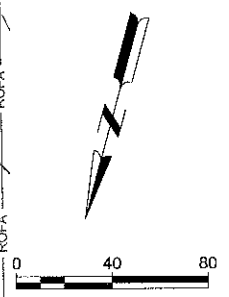
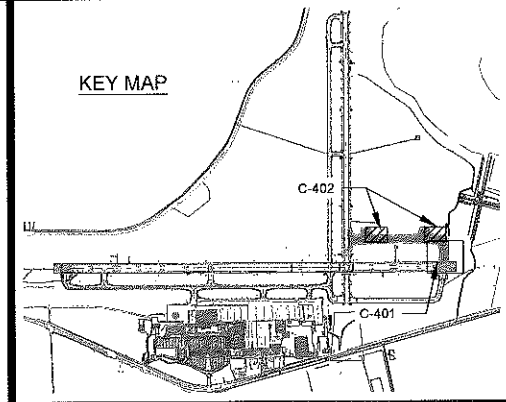
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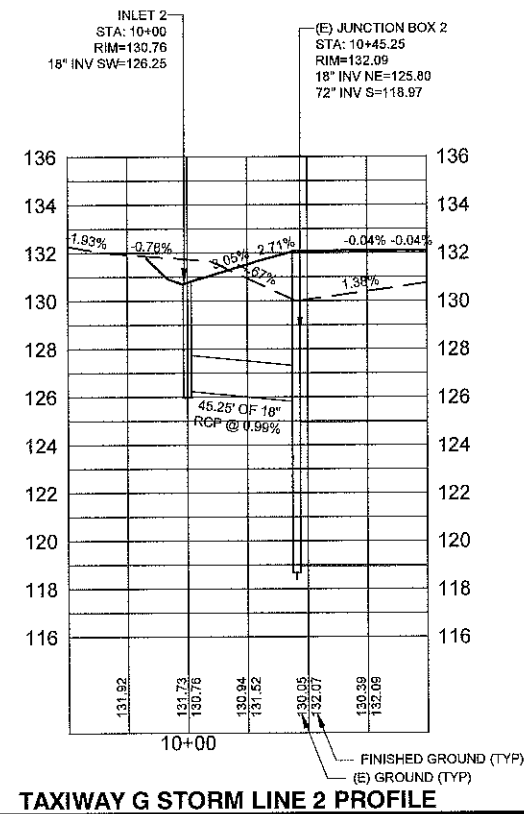
TAXIWAY G STORM LINE 1 PLAN



TAXIWAY G STORM LINE 2 PLAN



TAXIWAY G STORM LINE 1 PROFILE



TAXIWAY G STORM LINE 2 PROFILE

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**AUGUSTA REGIONAL AIRPORT  
 CONSTRUCT TAXIWAY G**

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SHEET CONTENTS  
 STORM PLAN AND PROFILE

SHEET NO.

**C-402**

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 CONSTRUCT TAXIWAY G**

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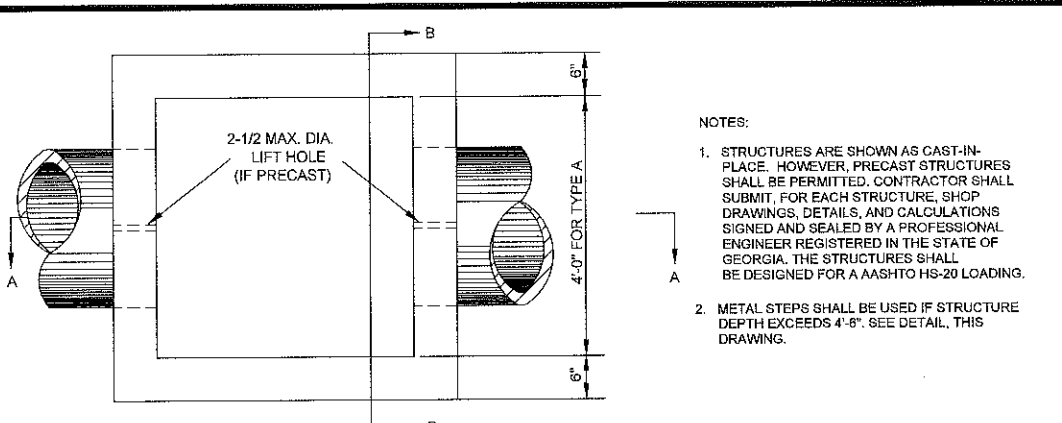
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SHEET CONTENTS  
 STORM DRAINAGE  
 DETAILS

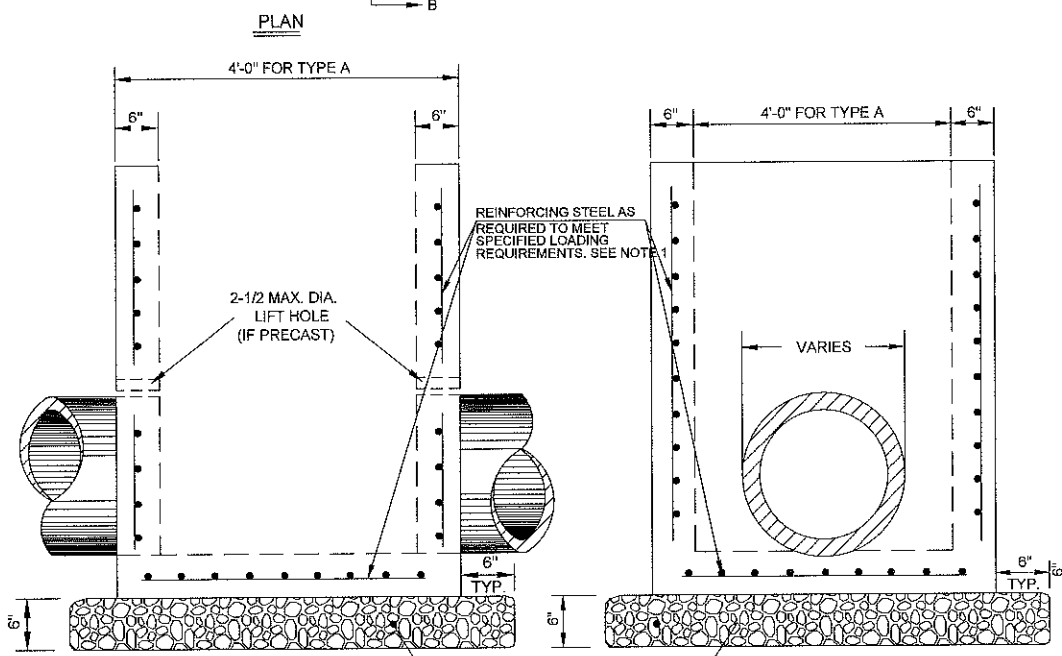
SHEET NO.

**C-403**

PIPE DIA. "D"	MIN. "A"
15" & LESS	8"
18" TO 21"	10"
24" TO 30"	12"
33" TO 42"	15"
48" & LARGER	18"



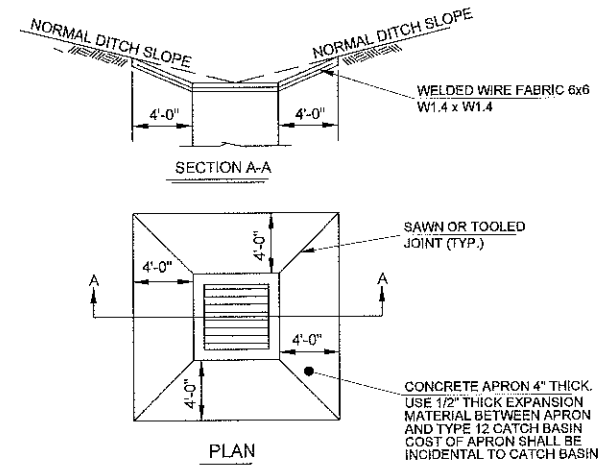
- NOTES:**
- STRUCTURES ARE SHOWN AS CAST-IN-PLACE. HOWEVER, PRECAST STRUCTURES SHALL BE PERMITTED. CONTRACTOR SHALL SUBMIT, FOR EACH STRUCTURE, SHOP DRAWINGS, DETAILS, AND CALCULATIONS SIGNED AND SEALED BY A PROFESSIONAL ENGINEER REGISTERED IN THE STATE OF GEORGIA. THE STRUCTURES SHALL BE DESIGNED FOR A AASHTO HS-20 LOADING.
  - METAL STEPS SHALL BE USED IF STRUCTURE DEPTH EXCEEDS 4'-6". SEE DETAIL, THIS DRAWING.



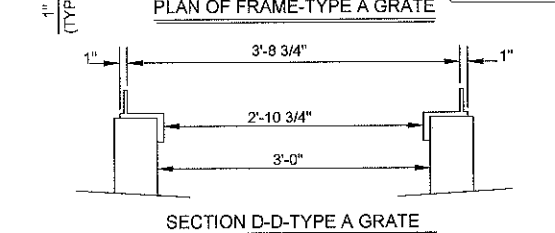
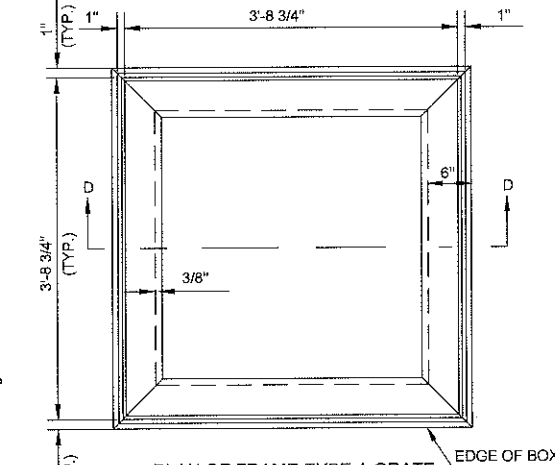
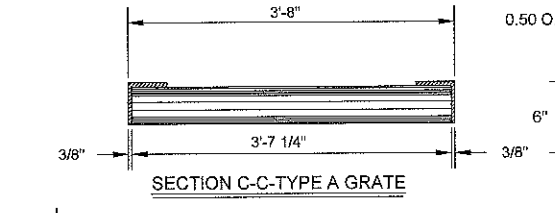
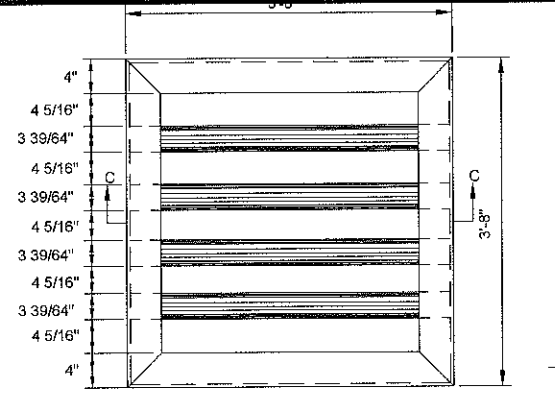
PLACE 6" LAYER OF #57 WASHED STONE AS BEDDING FOR STRUCTURE. COST SHALL BE INCIDENTAL TO COST OF STRUCTURE.  
 A MINIMUM OF 4'-0" OF PAVEMENT TO BE PLACED AROUND STRUCTURE AS SHOWN. PAYMENT FOR THIS PAVEMENT WILL BE INCLUDED IN BID PRICE FOR STRUCTURE.

**PRECAST CATCH BASIN**  
 SCALE: NTS  
 1  
 C-403

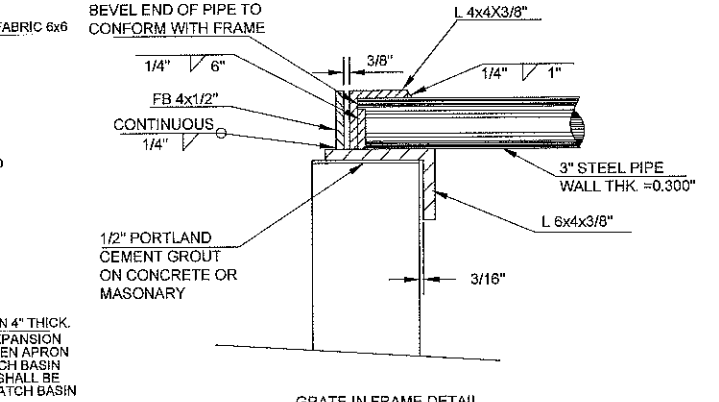
- GENERAL NOTES:**
- ALL PRECAST STRUCTURE SHOP DRAWINGS SHALL BE STAMPED BY A REGISTERED GEORGIA STRUCTURAL ENGINEER AND SUBMITTED TO THE PROJECT ENGINEER FOR APPROVAL. PRECAST MANHOLE RINGS AND CATCH BASIN STRUCTURES SHALL CONFORM TO ASTM C-478 AND BE DESIGNED TO SUPPORT 30,000 POUND WHEEL LOADS AT 250 POUNDS PER SQUARE INCH TIRE PRESSURE. ALLOWABLE SOIL PRESSURE IS 2,000 POUNDS PER SQUARE FEET.
  - CATCH BASIN DIMENSIONS ARE TYPICAL. ACTUAL DIMENSIONS SHALL BE SUITABLE TO WITHSTAND THE ANTICIPATED LOADS FOR THE GIVEN CATCH BASIN CONFIGURATIONS AND PIPE PENETRATIONS.
  - PIPE PENETRATIONS SHALL BE CORED OR BLOCKED OUT. CONCRETE STORM PIPE CONNECTIONS SHALL BE SEALED WITH NON-SHRINK GROUT. SUBDRAIN CONNECTIONS SHALL BE MADE WITH A FLEXIBLE CONNECTOR.
  - PROVIDE ROUND RUBBER GASKET MEETING ASTM C-443 BETWEEN MANHOLE SECTION JOINTS.
  - MANHOLE COVER AND CATCH BASIN COVER SHALL BE CAST IRON FRAME WITH HEAVY DUTY GRATE.
  - A. FRAMES SHALL BE CAST INTO THE CONCRETE OR BOLTED TO THE CONCRETE STRUCTURE.  
 B. CONTRACTOR SHALL SUBMIT SHOP DRAWINGS OF THE SELECTED COVERS FOR ENGINEER APPROVAL.
  - PLACE MANHOLES ON 12" OF COMPACTED BEDDING. PLACE CATCH BASINS ON 8" OF COMPACTED BEDDING.



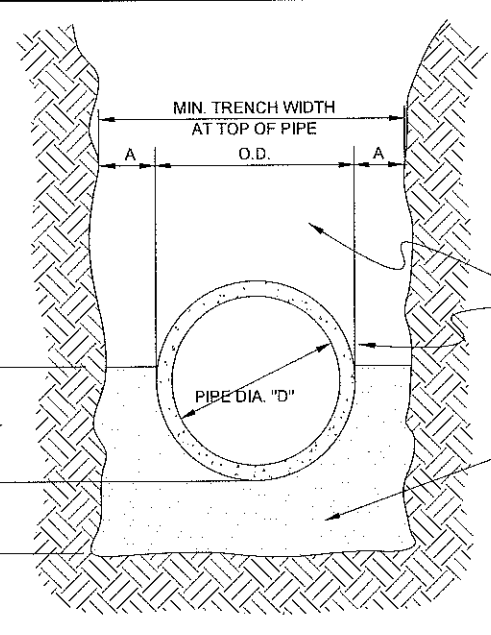
**CONCRETE APRON DETAIL**  
 SCALE: NTS  
 2  
 C-403



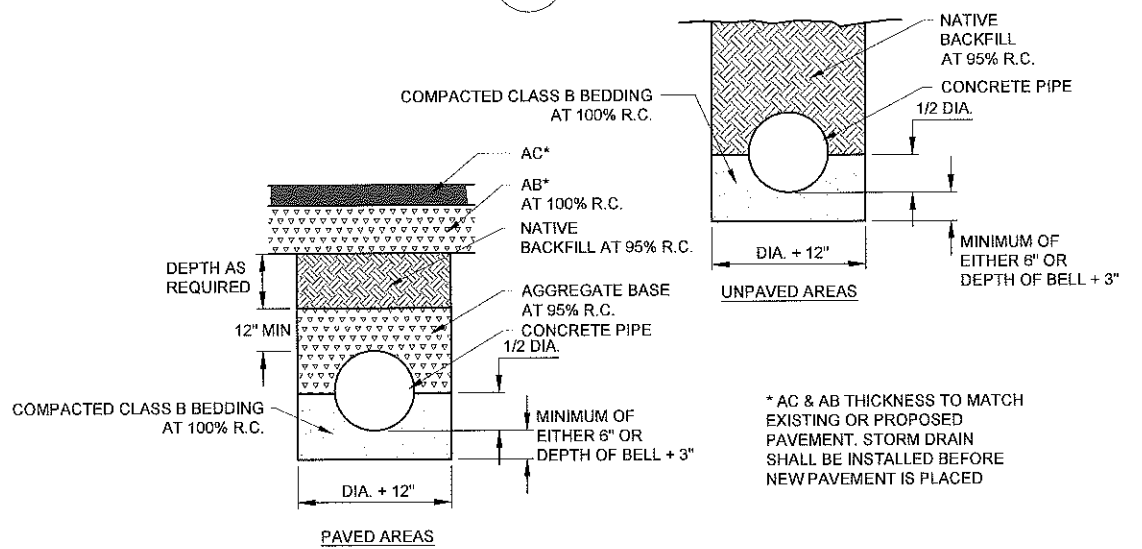
**SECTION D-D-TYPE A GRATE**



**GRATE IN FRAME DETAIL**

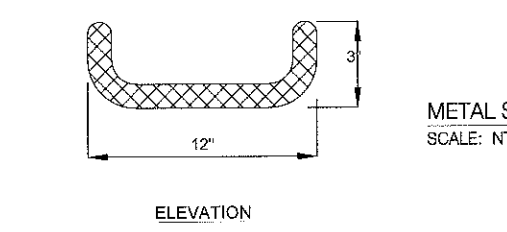
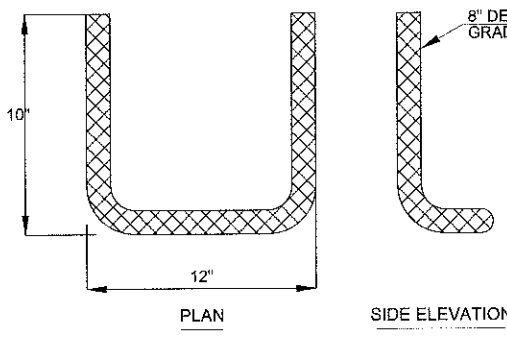


**PIPE BEDDING FOR R.C.P.**  
 SCALE: NTS  
 3  
 C-403



**CONCRETE PIPE MATERIAL**

**PIPE TRENCHING**  
 SCALE: NTS  
 1  
 C-403

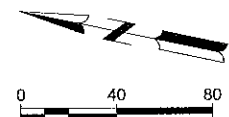
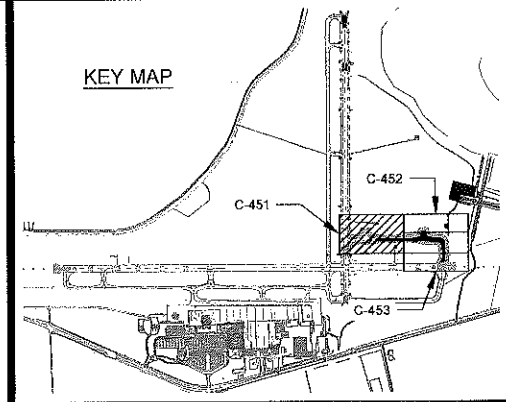
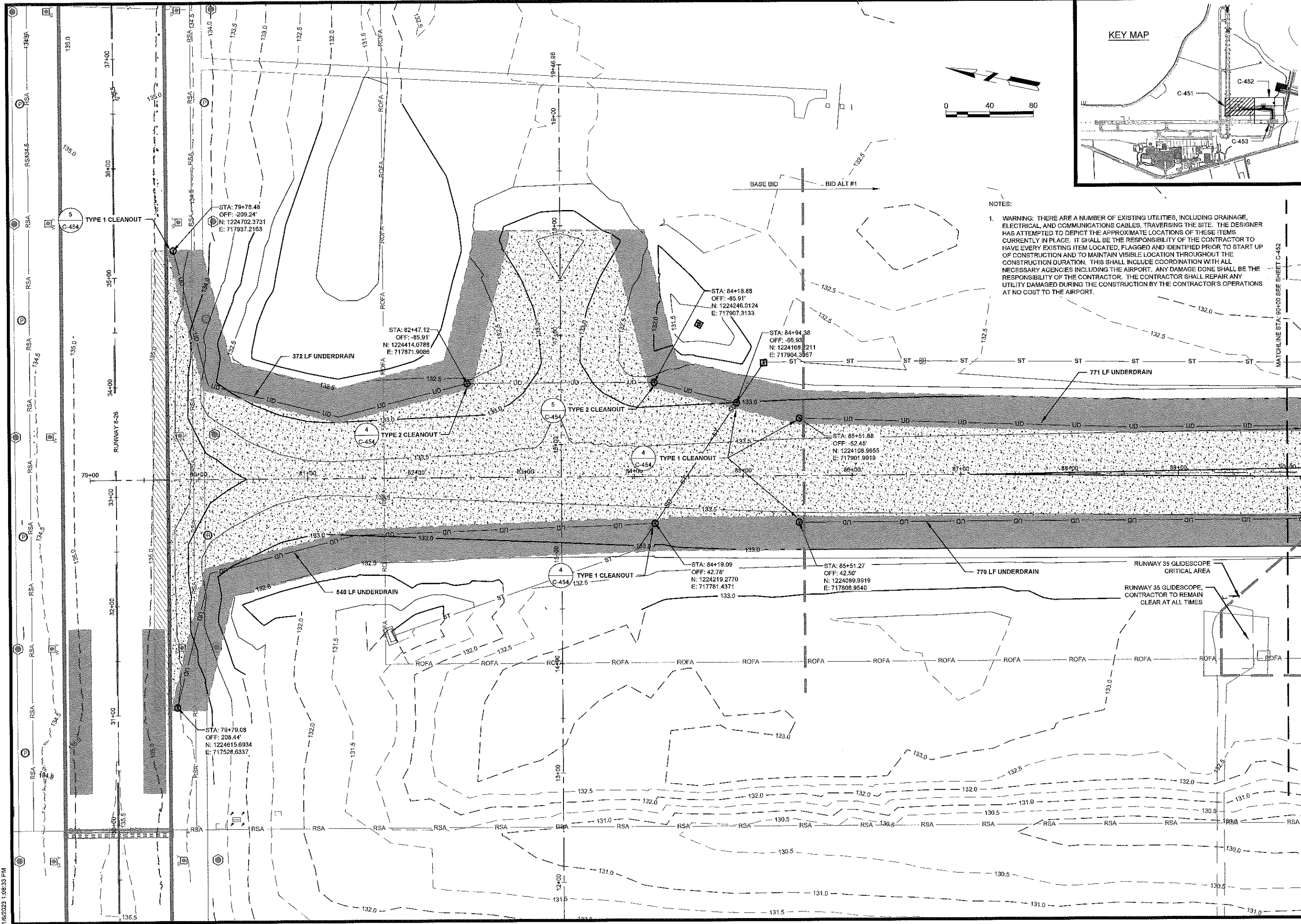


**METAL STEP DETAIL**  
 SCALE: NTS  
 4  
 C-403

**NOTE:**  
 STEPS DIFFERING IN DIMENSIONS, CONFIGURATION, OR MATERIALS FROM THOSE SHOWN MAY ALSO BE USED PROVIDED THE CONTRACTOR HAS FURNISHED THE ENGINEER WITH DETAILS OF THE PROPOSED STEPS AND HAS RECEIVED WRITTEN APPROVAL FROM THE ENGINEER FOR THE USE OF SUCH STEPS.  
 ALL STEPS SHALL PROTRUDE 7" FROM INSIDE FACE OF STRUCTURE WALL.

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1/6/2023 1:08:33 PM



NOTES:

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CONSTRUCT TAXIWAY G**

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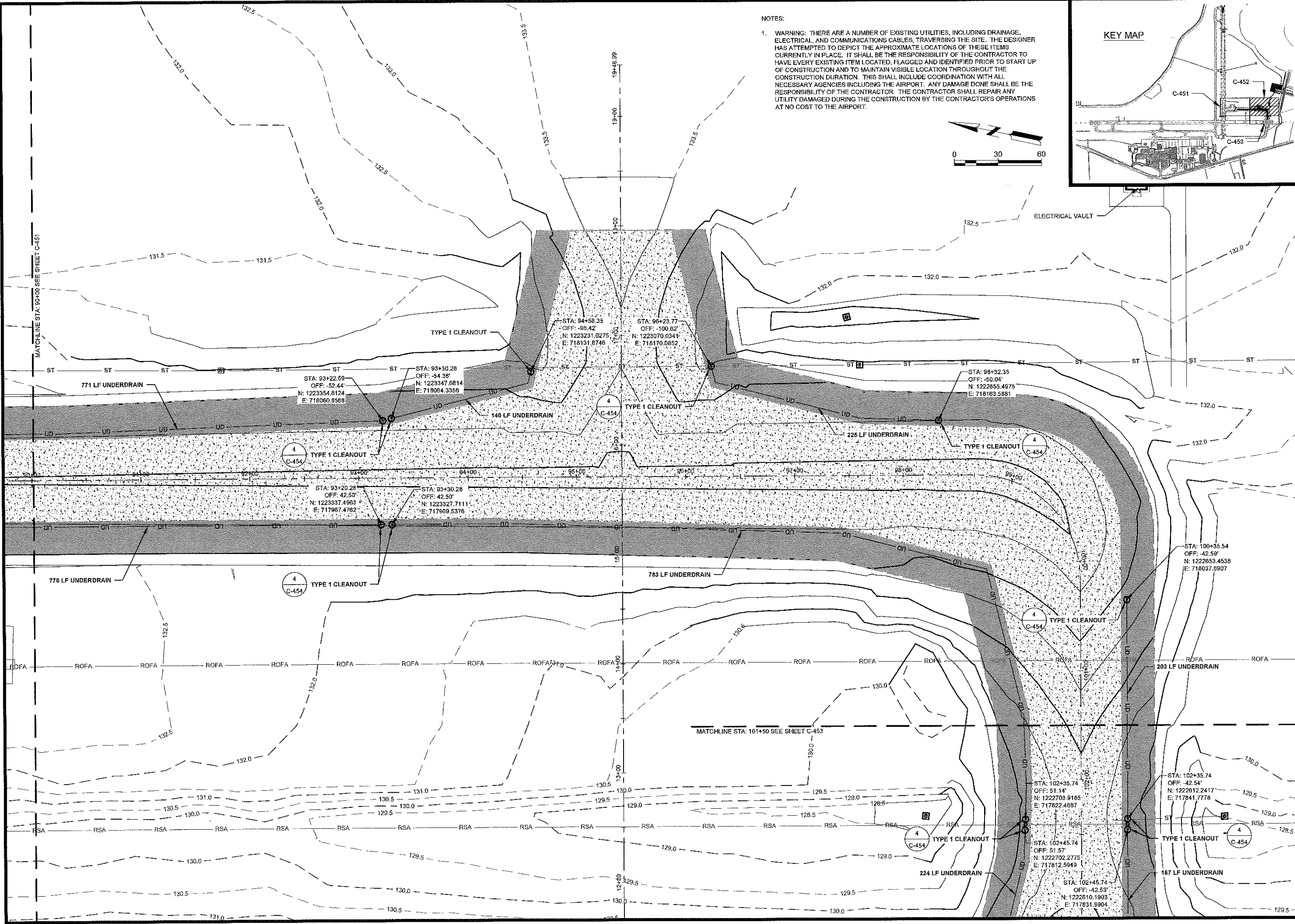
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**UNDERDRAIN PLAN**

SHEET NO.

**C-451**

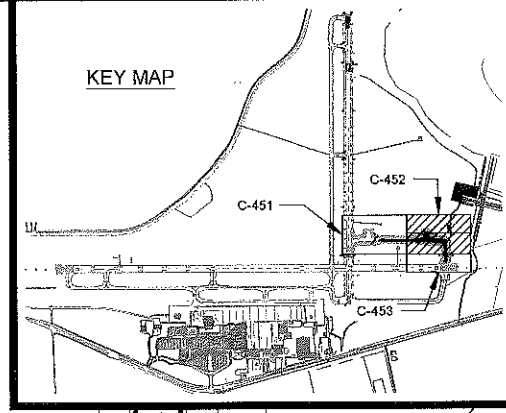


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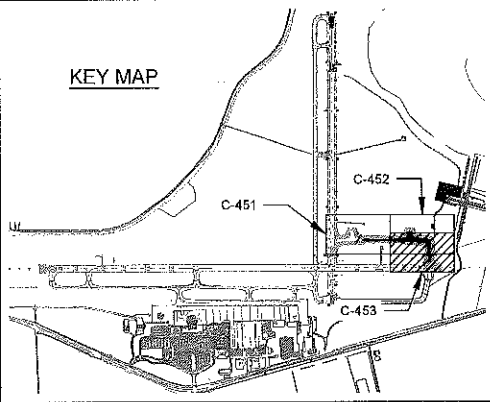
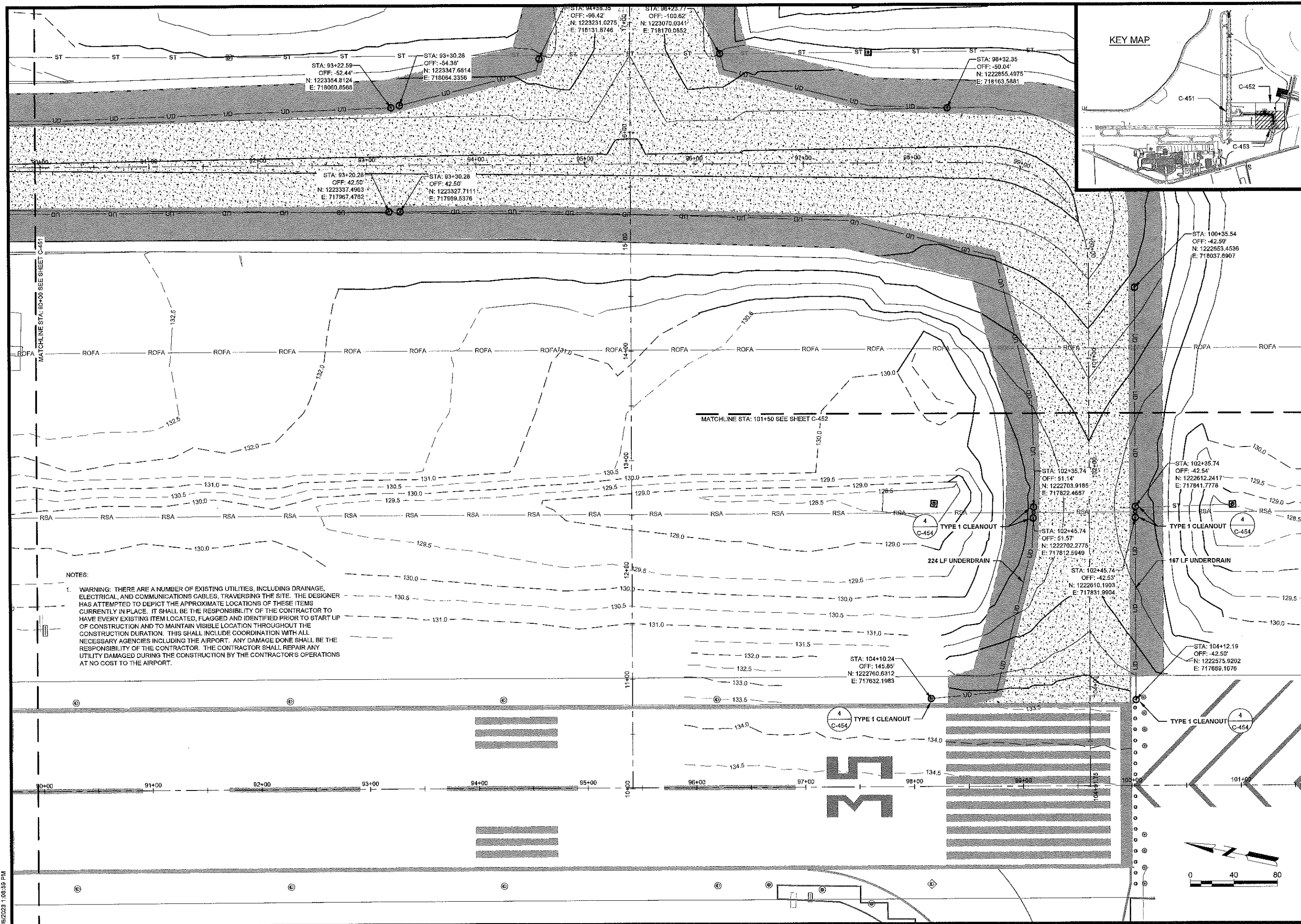
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 UNDERDRAIN PLAN

SHEET NO.

**C-452**

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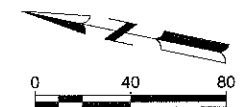
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UNDERDRAIN PLAN

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**C-453**





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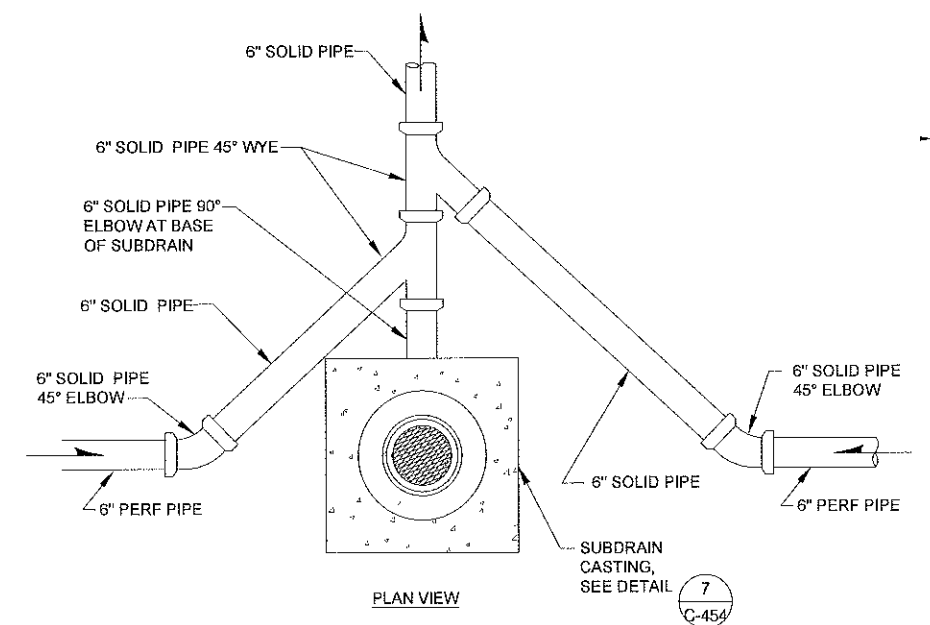
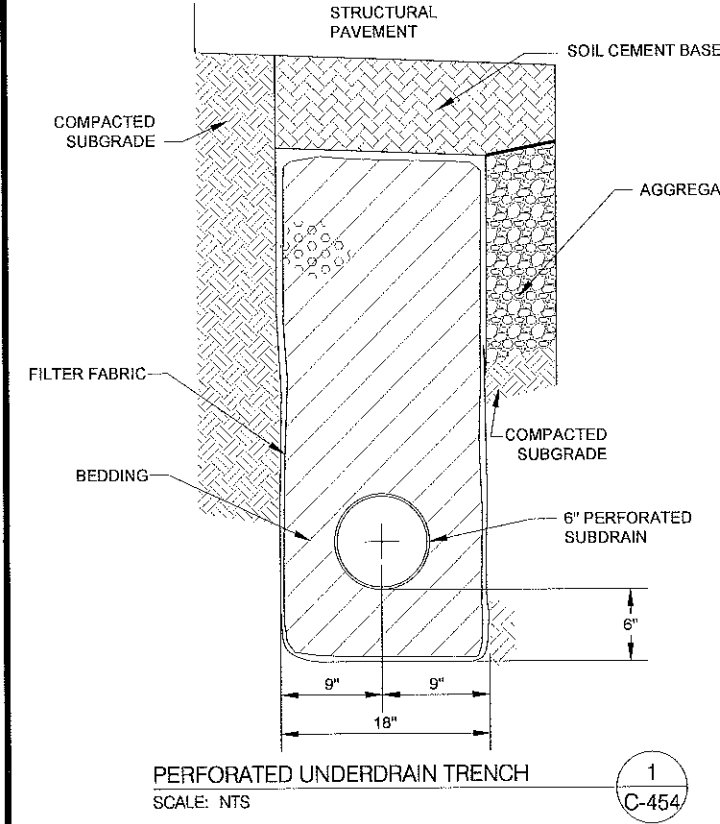
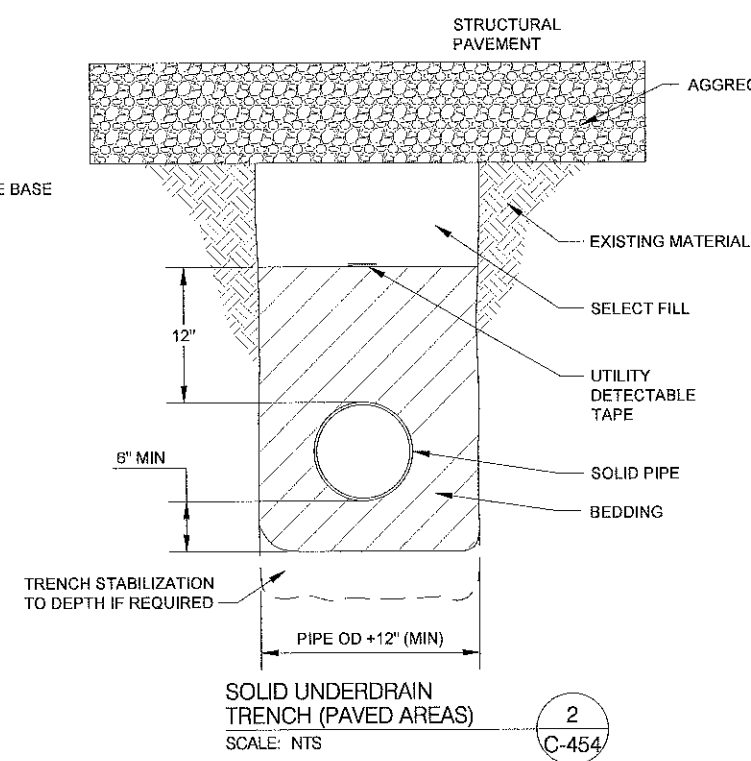
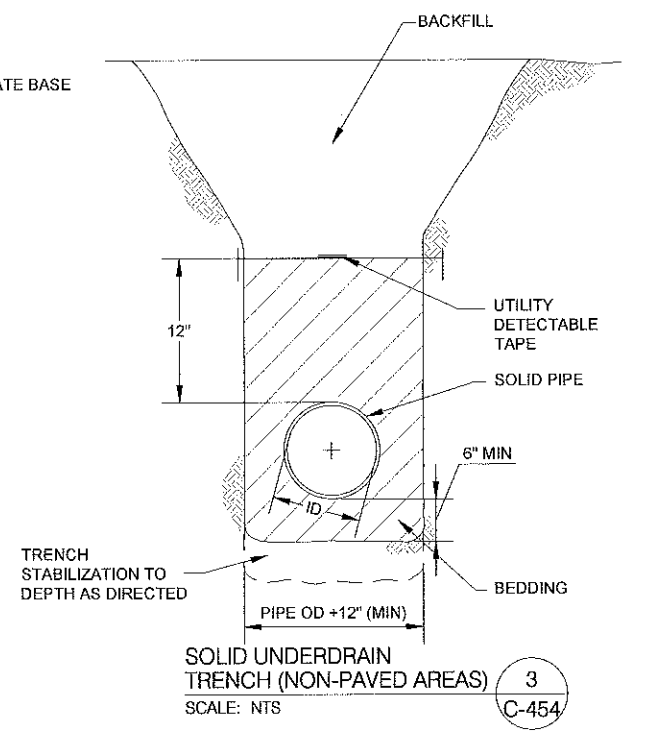
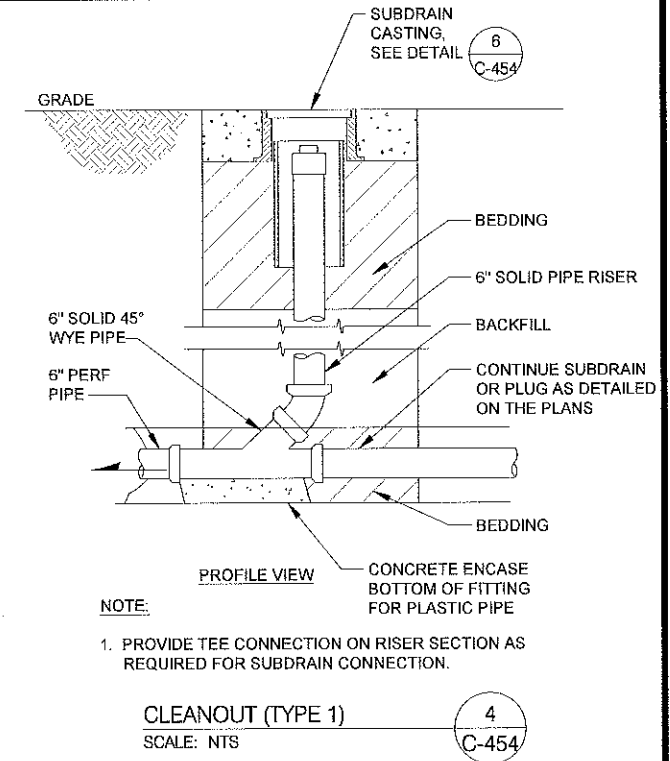
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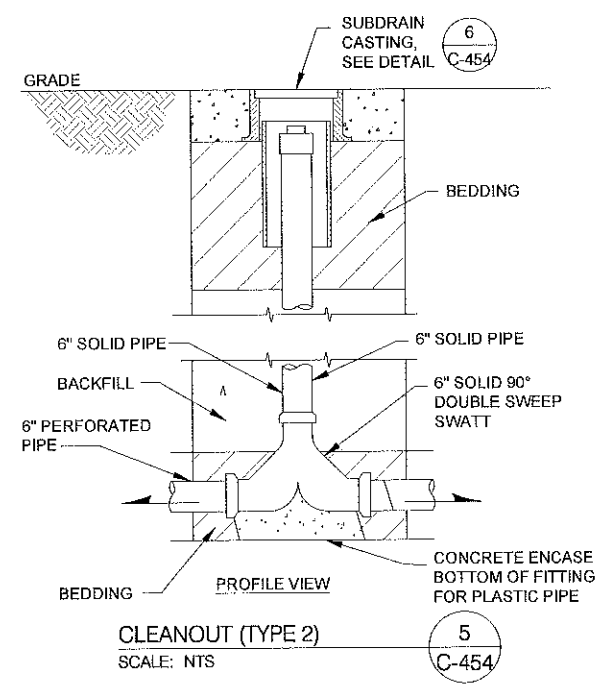
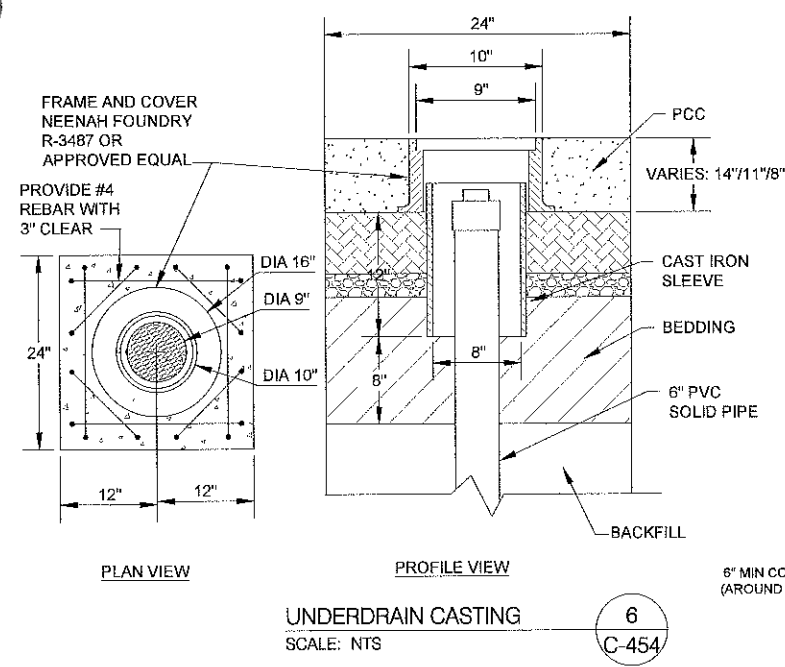
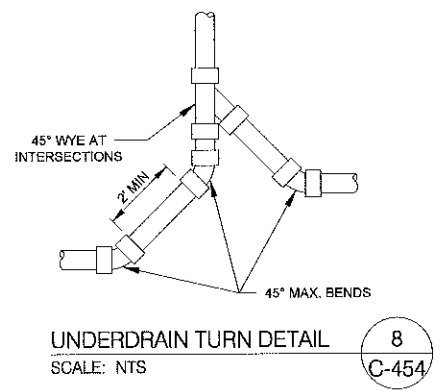
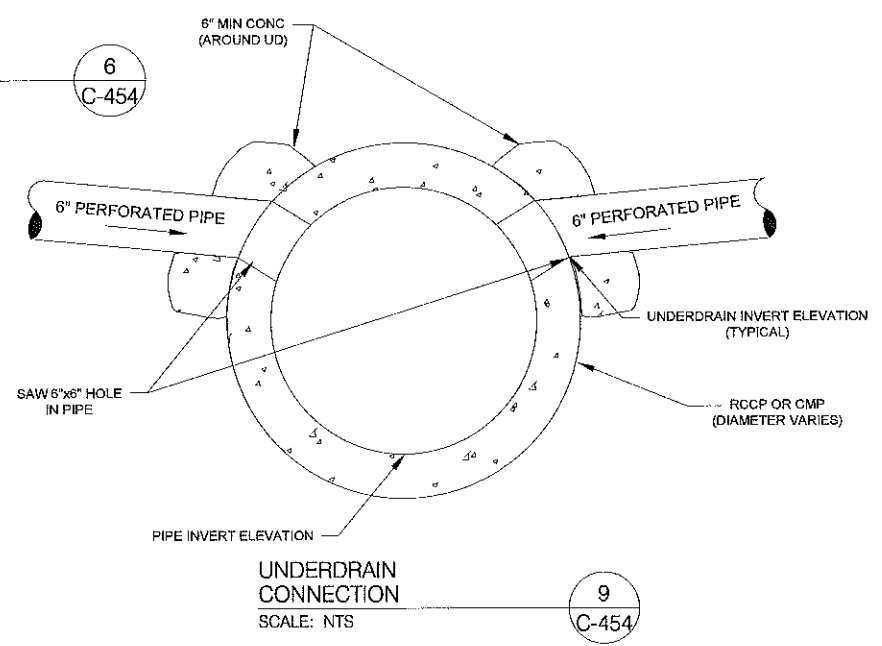
SHEET CONTENTS  
 UNDERDRAIN DETAILS

SHEET NO.

**C-454**

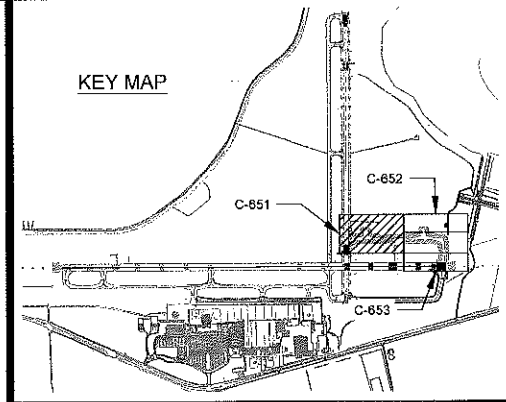
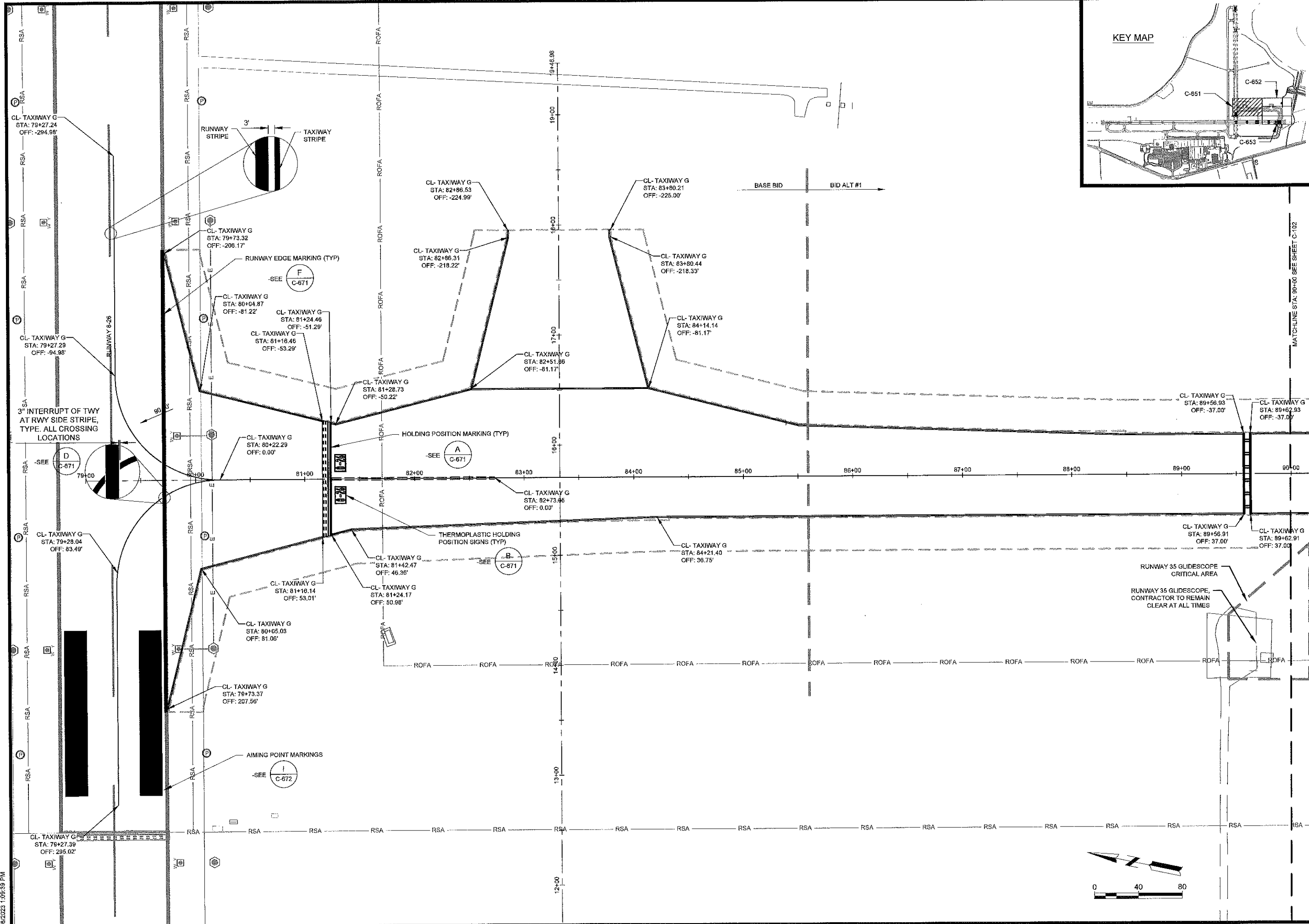


NOTE:  
 1. CONCRETE ENCASE BOTTOM OF FITTINGS FOR PLASTIC PIPE.



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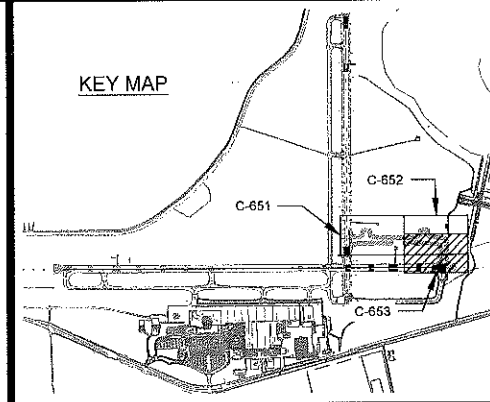
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SHEET CONTENTS  
MARKING PLAN

SHEET NO.

**C-651**





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CONSTRUCT TAXIWAY G**

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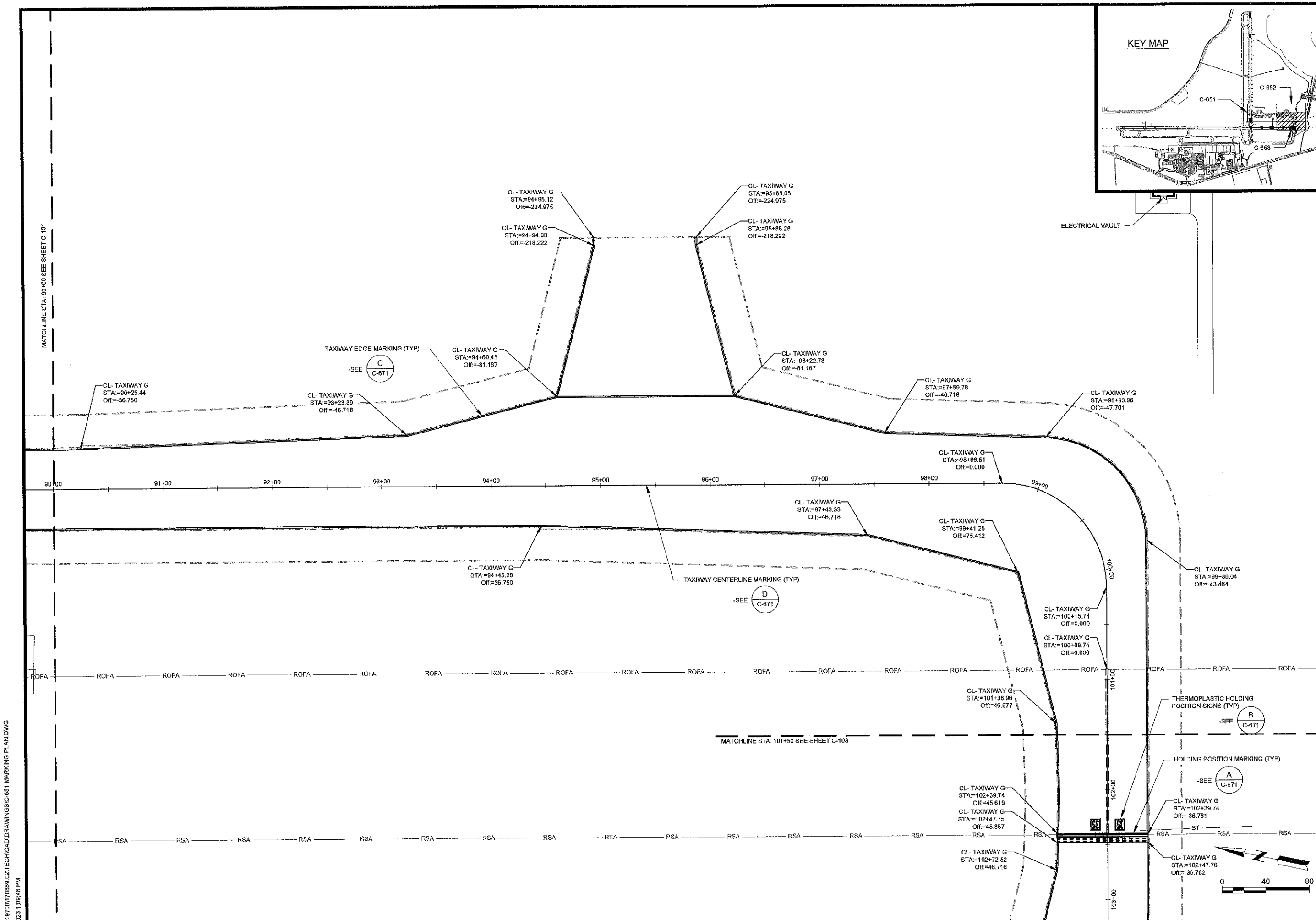
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SHEET CONTENTS  
MARKING PLAN

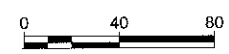
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**C-652**



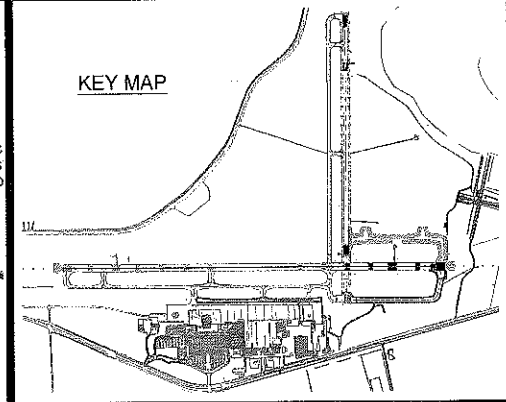
MATCHLINE STA: 90+00 SEE SHEET C-101

MATCHLINE STA: 101+50 SEE SHEET C-103



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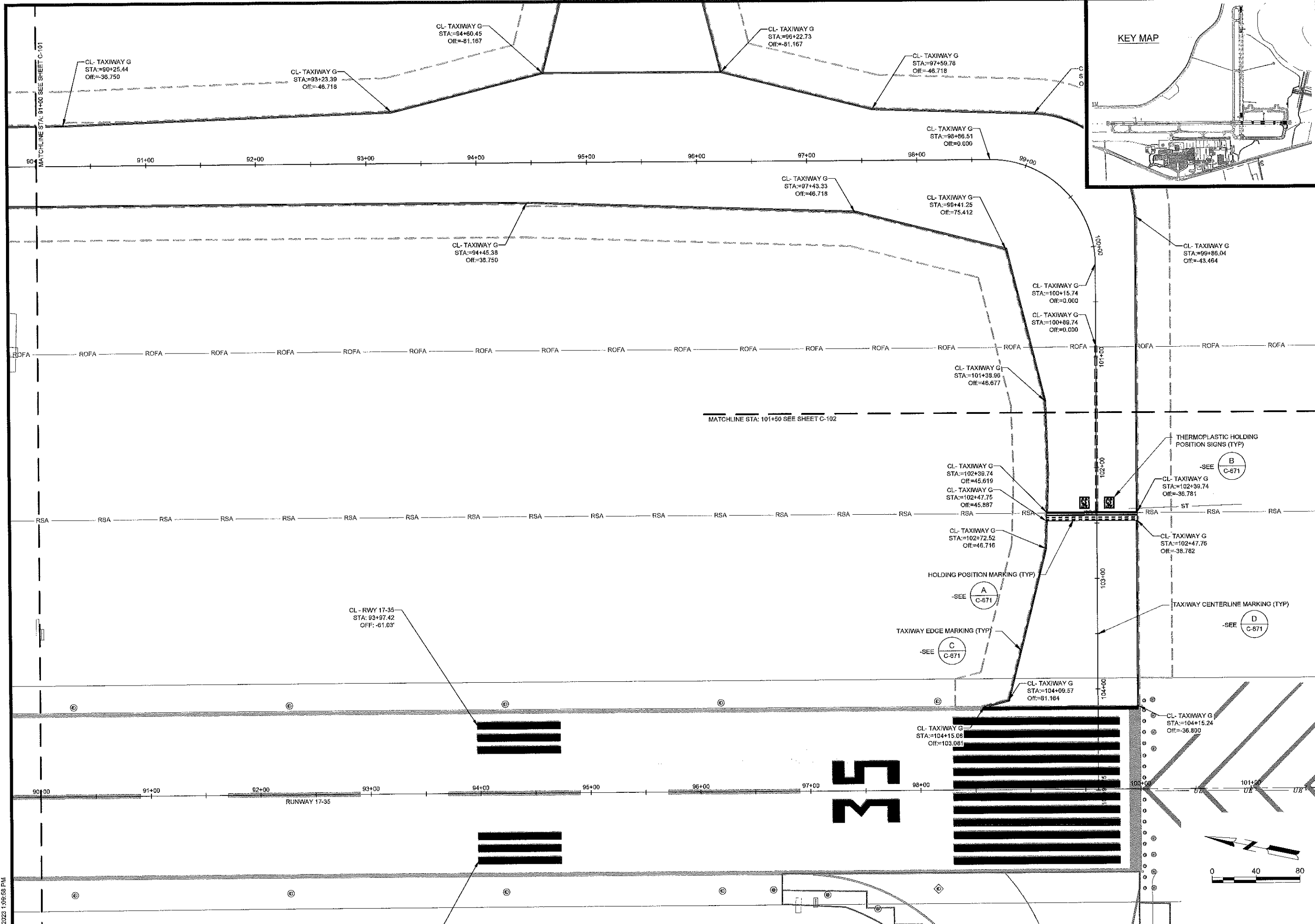
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SHEET CONTENTS  
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SHEET NO.

**C-653**

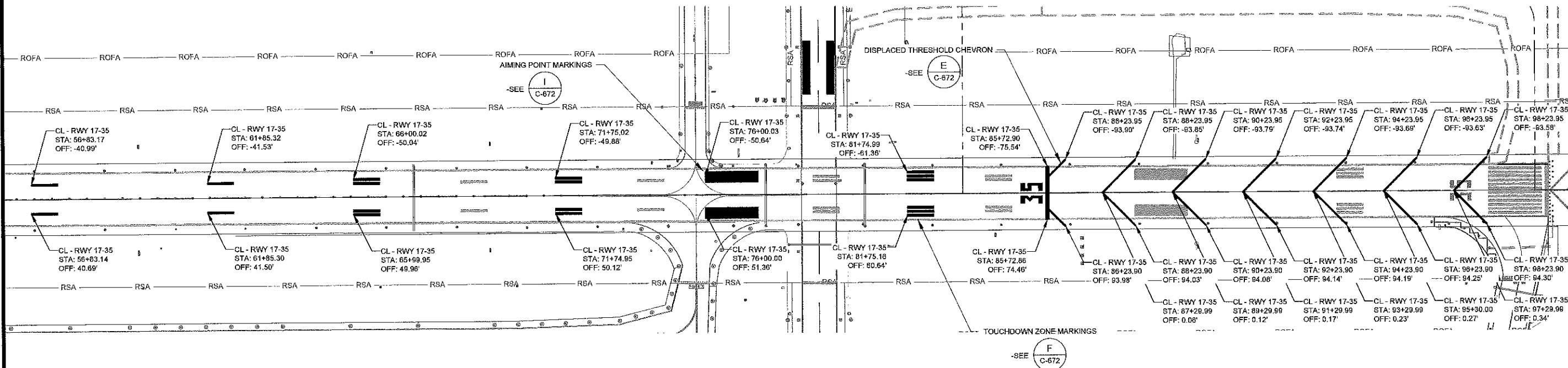


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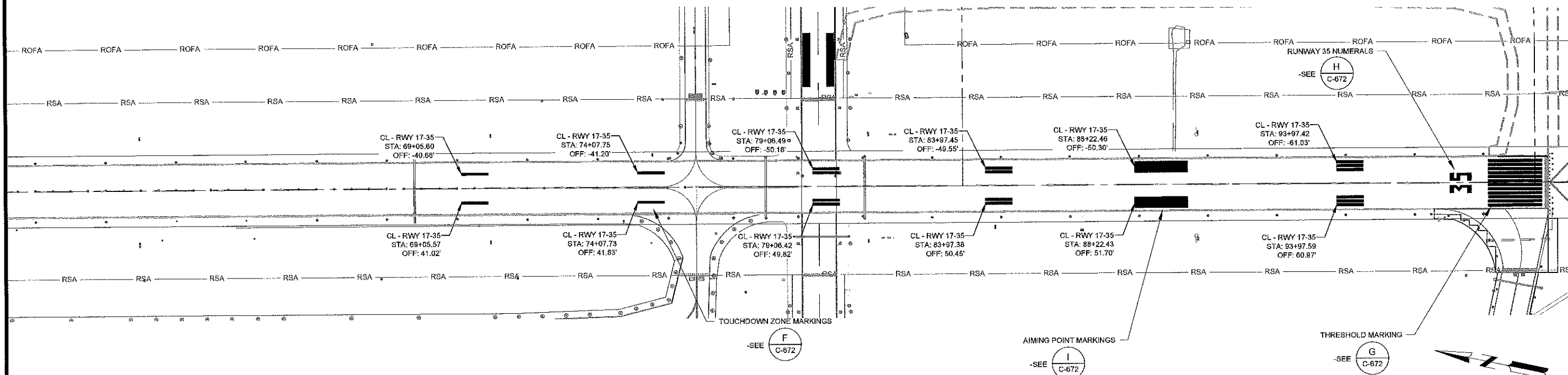


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**RUNWAY 17-35 TEMPORARY DISPLACED THRESHOLD MARKING PLAN**



**RUNWAY 17-35 FINAL MARKING PLAN**



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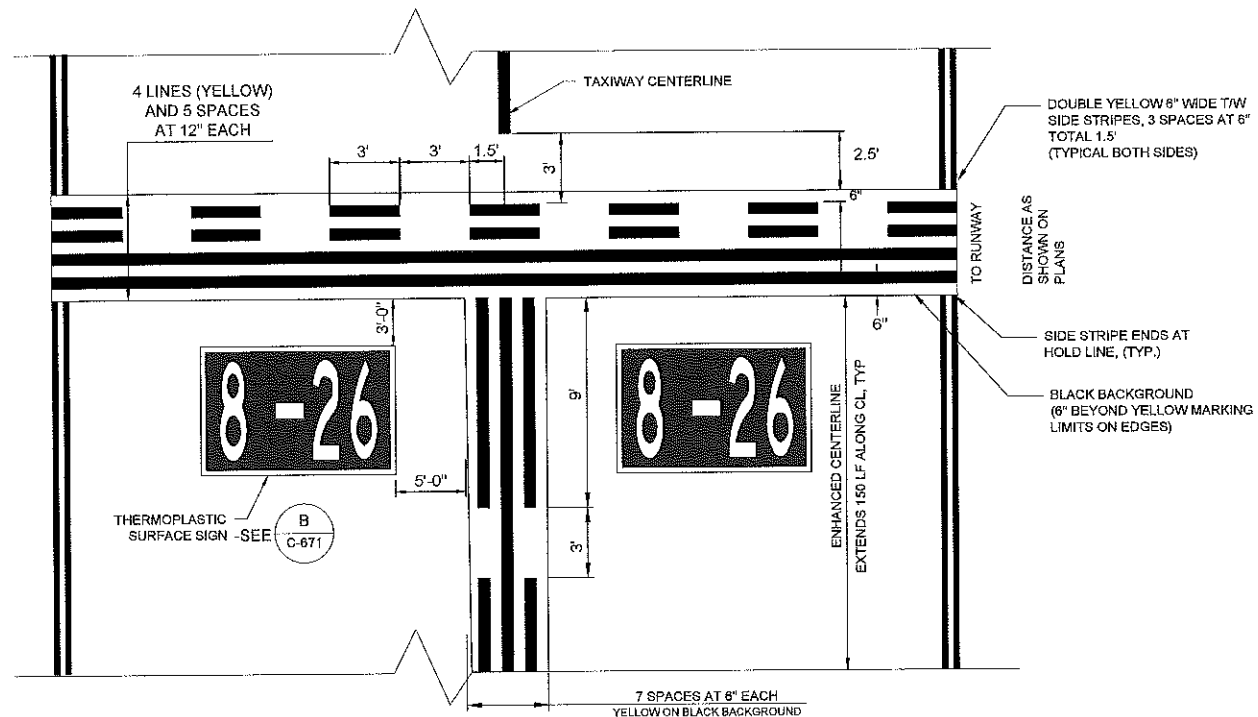
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SHEET CONTENTS  
MARKING PLAN -  
DISPLACED  
THRESHOLD

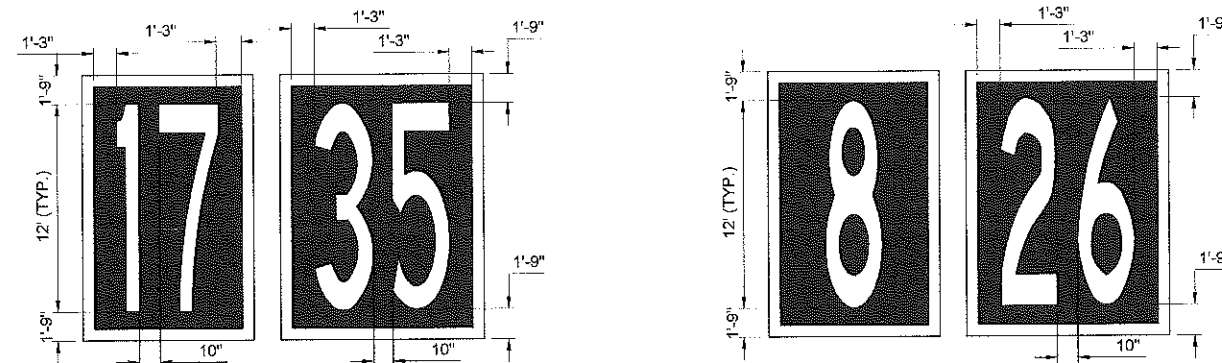
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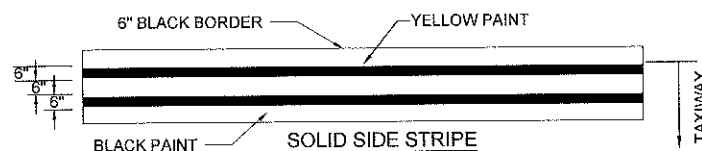
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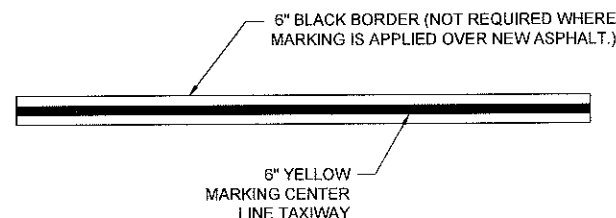
**A** ENHANCED TWY CENTERLINE MARKING AND THERMOPLASTIC HOLD LINE  
 NOT DRAWN TO SCALE



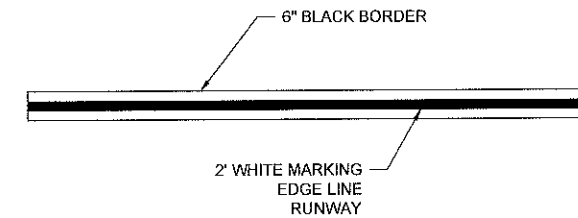
**B** THERMOPLASTIC HOLDING POSITION SIGNS  
 NOT DRAWN TO SCALE



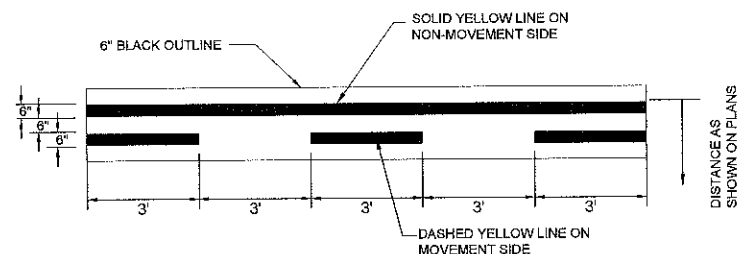
**C** TAXIWAY EDGE MARKING  
 NOT DRAWN TO SCALE



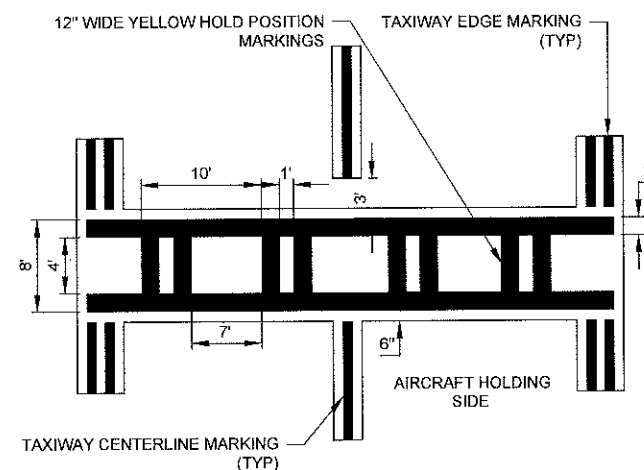
**D** TAXIWAY CENTERLINE AND LEAD-IN MARKING  
 NOT DRAWN TO SCALE



**E** RUNWAY EDGE MARKING  
 NOT DRAWN TO SCALE



**D** NON-MOVEMENT AREA MARKING  
 NOT DRAWN TO SCALE



**E** ILS HOLD POSITION MARKING  
 NOT DRAWN TO SCALE

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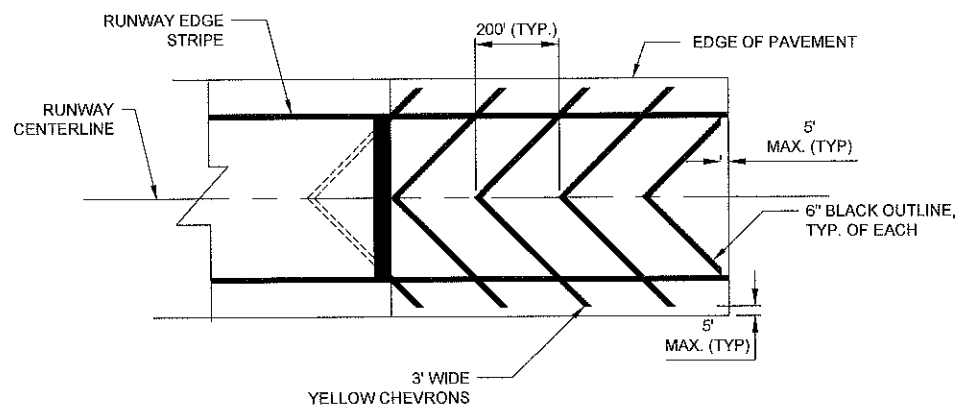
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 MARKING DETAILS

SHEET NO.

**C-671**

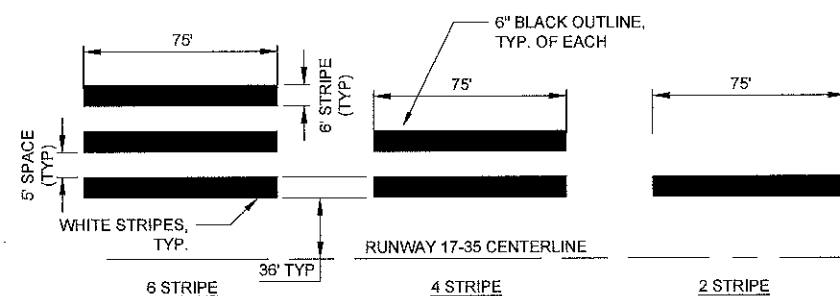


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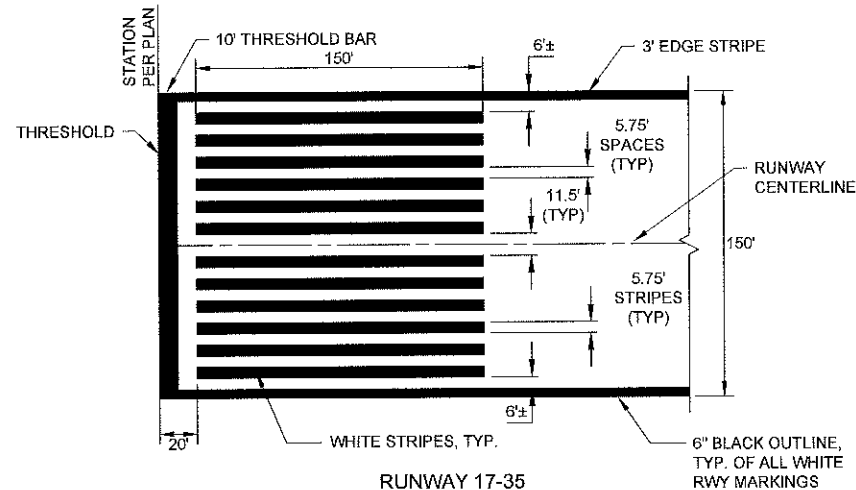


NOTE:  
 THE FIRST FULL CHEVRON STARTS AT THE INDEX POINT (INTERSECTION OF  
 RUNWAY CENTERLINE AND RUNWAY THRESHOLD TOE).

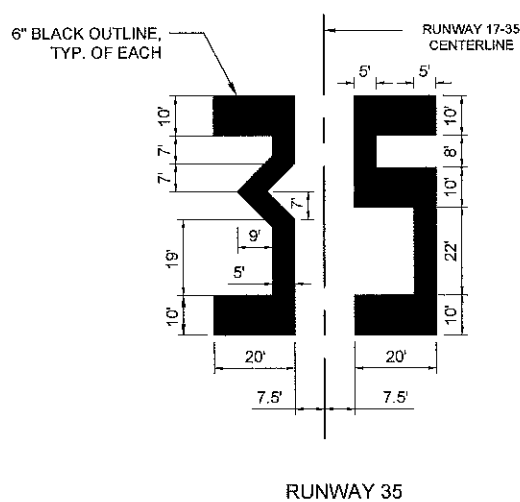
**E** DISPLACED THRESHOLD CHEVRONS  
 NOT DRAWN TO SCALE



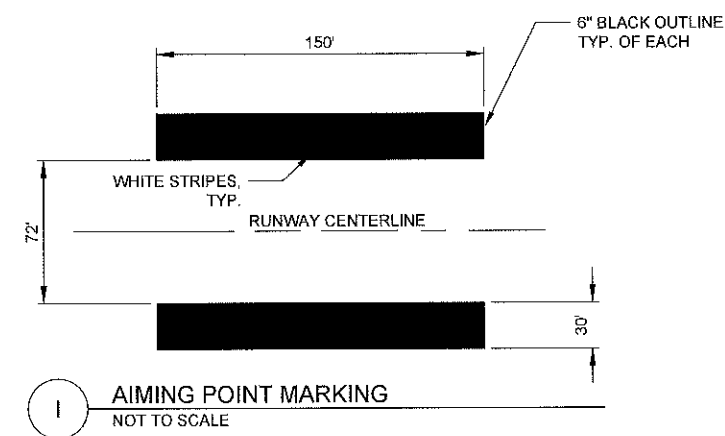
**F** TYPICAL TOUCHDOWN ZONE MARKING  
 NOT DRAWN TO SCALE



**G** THRESHOLD MARKING  
 NOT DRAWN TO SCALE



**H** RUNWAY 35 NUMERALS  
 NOT DRAWN TO SCALE



**I** AIMING POINT MARKING  
 NOT TO SCALE

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 MARKING DETAILS

SHEET NO.

C-672

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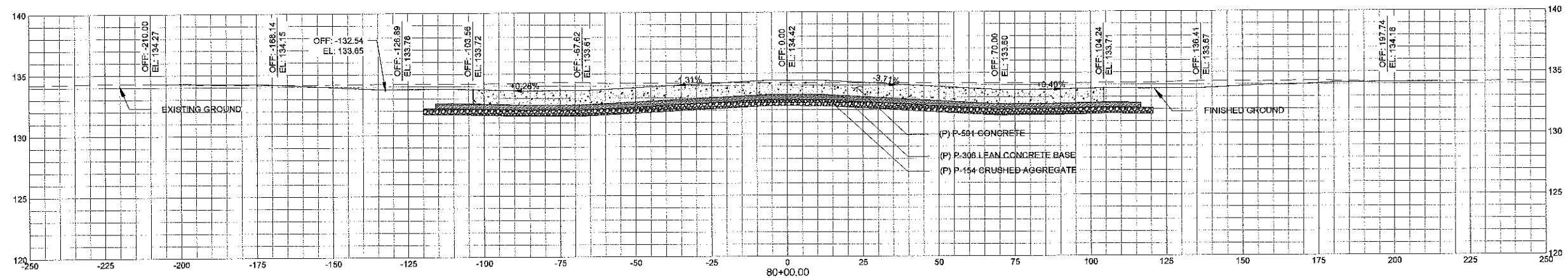
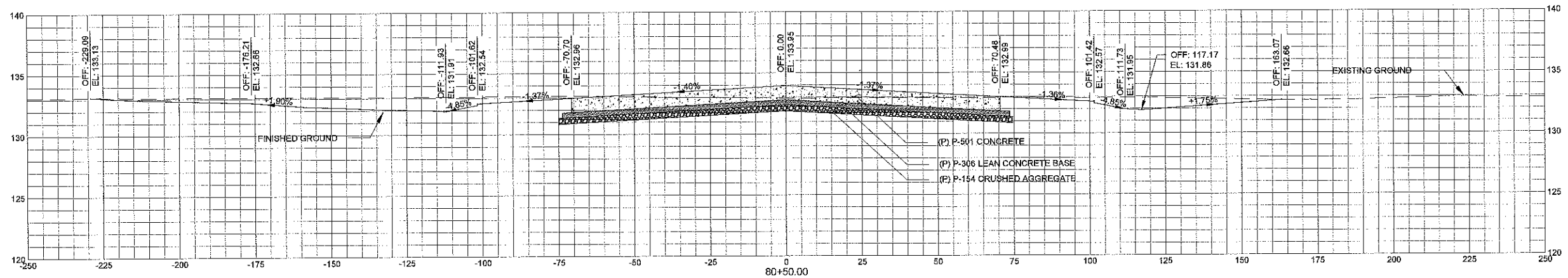
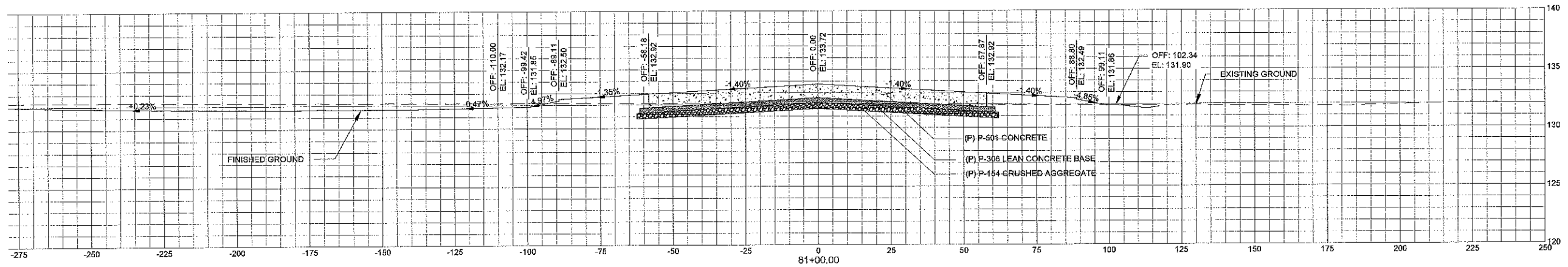
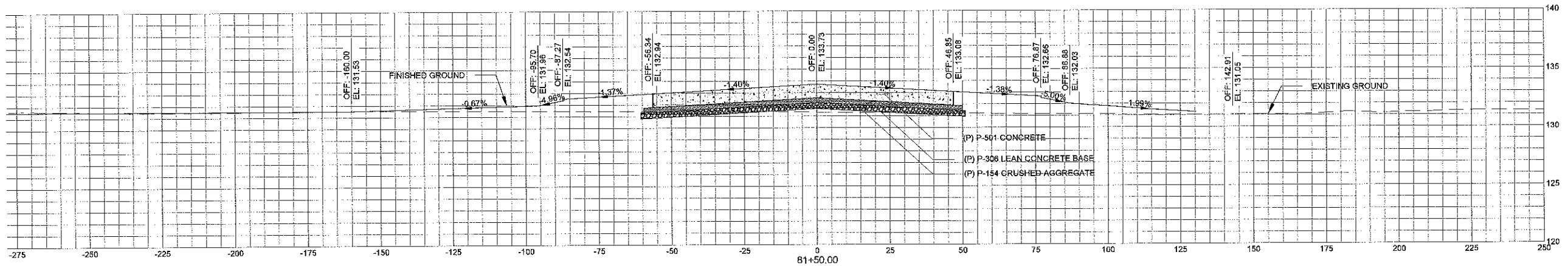
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**C-901**



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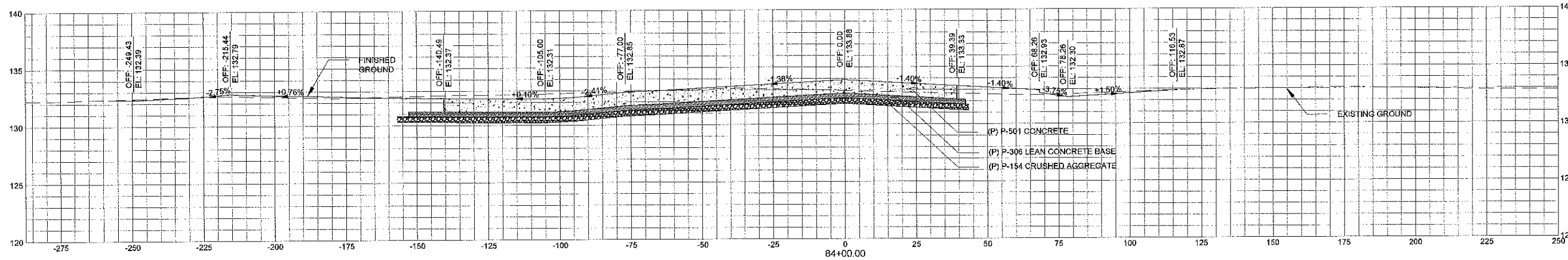
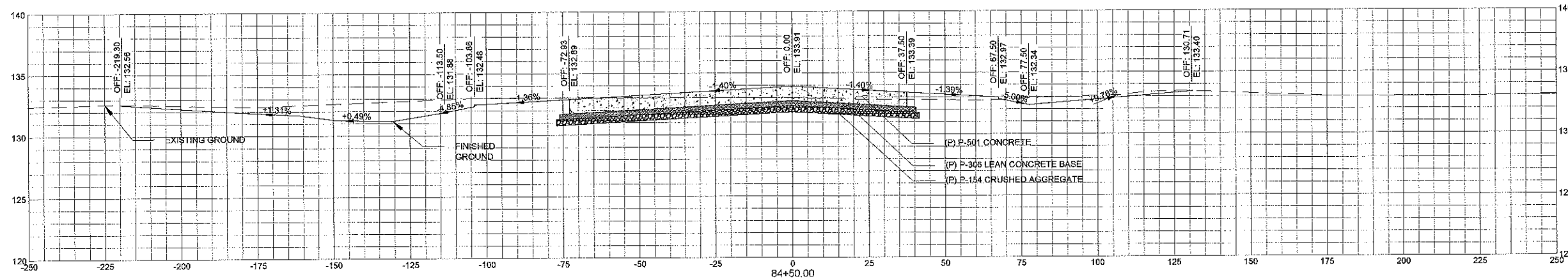
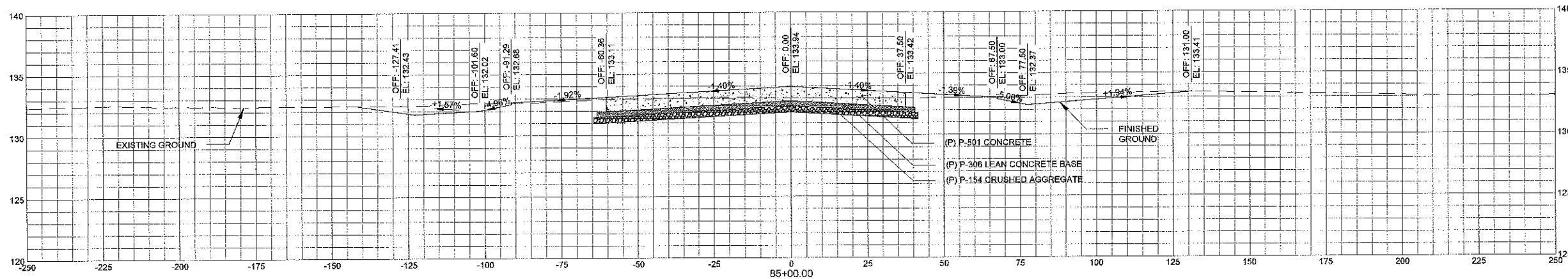
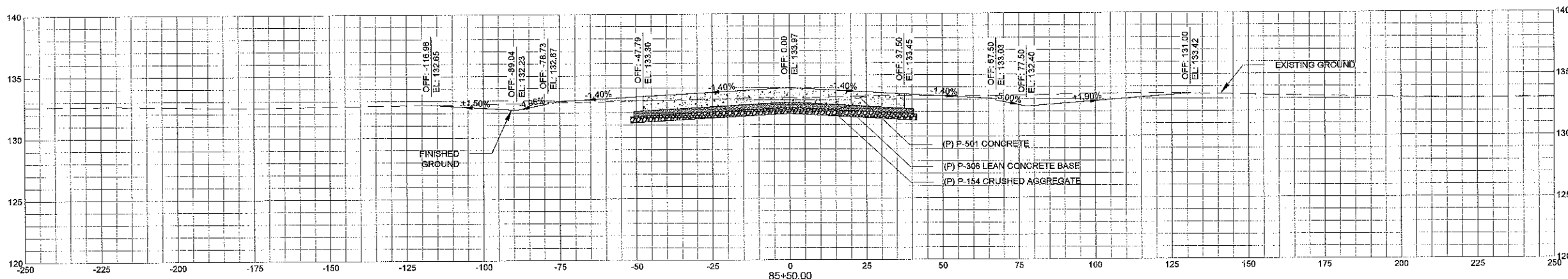
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**C-902**



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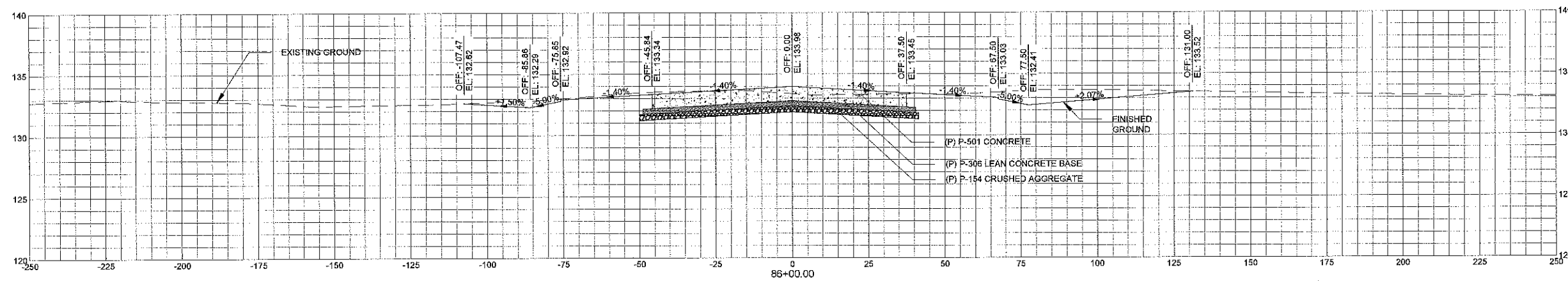
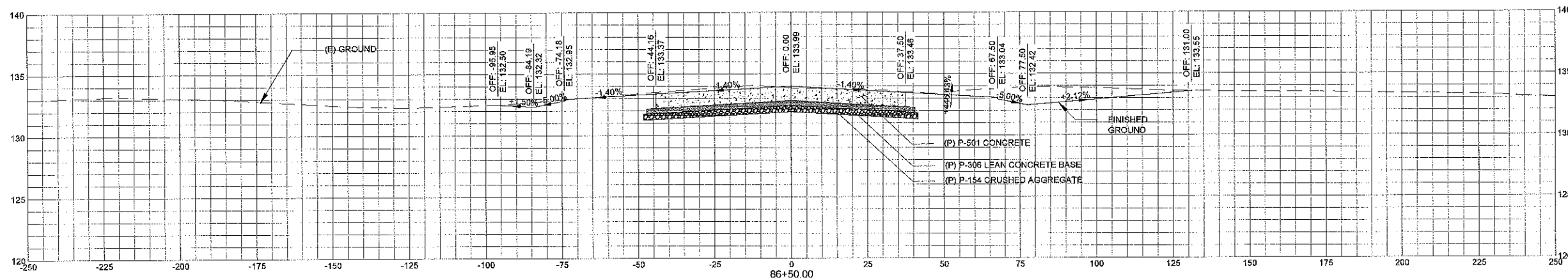
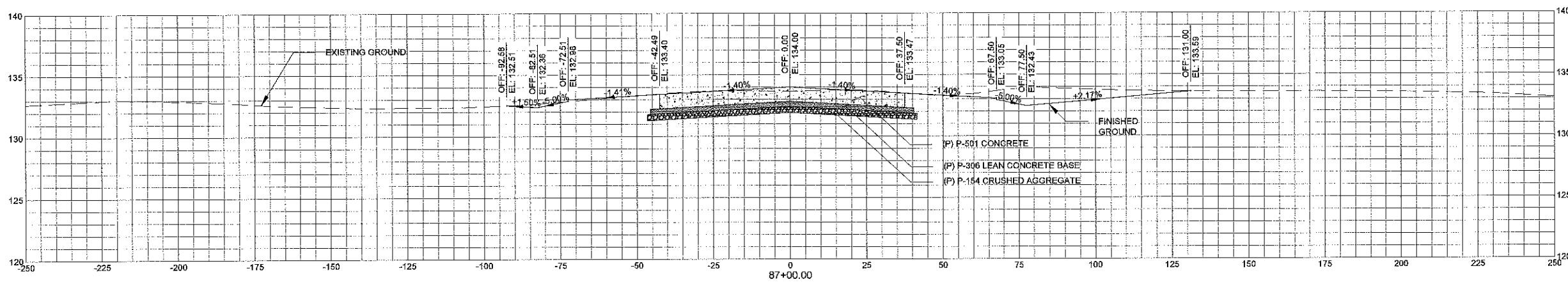
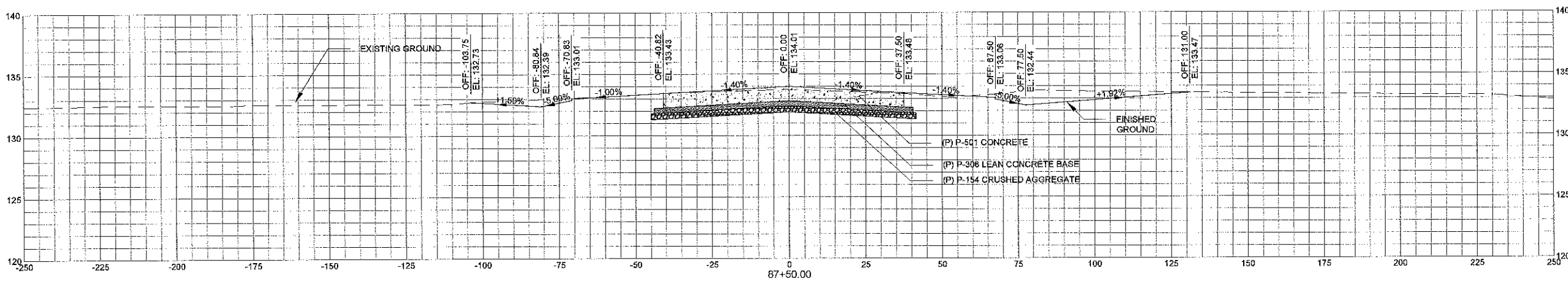
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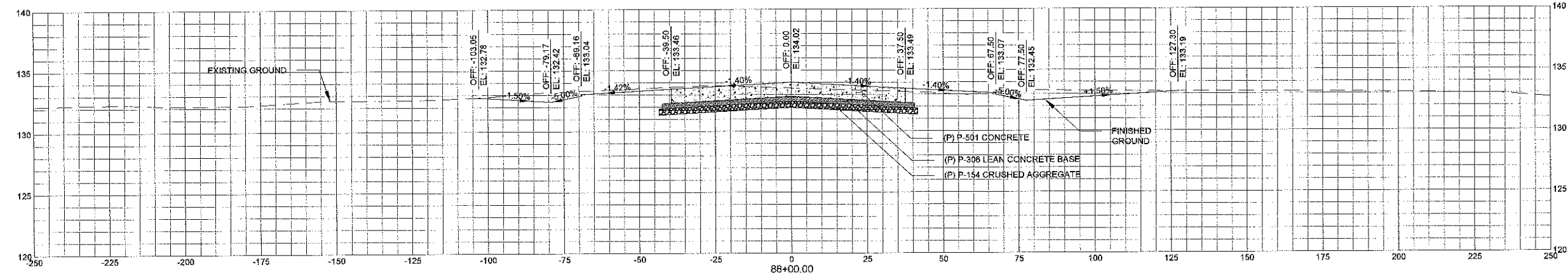
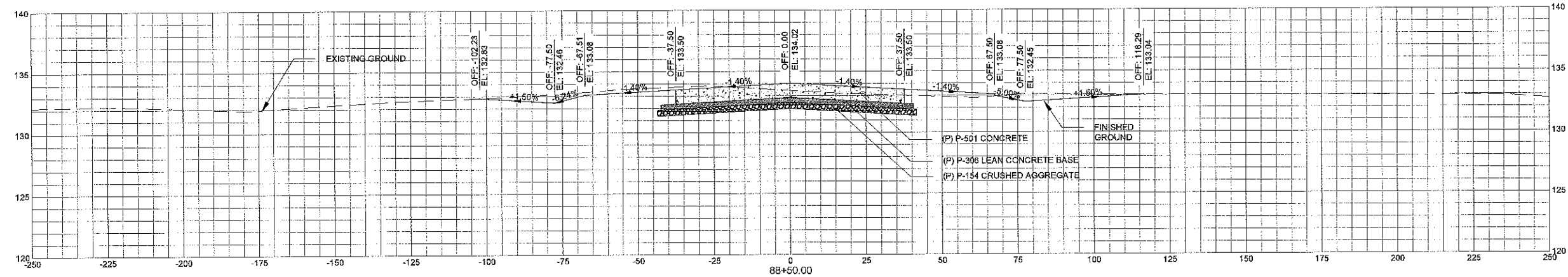
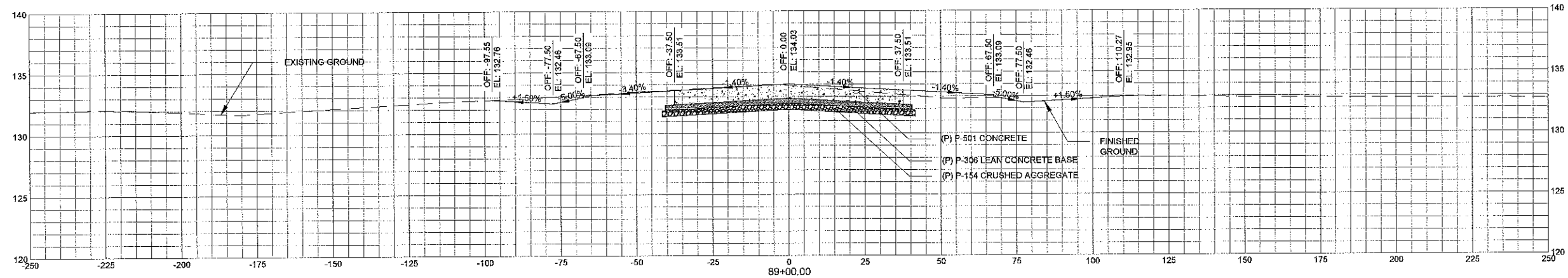
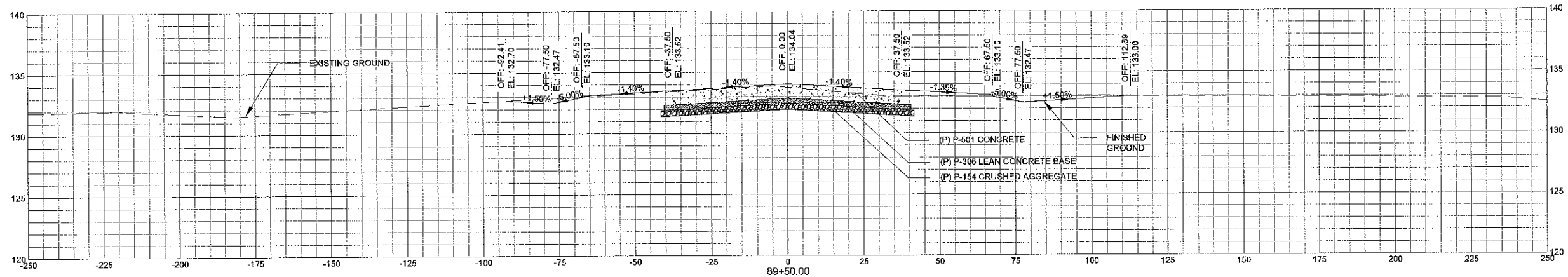
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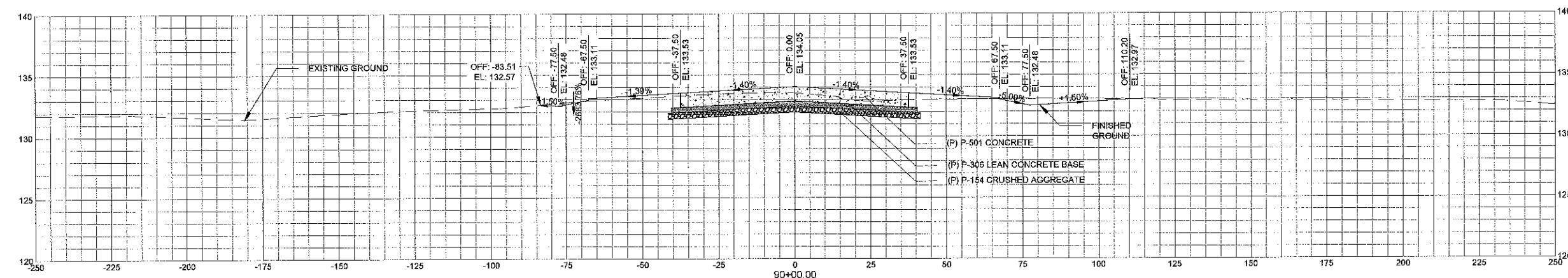
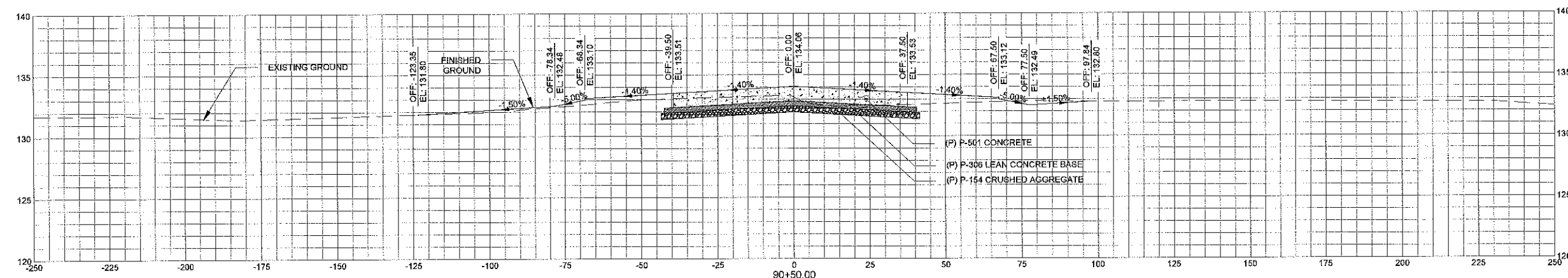
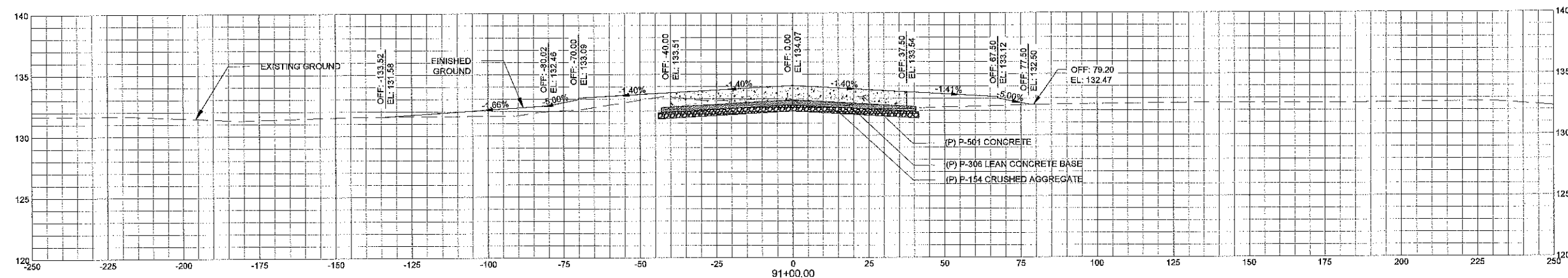
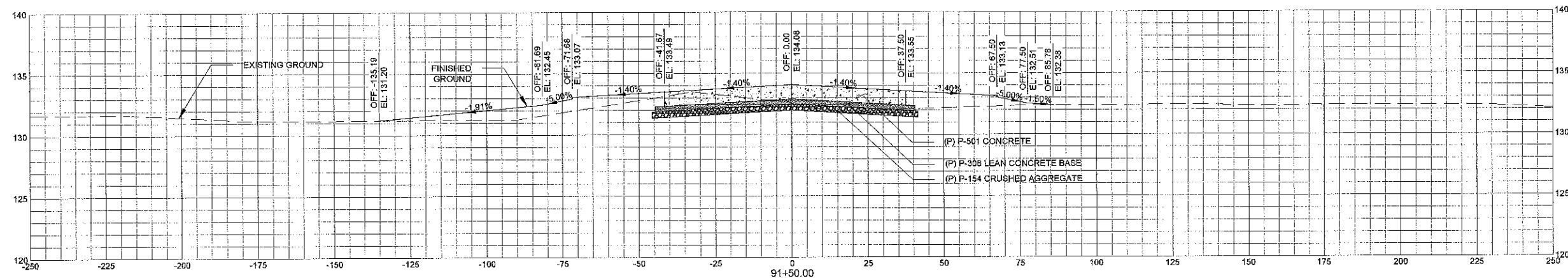
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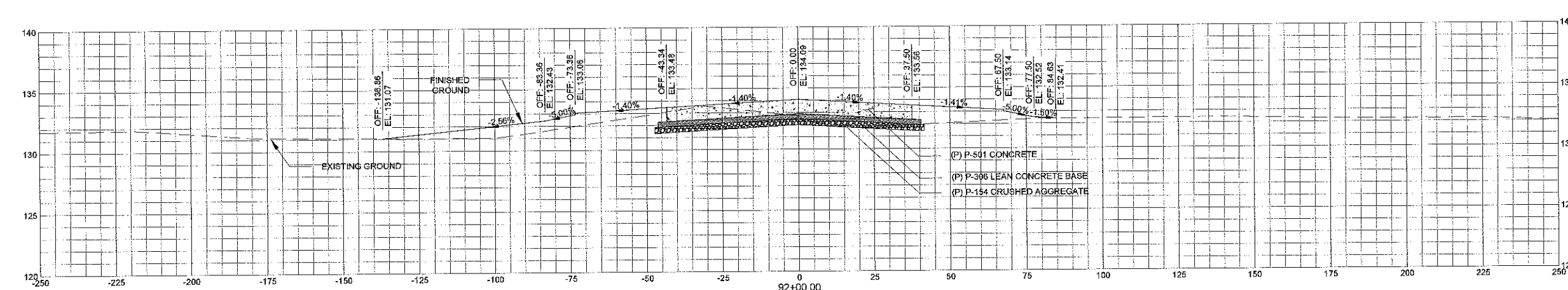
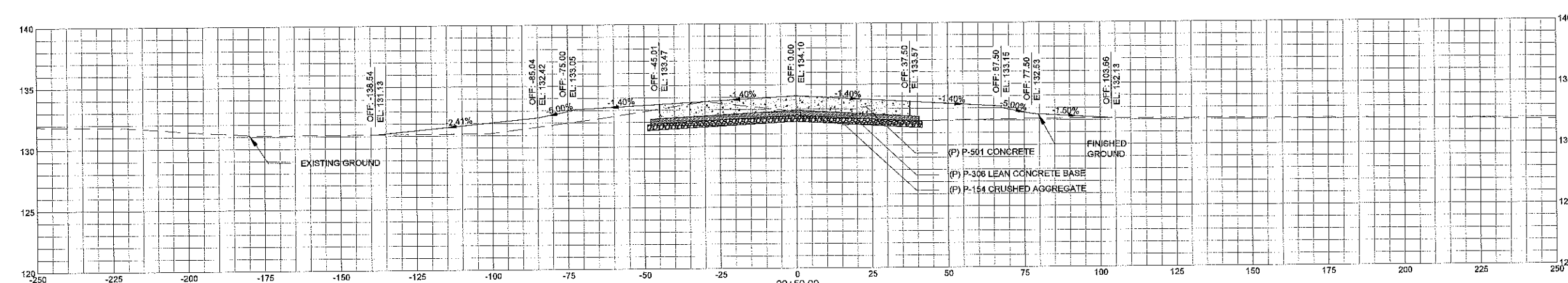
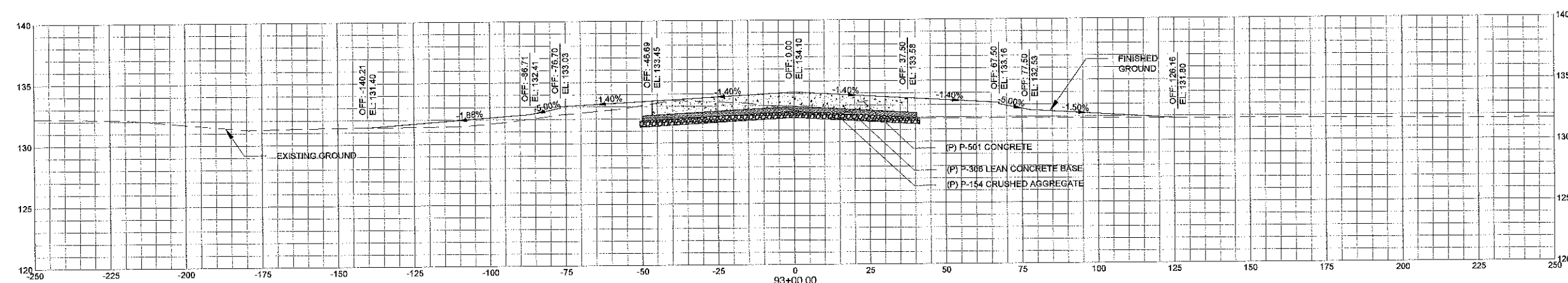
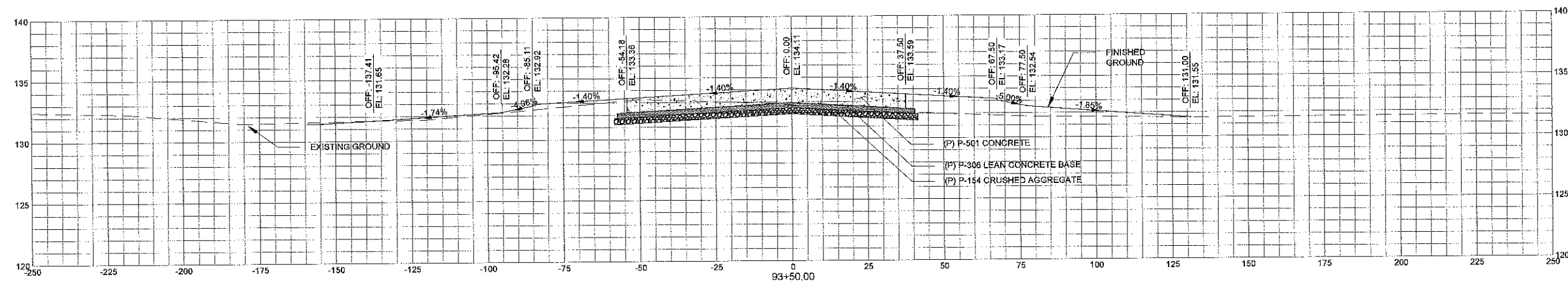
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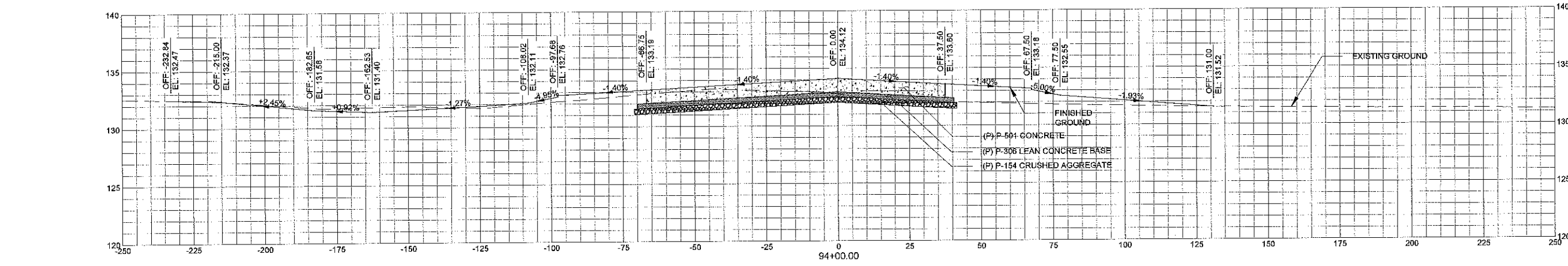
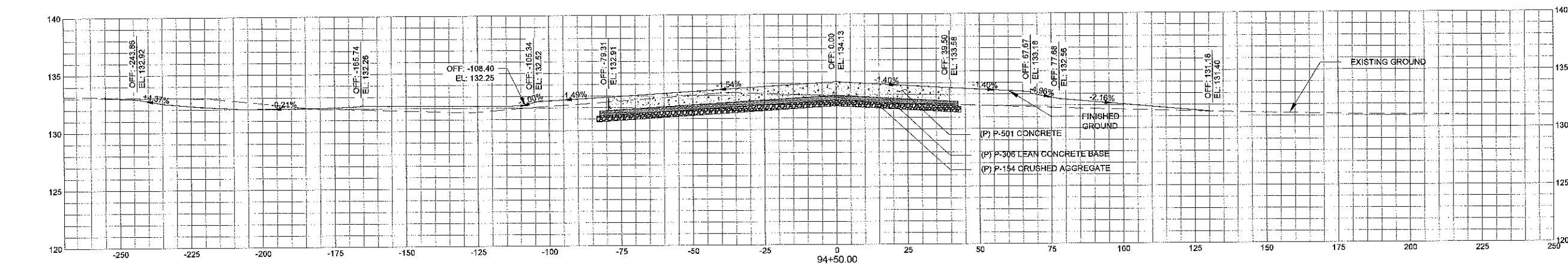
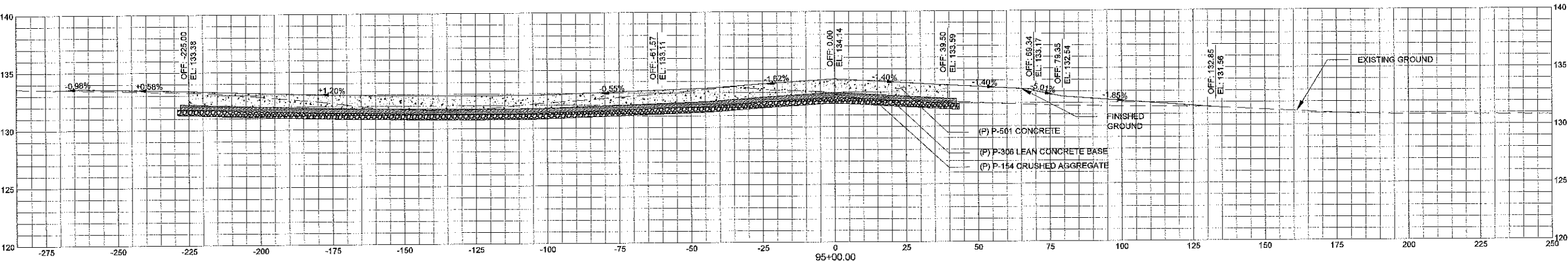
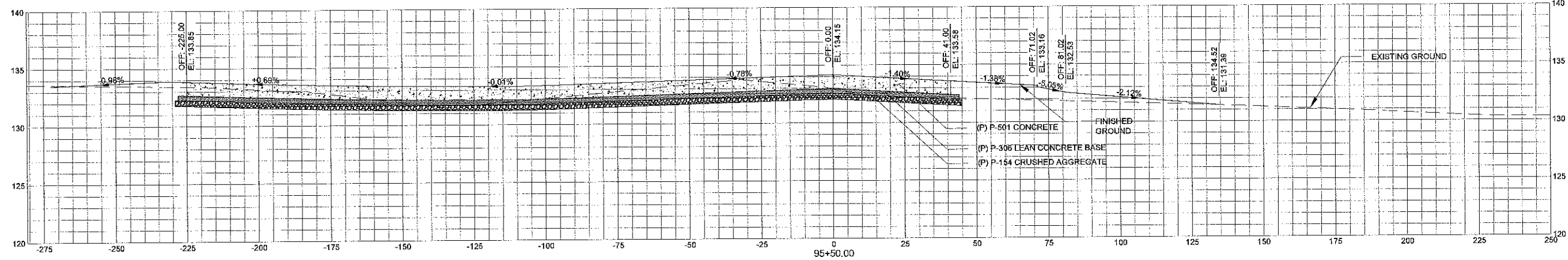
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**C-907**



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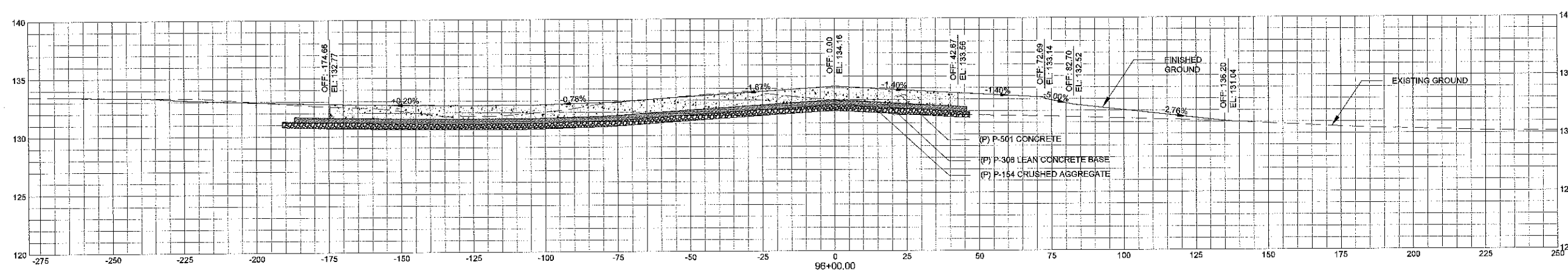
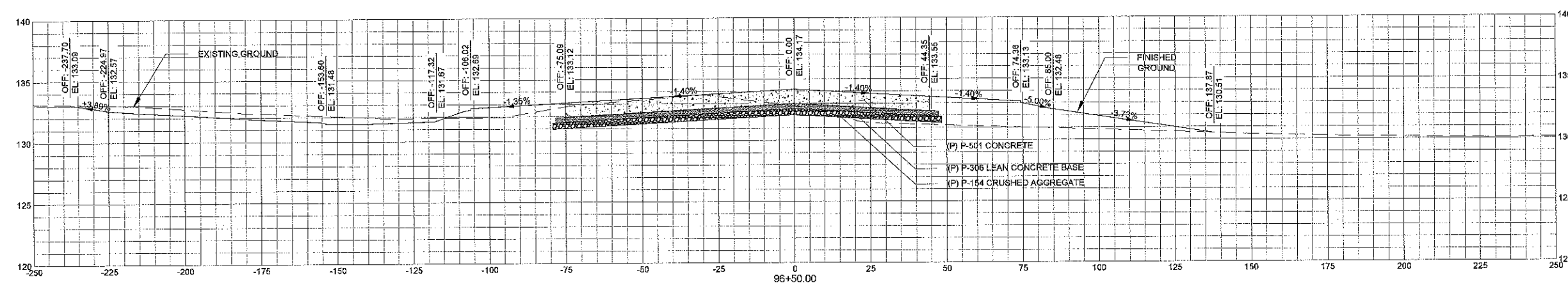
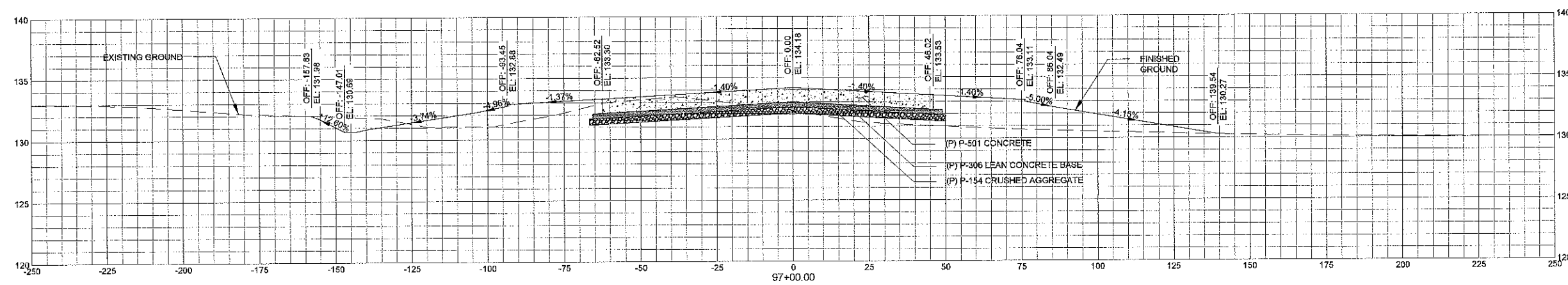
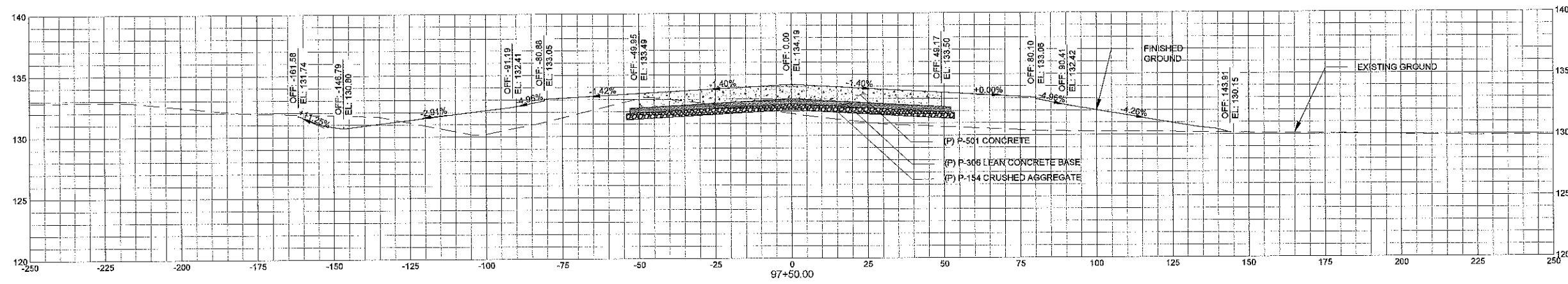
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**C-908**



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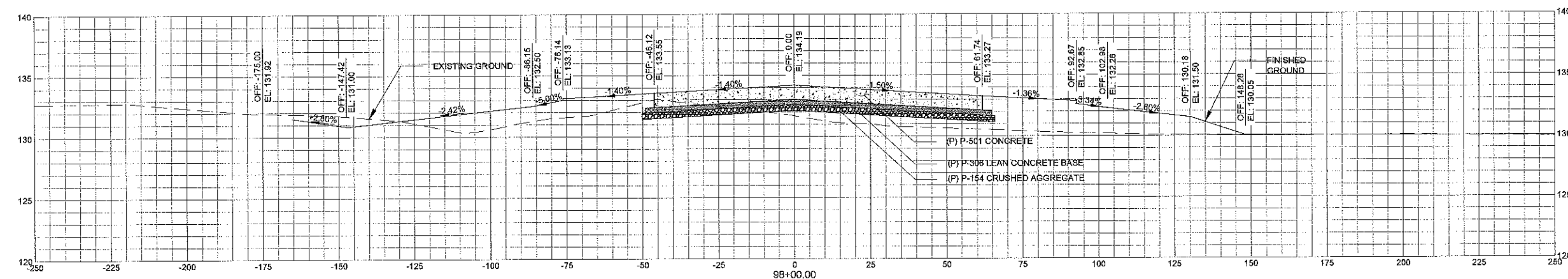
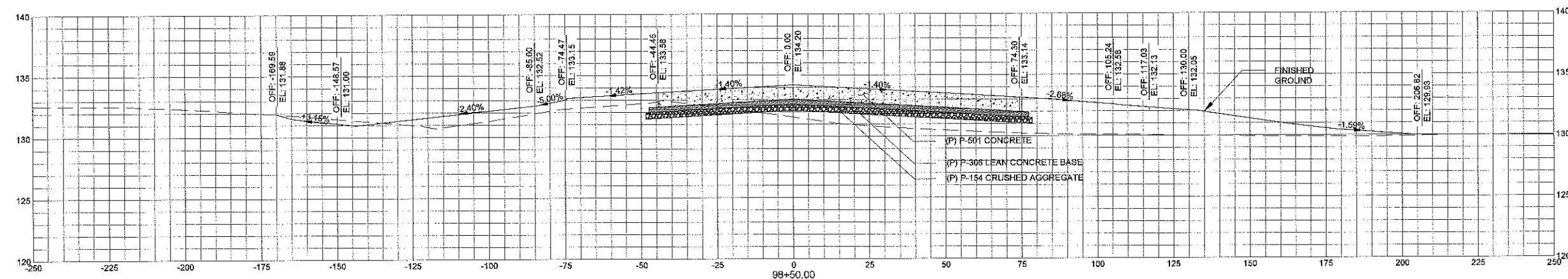
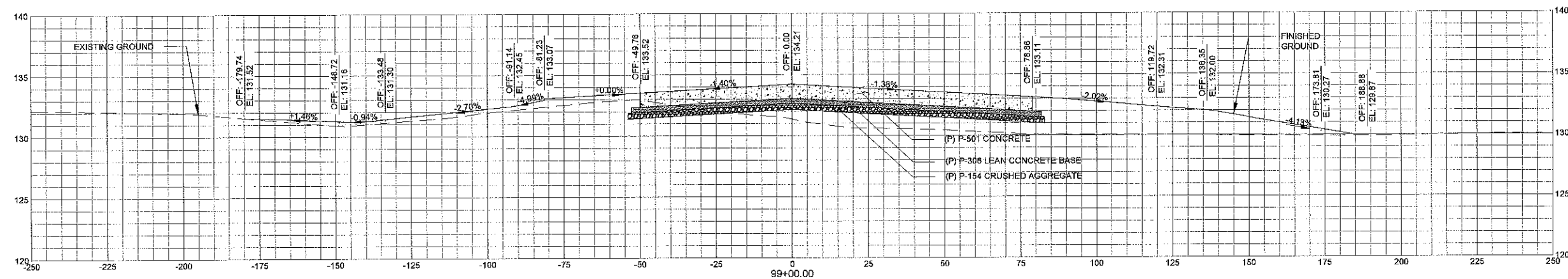
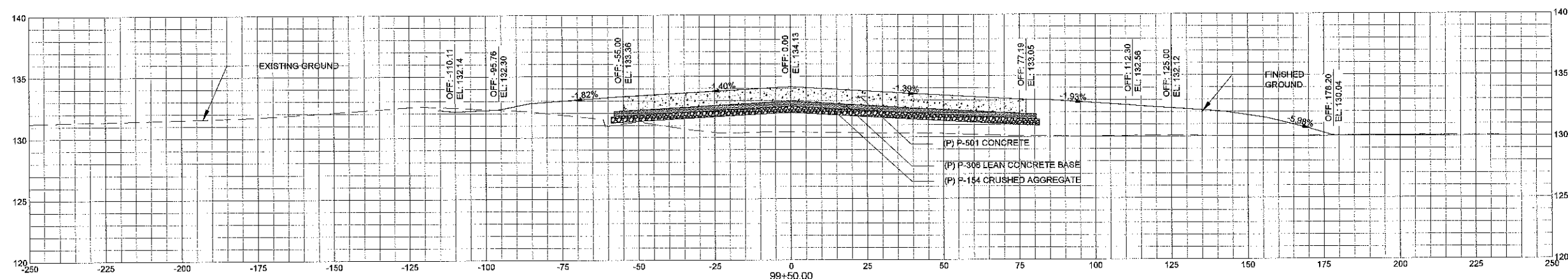
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**C-909**





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**AUGUSTA REGIONAL AIRPORT  
 CONSTRUCT TAXIWAY G**

1501 AVIATION WAY  
 AUGUSTA, GA 30906-9620

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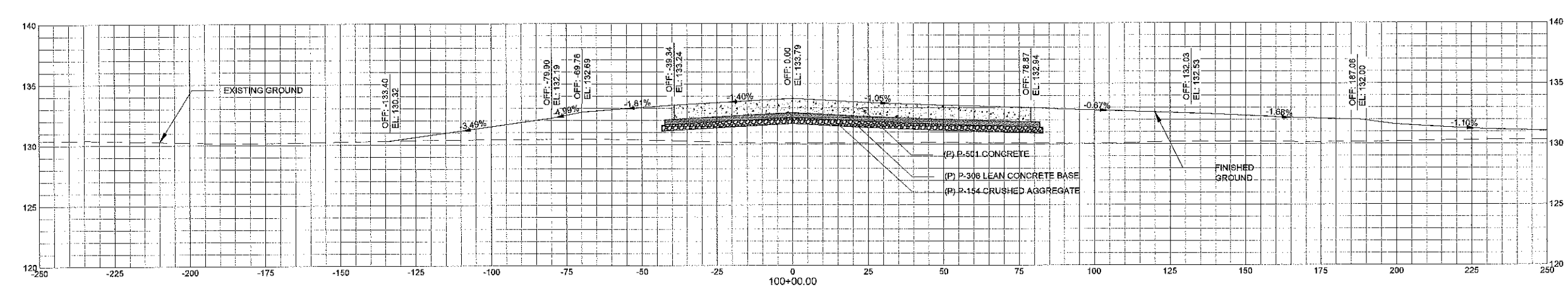
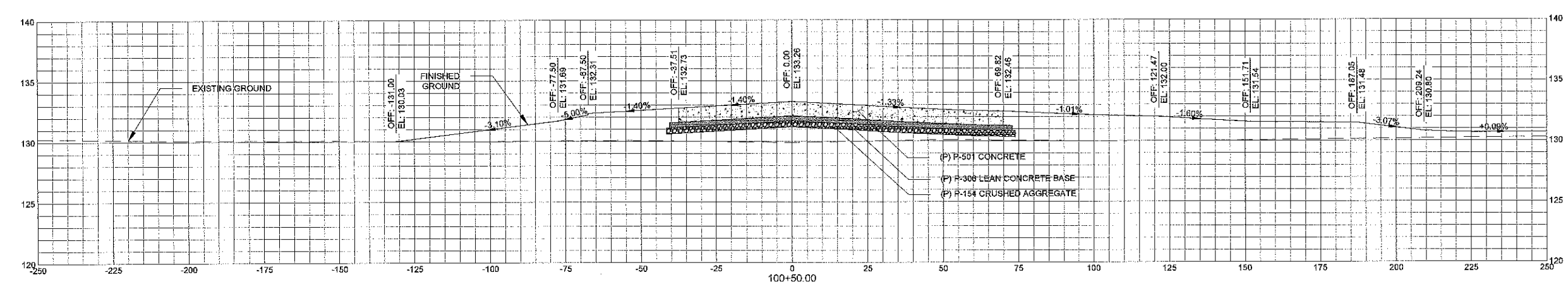
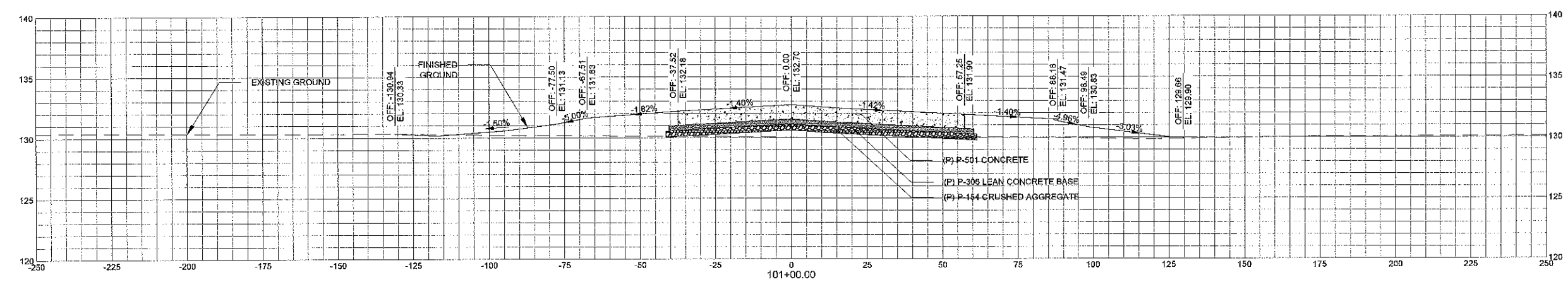
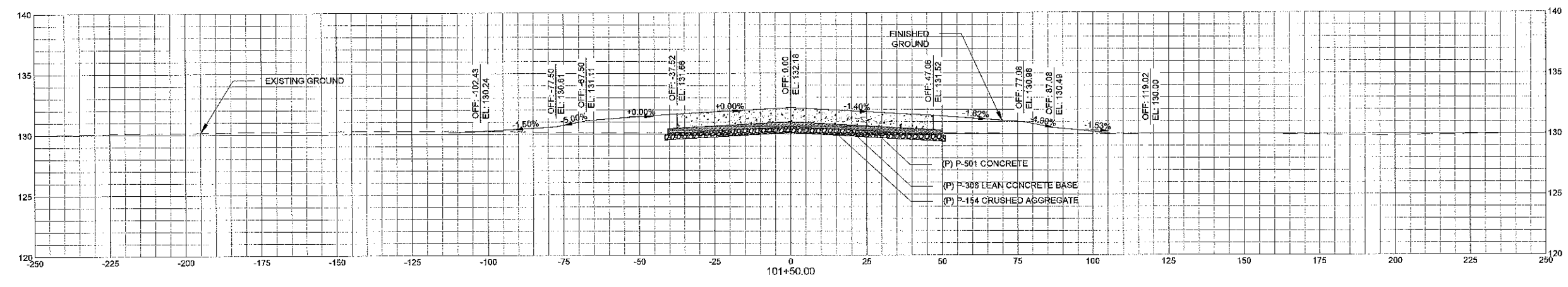
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**C-910**



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**AUGUSTA REGIONAL AIRPORT  
 CONSTRUCT TAXIWAY G**

1501 AVIATION WAY  
 AUGUSTA, GA 30906-9620

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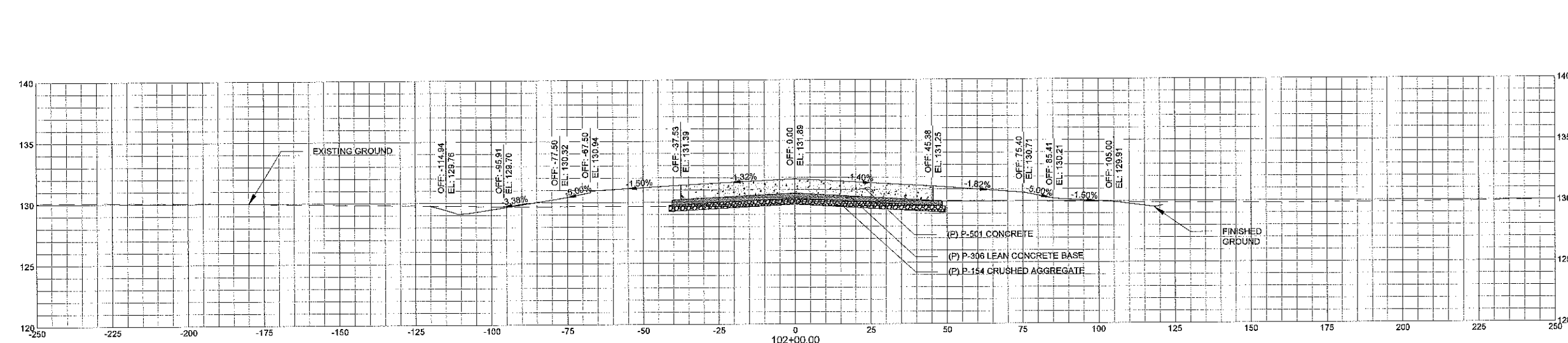
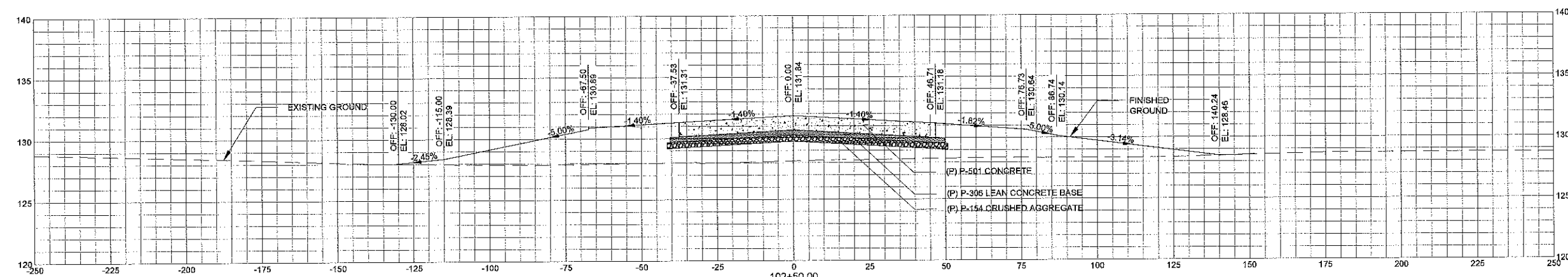
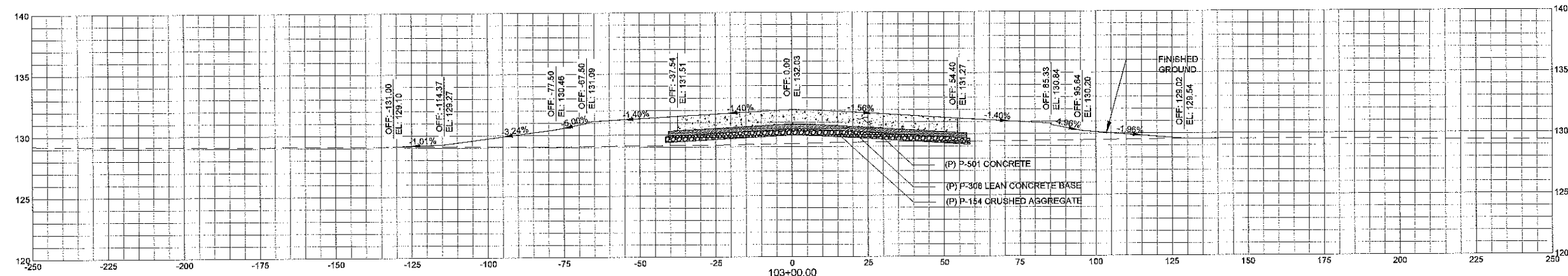
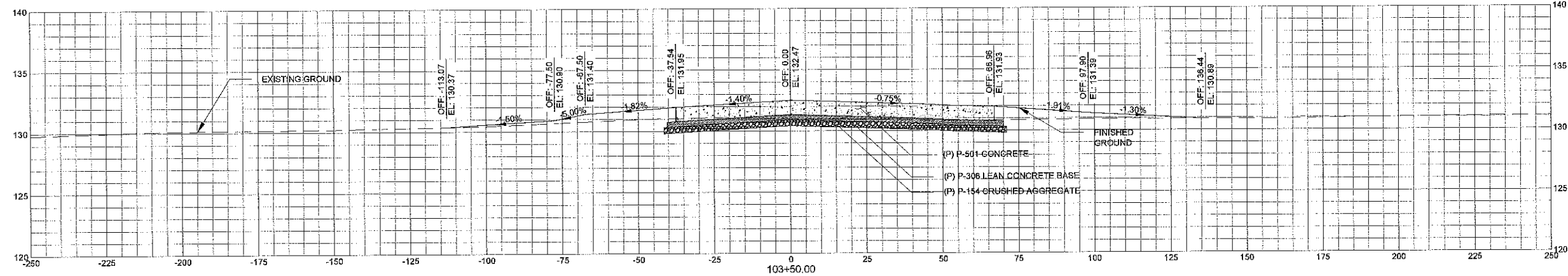
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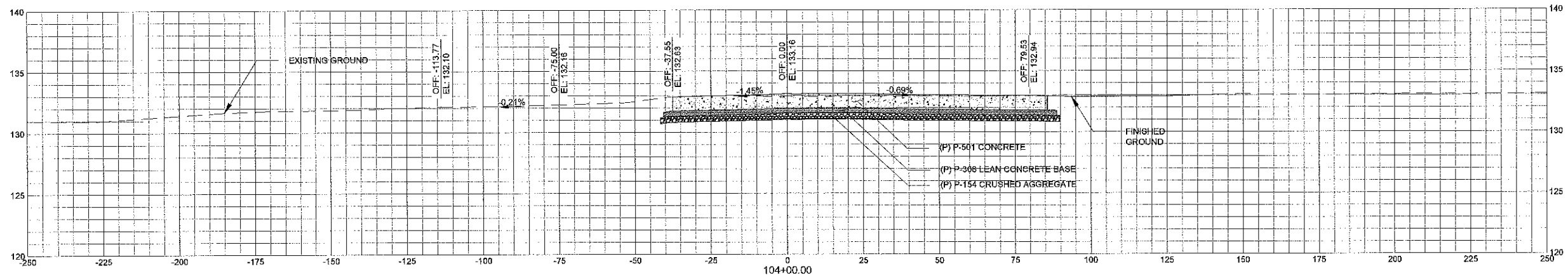
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**Mead & Hunt**

Mead and Hunt, Inc.  
878 South Lake Drive  
Lexington, SC 29072  
phone: 803-996-2900  
meadhunt.com



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**AUGUSTA REGIONAL AIRPORT  
CONSTRUCT TAXIWAY G**

1501 AVIATION WAY  
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**AUGUSTA REGIONAL AIRPORT  
 CONSTRUCT TAXIWAY G**

1501 AVIATION WAY  
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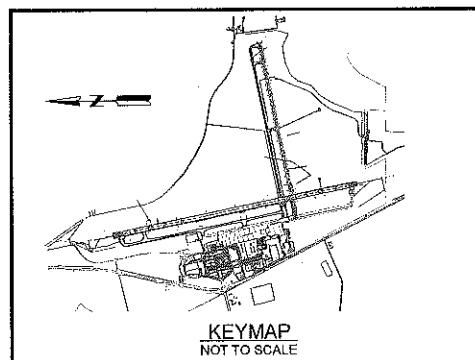
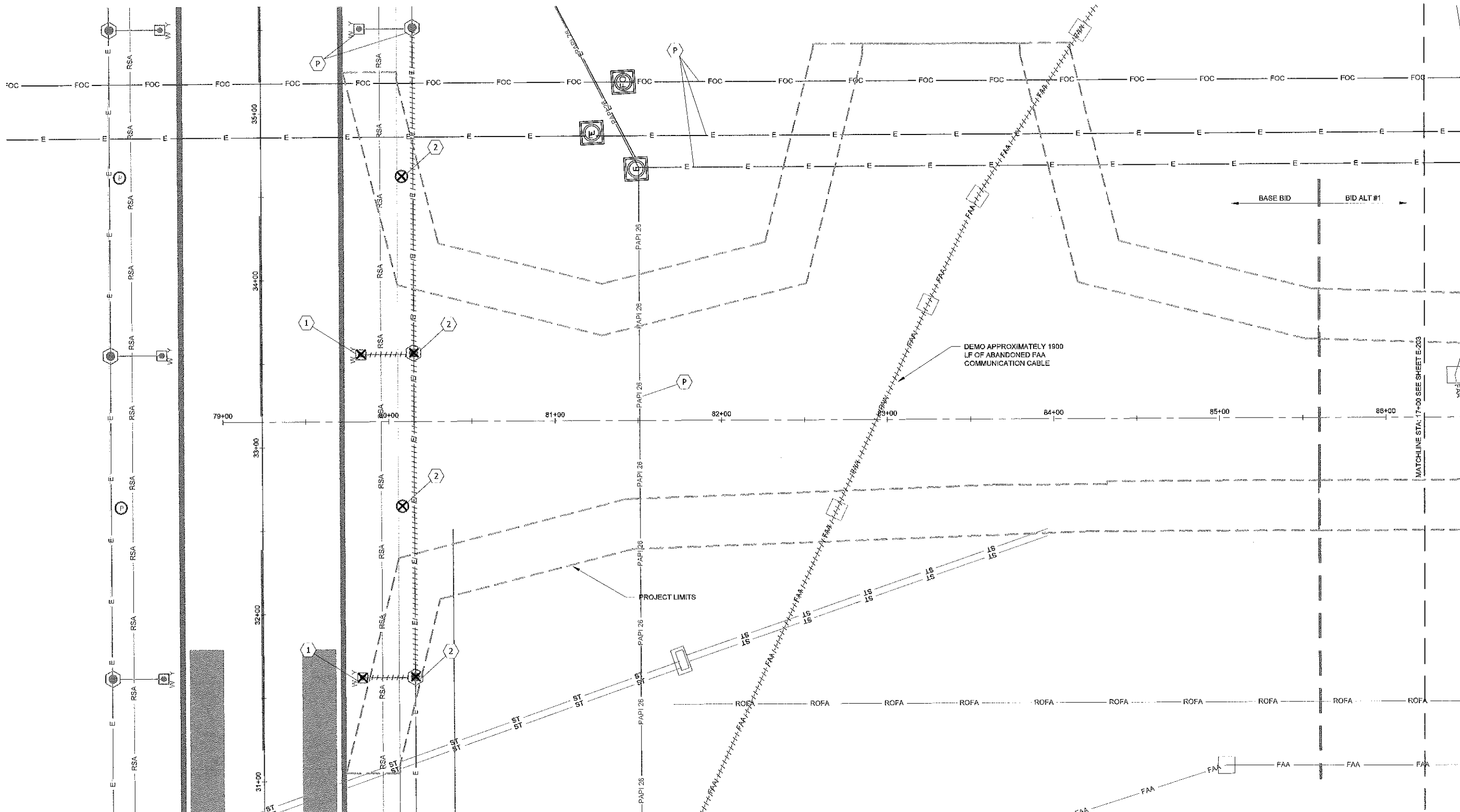
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SHEET CONTENTS  
 ELECTRICAL DEMO  
 PLAN

SHEET NO.

**E-101**



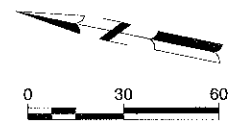
**ELECTRICAL LEGEND**

- EDGE LIGHT REMOVAL
- GUIDANCE SIGN REMOVAL
- REMOVE (E) CONDUCTORS UNLESS OTHERWISE NOTED
- REMOVE (E) CONDUIT UNLESS OTHERWISE NOTED
- (E) PVC CONDUIT UNLESS OTHERWISE NOTED
- (E) FAA FACILITIES
- ABANDON (E) CONDUCTORS OR CONDUITS

**ELECTRICAL KEY NOTES**

- 1** REMOVE (E) TAXIWAY EDGE LIGHT FIXTURE AND BASE CAN.
- 2** REMOVE (E) RUNWAY EDGE LIGHT FIXTURE BASE CAN, AND 2" PVC. SALVAGE FIXTURE TO THE AIRPORT
- 3** REPLACE (E) FIXTURE, AND TRANSFORMER, CONNECT WITH (N) CABLE
- 4** REMOVE (E) CONCRETE ENCASED CONDUIT. CONTRACTOR TO VERIFY CIRCUITS AND MAINTAIN CIRCUITS NECESSARY FOR AIRFIELD OPERATIONS WITH JUMPERS
- 5** REMOVE (E) FIXTURE AND TRANSFORMER. REPLACE WITH (N) CAN COVER.
- 6** REMOVE (E) ELECTRICAL PULLBOX OR JUNCTION CAN. CONTRACTOR TO VERIFY CIRCUITS, AND MAINTAIN CIRCUITS NECESSARY FOR AIRFIELD OPERATIONS.
- 7** INSTALL TEMPORARY #8, 5KV JUMPER CABLES, AS NECESSARY TO KEEP RWY AND/OR TWY CIRCUIT ENERGIZED.
- 8** PROTECT (E) UTILITIES AT ALL TIMES DURING THE PROJECT.
- 9** CLEAN (E) CONDUIT PRIOR TO REUSING.
- P** CONTRACTOR TO PROTECT.

- NOTES:**
1. SEE SHEETS C-881 THROUGH C-892 FOR LIGHTING DETAILS DURING THRESHOLD RELOCATION.
  2. DIRECT BURIED CONDUITS AND CABLES WITHIN THE LIMITS OF EXCAVATION SHALL BE REMOVED AND THE REMOVAL SHALL BE CONSIDERED INCIDENTAL TO ITEM P-152 UNCLASSIFIED EXCAVATION. DIRECT BURIED CONDUITS AND CABLES NOT WITHIN THE LIMITS OF EXCAVATION SHALL BE ABANDONED IN PLACE UNLESS OTHERWISE NOTED.
  3. WARNING: THERE ARE A NUMBER OF EXISTING UTILITIES, INCLUDING DRAINAGE, SANITARY SEWER, WATER MAIN, ELECTRICAL, AND COMMUNICATIONS CABLES, TRaversing THE SITE. THE DESIGNER HAS ATTEMPTED TO DEPICT THE APPROXIMATE LOCATIONS OF THESE ITEMS CURRENTLY IN PLACE. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO HAVE EVERY EXISTING ITEM LOCATED, FLAGGED AND IDENTIFIED PRIOR TO START UP OF CONSTRUCTION AND TO MAINTAIN VISIBLE LOCATION THROUGHOUT THE CONSTRUCTION DURATION. THIS SHALL INCLUDE COORDINATION WITH ALL NECESSARY AGENCIES INCLUDING THE AIRPORT. ANY DAMAGE DONE SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR SHALL REPAIR ANY UTILITY DAMAGED DURING THE CONSTRUCTION BY THE CONTRACTOR'S OPERATIONS AT NO COST TO THE AIRPORT.





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**AUGUSTA REGIONAL AIRPORT  
CONSTRUCT TAXIWAY G**

1501 AVIATION WAY  
AUGUSTA, GA 30906-9620

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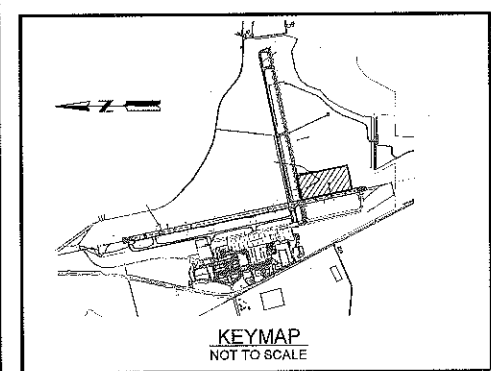
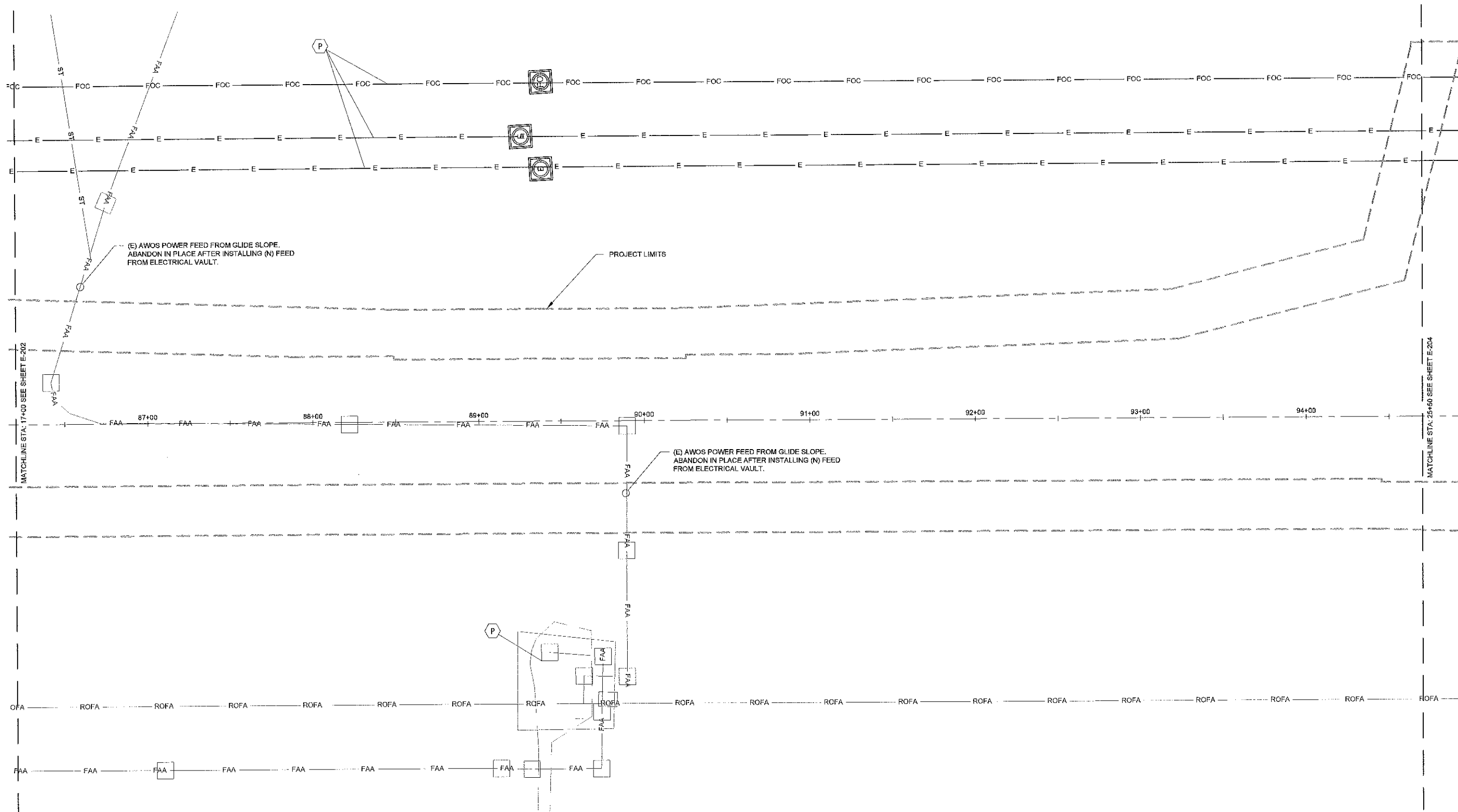
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SHEET CONTENTS  
**ELECTRICAL DEMO PLAN**

SHEET NO.

**E-102**



**ELECTRICAL LEGEND**

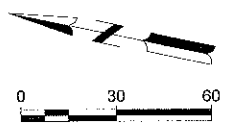
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- GUIDANCE SIGN REMOVAL
- REMOVE (E) CONDUCTORS UNLESS OTHERWISE NOTED
- REMOVE (E) CONDUIT UNLESS OTHERWISE NOTED
- (E) PVC CONDUIT UNLESS OTHERWISE NOTED
- (E) FAA FACILITIES
- ABANDON (E) CONDUCTORS OR CONDUITS

**ELECTRICAL KEY NOTES**

- 1 REMOVE (E) TAXIWAY EDGE LIGHT FIXTURE AND BASE CAN.
- 2 REMOVE (E) RUNWAY EDGE LIGHT FIXTURE BASE CAN, AND 2" PVC. SALVAGE FIXTURE TO THE AIRPORT
- 3 REPLACE (E) FIXTURE, AND TRANSFORMER, CONNECT WITH (N) CABLE
- 4 REMOVE (E) CONCRETE ENCASED CONDUIT. CONTRACTOR TO VERIFY CIRCUITS AND MAINTAIN CIRCUITS NECESSARY FOR AIRFIELD OPERATIONS WITH JUMPERS
- 5 REMOVE (E) FIXTURE AND TRANSFORMER. REPLACE WITH (N) CAN COVER.
- 6 REMOVE (E) ELECTRICAL PULLBOX OR JUNCTION CAN. CONTRACTOR TO VERIFY CIRCUITS, AND MAINTAIN CIRCUITS NECESSARY FOR AIRFIELD OPERATIONS.
- 7 INSTALL TEMPORARY #8, 5KV JUMPER CABLES, AS NECESSARY TO KEEP RWY AND/OR TWY CIRCUIT ENERGIZED.
- 8 PROTECT (E) UTILITIES AT ALL TIMES DURING THE PROJECT.
- 9 CLEAN (E) CONDUIT PRIOR TO REUSING.
- P CONTRACTOR TO PROTECT.

**NOTES:**

1. SEE SHEETS C-891 THROUGH C-892 FOR LIGHTING DETAILS DURING THRESHOLD RELOCATION.
2. DIRECT BURIED CONDUITS AND CABLES WITHIN THE LIMITS OF EXCAVATION SHALL BE REMOVED AND THE REMOVAL SHALL BE CONSIDERED INCIDENTAL TO ITEM P-152 UNCLASSIFIED EXCAVATION. DIRECT BURIED CONDUITS AND CABLES NOT WITHIN THE LIMITS OF EXCAVATION SHALL BE ABANDONED IN PLACE UNLESS OTHERWISE NOTED.
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**AUGUSTA REGIONAL AIRPORT  
CONSTRUCT TAXIWAY G**

1501 AVIATION WAY  
AUGUSTA, GA 30906-9620

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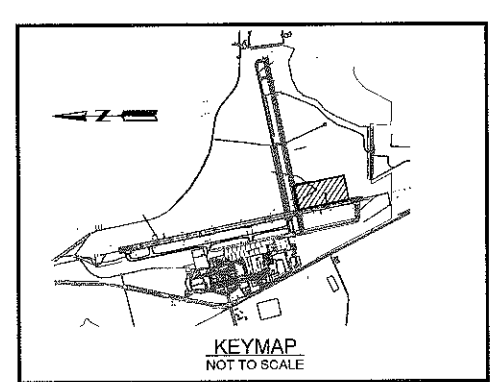
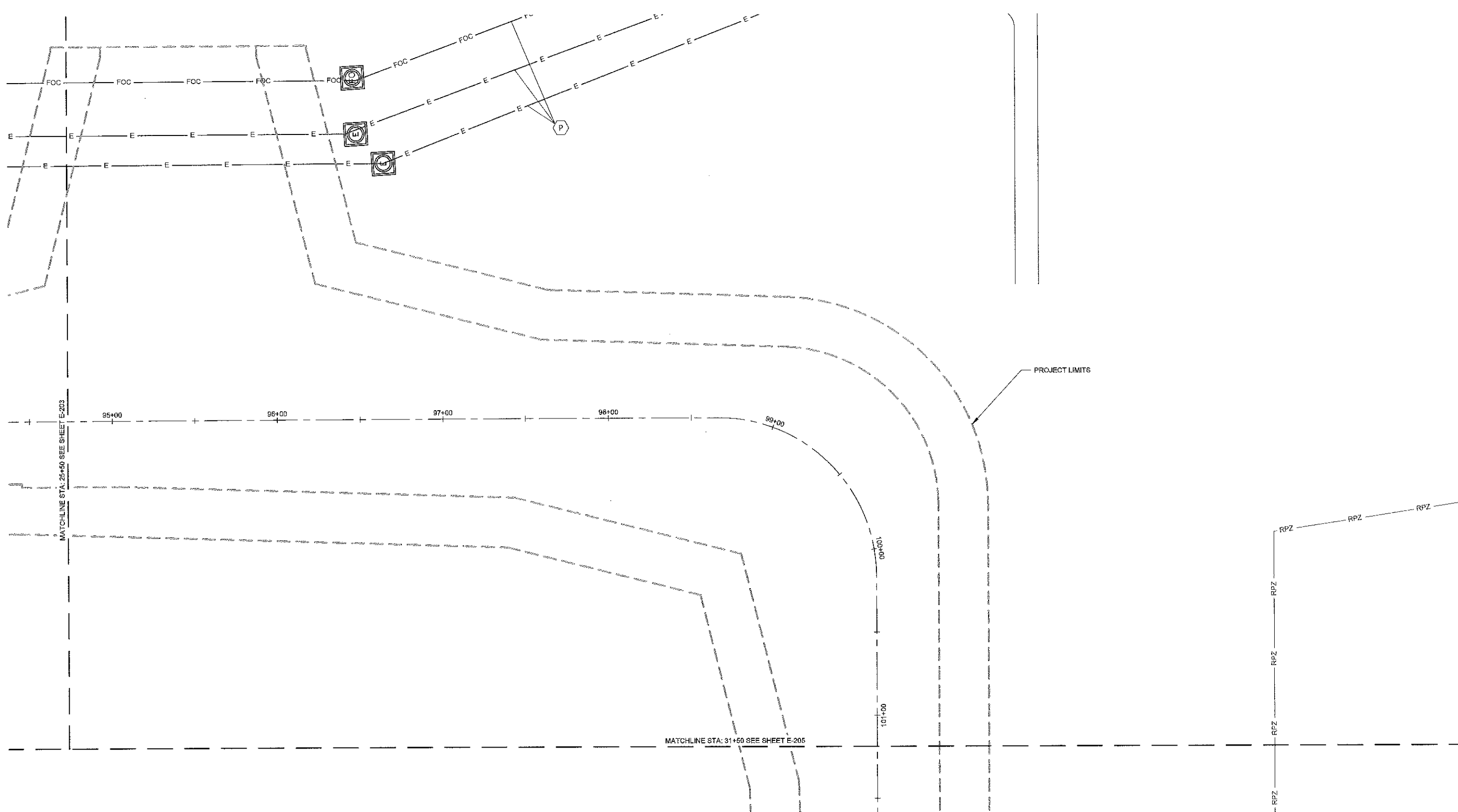
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SHEET CONTENTS  
ELECTRICAL DEMO  
PLAN

SHEET NO.

E-103



**ELECTRICAL LEGEND**

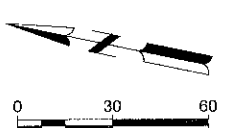
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- GUIDANCE SIGN REMOVAL
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- REMOVE (E) CONDUIT UNLESS OTHERWISE NOTED
- (E) PVC CONDUIT UNLESS OTHERWISE NOTED
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**ELECTRICAL KEY NOTES**

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- PROTECT (E) UTILITIES AT ALL TIMES DURING THE PROJECT.
- CLEAN (E) CONDUIT PRIOR TO REUSING.
- CONTRACTOR TO PROTECT.

**NOTES:**

1. SEE SHEETS C-681 THROUGH C-692 FOR LIGHTING DETAILS DURING THRESHOLD RELOCATION.
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**AUGUSTA REGIONAL AIRPORT  
 CONSTRUCT TAXIWAY G**

1501 AVIATION WAY  
 AUGUSTA, GA 30906-9620

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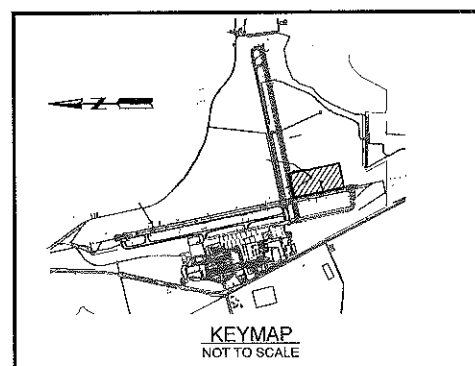
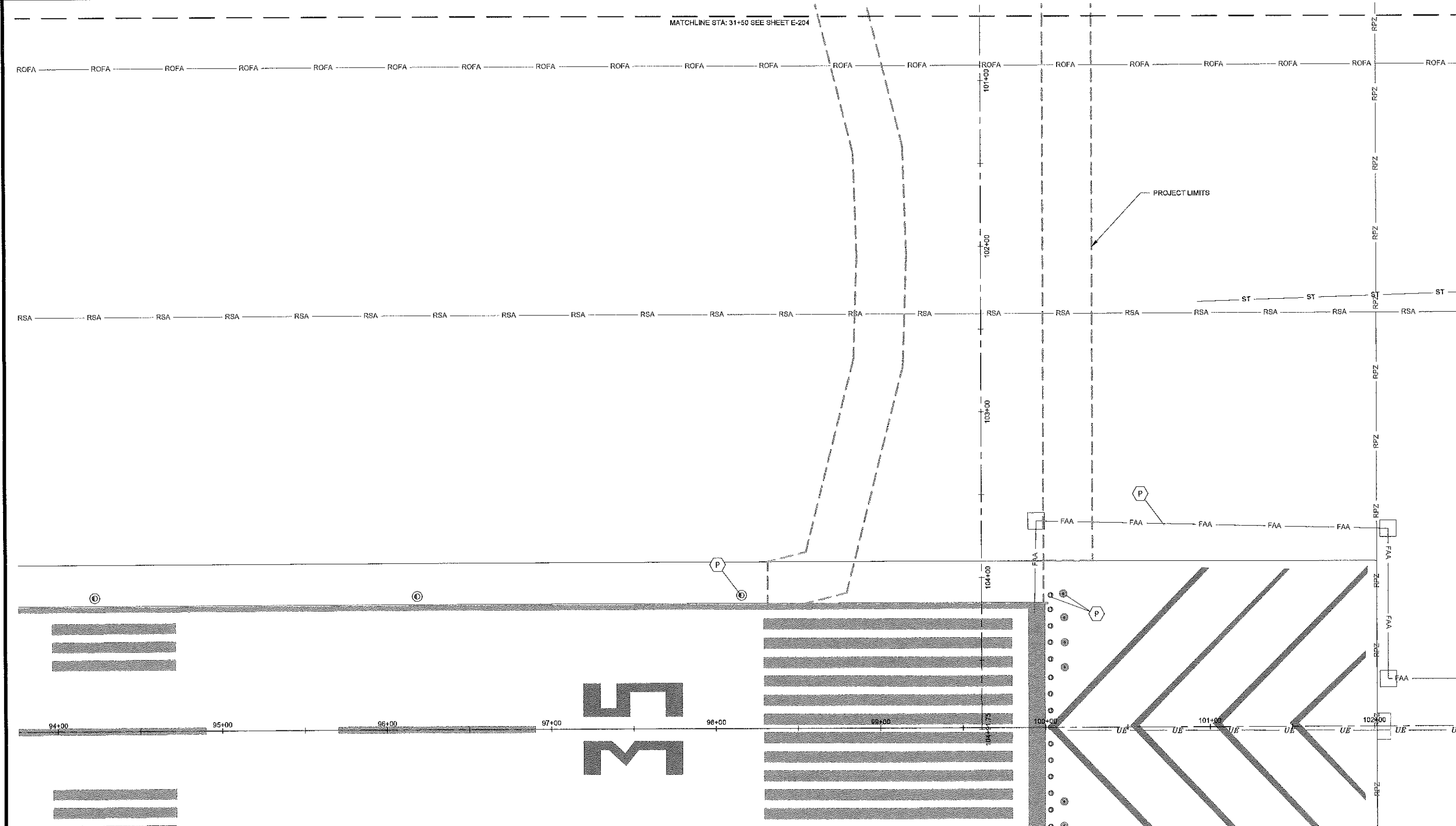
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SHEET CONTENTS  
**ELECTRICAL DEMO PLAN**

SHEET NO

**E-104**



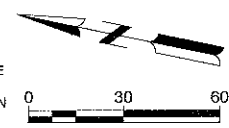
**ELECTRICAL LEGEND**

- EDGE LIGHT REMOVAL
- GUIDANCE SIGN REMOVAL
- REMOVE (E) CONDUCTORS UNLESS OTHERWISE NOTED
- REMOVE (E) CONDUIT UNLESS OTHERWISE NOTED
- (E) PVC CONDUIT UNLESS OTHERWISE NOTED
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- CLEAN (E) CONDUIT PRIOR TO REUSING.
- CONTRACTOR TO PROTECT.

- NOTES:**
- SEE SHEETS C-681 THROUGH C-692 FOR LIGHTING DETAILS DURING THRESHOLD RELOCATION.
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**AUGUSTA REGIONAL AIRPORT  
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 AUGUSTA, GA 30906-9620

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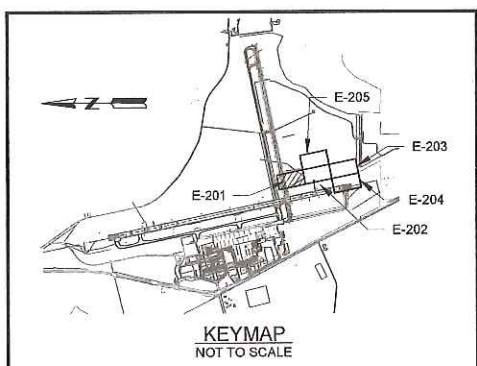
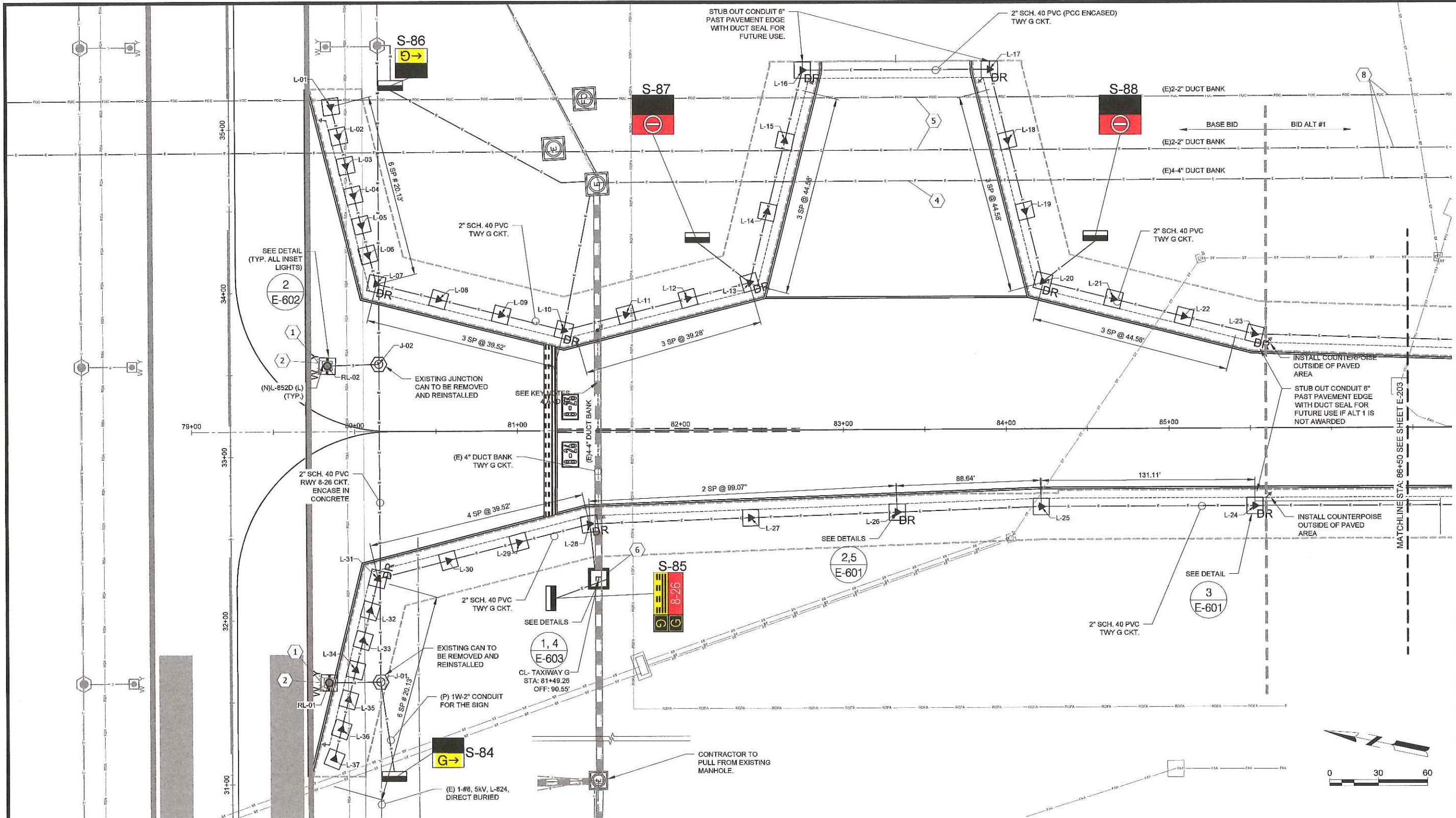
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SHEET CONTENTS  
**ELECTRICAL LAYOUT**

SHEET NO.

**E-201**



**ELECTRICAL LEGEND**

	(E) WIRE/CONDUIT TO REMAIN		(N) TAXIWAY GUIDANCE SIGN PCC BASE
	(N) #8 5KV, L-824C, IN (N) 2" SCH. 40 PVC		L-867D J-CAN W/ 3/4" SOLID COVER (PULL THRU ONLY)
	(N) #6 SOLID BARE COPPER COUNTERPOISE		(N) 4"x4"x4" AIRCRAFT RATED HANDHOLE WITH SPRING-ASSISTED LID
	(N) L-861T LED MITL, ON L-867 BASE		(N) 3/4"x10" COPPER CLAD GROUND ROD
	(N) L-861T LED MITL, ON L-867 BASE WITH DRAINAGE		FUTURE ELEVATED LIGHT NUMBER, SEE SCHEDULE E-211
	(N) L852D(L) INSET, LED MIRL ON L868 BASE		

- ELECTRICAL KEY NOTES**
- 1 REMOVE (E) RUNWAY EDGE LIGHT FIXTURE BASE CAN, AND 2" PVC. SALVAGE FIXTURE TO THE AIRPORT
  - 2 INSTALL (N) L852D(L), (COLOR AS SHOWN) ON (N) L868 BASE CAN. SEE DETAIL ON SHEET E-601
  - 3 RUNWAY EDGE LIGHTS IMPACTED BY THE SHOULDER RECONSTRUCTION ON RUNWAY 8-26 AND NOT SCHEDULED FOR REPLACEMENT, SHALL BE REMOVED, STORED, AND REINSTALLED ON (N) L867 LIGHT BASE. SEE DETAIL ON SHEET E-601
  - 4 LOCATE (E) 4-4" DUCT BANK. EXPOSE CAREFULLY AND CONCRETE ENCASE UNDER PROPOSED PAVEMENT. A MINIMUM OF 3" COVER AROUND DUCT BANK IS REQUIRED. CONTRACTOR TO PROTECT EXISTING COUNTERPOISE.
  - 5 LOCATE (E) 2-2" DUCT BANK. EXPOSE CAREFULLY AND CONCRETE ENCASE UNDER PROPOSED PAVEMENT. A MINIMUM OF 3" COVER AROUND DUCT BANK IS REQUIRED. CONTRACTOR TO PROTECT EXISTING COUNTERPOISE.
  - 6 INSTALL (N) AIRCRAFT-RATED HANDHOLE 4"x4"x4" (INSIDE DIMENSIONS) WITH SPRING ASSISTED COVER, ON TO (E) 4-4" DUCT BANK. INSTALLATION SHALL INCLUDE: LOCATING EXISTING DUCT; PULLING BACK (E) 14-#5KV HOMERUN CABLES; RE-INSTALLING AND RECONNECTING (E) HOMERUNS USING L823 SUPERKIT OR COMPLETE KIT CONNECTORS.
  - 7 INSTALL TEMPORARY #8, 5KV JUMPER CABLES, AS NECESSARY TO KEEP RWY AND/OR TWY CIRCUIT ENERGIZED.
  - 8 PROTECT (E) UTILITIES AT ALL TIMES DURING THE PROJECT.
  - 9 CLEAN (E) CONDUIT PRIOR TO REUSING.
  - 10 IF CONFLICT BETWEEN THE ELECTRICAL DUCT RUN AND PAVEMENT EDGE DRAIN OR STORM LINE ARISES, CONTRACTOR SHALL ROUTE DUCT 6" (MIN) BELOW EDGE DRAIN OR STORM LINE INVERT.
  - 11 CORE (E) L867 LIGHT BASE AND INSTALL 2" PVC MALE ADAPTER. CONNECT (N) 2" PVC TO (E) LIGHT BASE
  - 12 INSTALL COUNTERPOISE USING THE EQUIPOTENTIAL METHOD WHERE THE COUNTERPOISE IS CENTERED OVER THE DUCT AND BONDED TO THE LIGHT BASE/GROUND ROD. ALL CONDUIT/DUCT RUNS REQUIRES COUNTERPOISE INSTALLATION.
  - 13 INSTALL 3/4" BLANK COVER. DELIVER LIGHT FIXTURE AND ISO TRANSFORMER TO THE AIRPORT FOR LATER USE.



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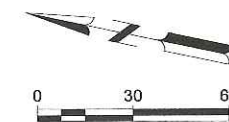
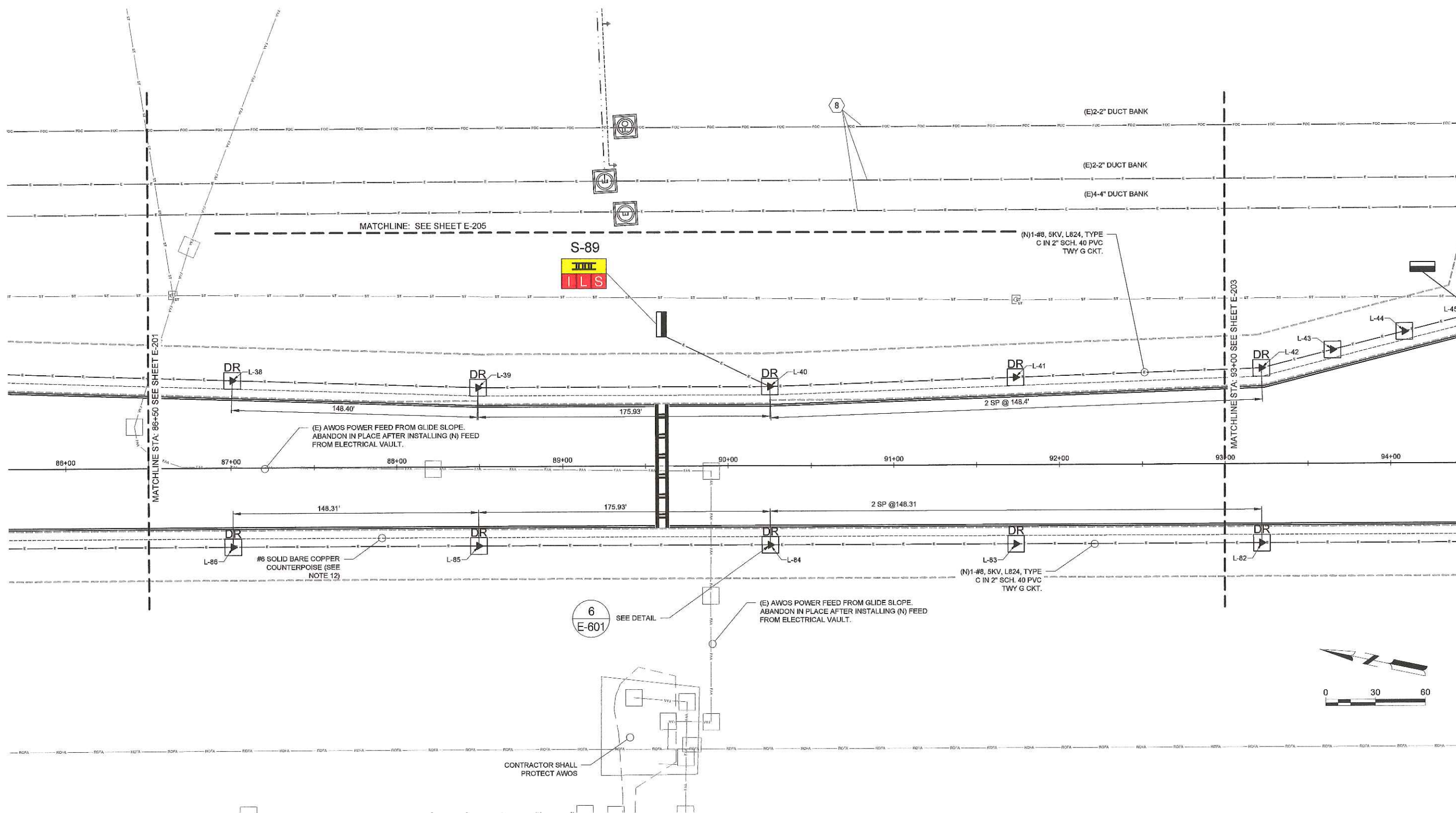
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SHEET CONTENTS  
 ELECTRICAL LAYOUT

SHEET NO.

**E-202**



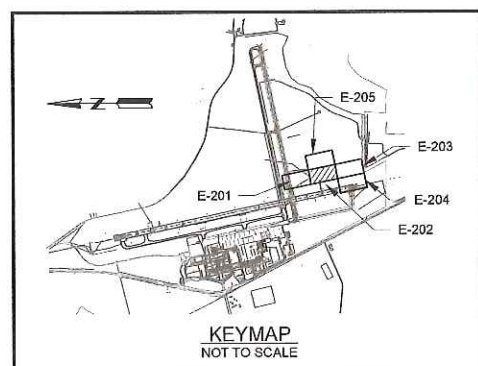
CONTRACTOR SHALL PROTECT AWOS

**ELECTRICAL LEGEND**

- |  |  |  |   |
|--|--|--|---|
|  | (E) WIRE AND CONDUIT TO REMAIN                   |  | (N) L858 (L) TAXIWAY GUIDANCE SIGN ON PCC BASE.               |
|  | (N) #8 5KV, L-824C, IN (N) 2" SCH. 40 PVC        |  | L868D J-CAN W/ 1-1/4" SOLID COVER (PULL THRU ONLY)            |
|  | (N) #6 SOLID BARE COPPER COUNTERPOISE            |  | (N) 4"X4"X4" AIRCRAFT RATED HANDHOLE WITH SPRING-ASSISTED LID |
|  | (N) L-861T LED MITL, ON L-867 BASE               |  | (N) 3/4"X10" COPPER CLAD GROUND ROD                           |
|  | (N) L-861T LED MITL, ON L-867 BASE WITH DRAINAGE |  | S - 11 ← SIGN NUMBER, SEE SCHEDULE E-212                      |
|  | (N) L852D(L) INSET, LED MRL ON L868 BASE         |  | A - 101 ← ELEVATED LIGHT NUMBER, SEE SCHEDULE E-211           |

**ELECTRICAL KEY NOTES**

- 1 REMOVE (E) RUNWAY EDGE LIGHT FIXTURE BASE CAN, AND 2" PVC. SALVAGE FIXTURE TO THE AIRPORT
- 2 INSTALL (N) L852D(L), (COLOR AS SHOWN) ON (N) L868 BASE CAN. SEE DETAIL ON SHEET E-601
- 3 RUNWAY EDGE LIGHTS IMPACTED BY THE SHOULDER RECONSTRUCTION ON RUNWAY 8-26 AND NOT SCHEDULED FOR REPLACEMENT, SHALL BE REMOVED, STORED, AND REINSTALLED ON (N) L867 LIGHT BASE. SEE DETAIL ON SHEET E-601
- 4 LOCATE (E) 4-4" DUCT BANK. EXPOSE CAREFULLY AND CONCRETE ENCASE UNDER PROPOSED PAVEMENT. A MINIMUM OF 3" COVER AROUND DUCT BANK IS REQUIRED. CONTRACTOR TO PROTECT EXISTING COUNTERPOISE.
- 5 LOCATE (E) 2-2" DUCT BANK. EXPOSE CAREFULLY AND CONCRETE ENCASE UNDER PROPOSED PAVEMENT. A MINIMUM OF 3" COVER AROUND DUCT BANK IS REQUIRED. CONTRACTOR TO PROTECT EXISTING COUNTERPOISE.
- 6 INSTALL (N) AIRCRAFT-RATED HANDHOLE 4"X4"X4" (INSIDE DIMENSIONS) WITH SPRING ASSISTED COVER, ON TO (E) 4-4" DUCT BANK. INSTALLATION SHALL INCLUDE: LOCATING EXISTING DUCT; PULLING BACK (E) 14-#5KV HOMERUN CABLES; RE-INSTALLING AND RECONNECTING (E) HOMERUNS USING L823 SUPERKIT OR COMPLETE KIT CONNECTORS.
- 7 INSTALL TEMPORARY #8, 5KV JUMPER CABLES, AS NECESSARY TO KEEP RWY AND/OR TWY CIRCUIT ENERGIZED.
- 8 PROTECT (E) UTILITIES AT ALL TIMES DURING THE PROJECT.
- 9 CLEAN (E) CONDUIT PRIOR TO REUSING.
- 10 IF CONFLICT BETWEEN THE ELECTRICAL DUCT RUN AND PAVEMENT EDGE DRAIN OR STORM LINE ARISES, CONTRACTOR SHALL ROUTE DUCT 6" (MIN) BELOW EDGE DRAIN OR STORM LINE INVERT.
- 11 CORE (E) L867 LIGHT BASE AND INSTALL 2" PVC MALE ADAPTER. CONNECT (N) 2" PVC TO (E) LIGHT BASE
- 12 INSTALL COUNTERPOISE USING THE EQUIPOTENTIAL METHOD WHERE THE COUNTERPOISE IS CENTERED OVER THE DUCT AND BONDED TO THE LIGHT BASE/GROUND ROD. ALL CONDUIT/DUCT RUNS REQUIRES COUNTERPOISE INSTALLATION.
- 13 INSTALL 3/4" BLANK COVER. DELIVER LIGHT FIXTURE AND ISO TRANSFORMER TO THE AIRPORT FOR LATER USE.





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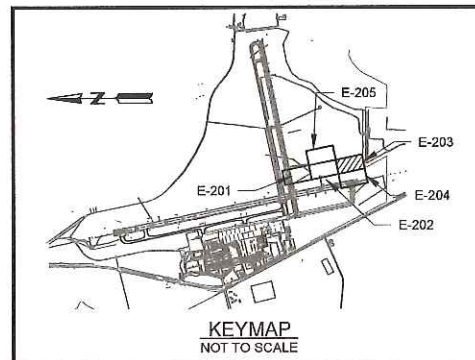
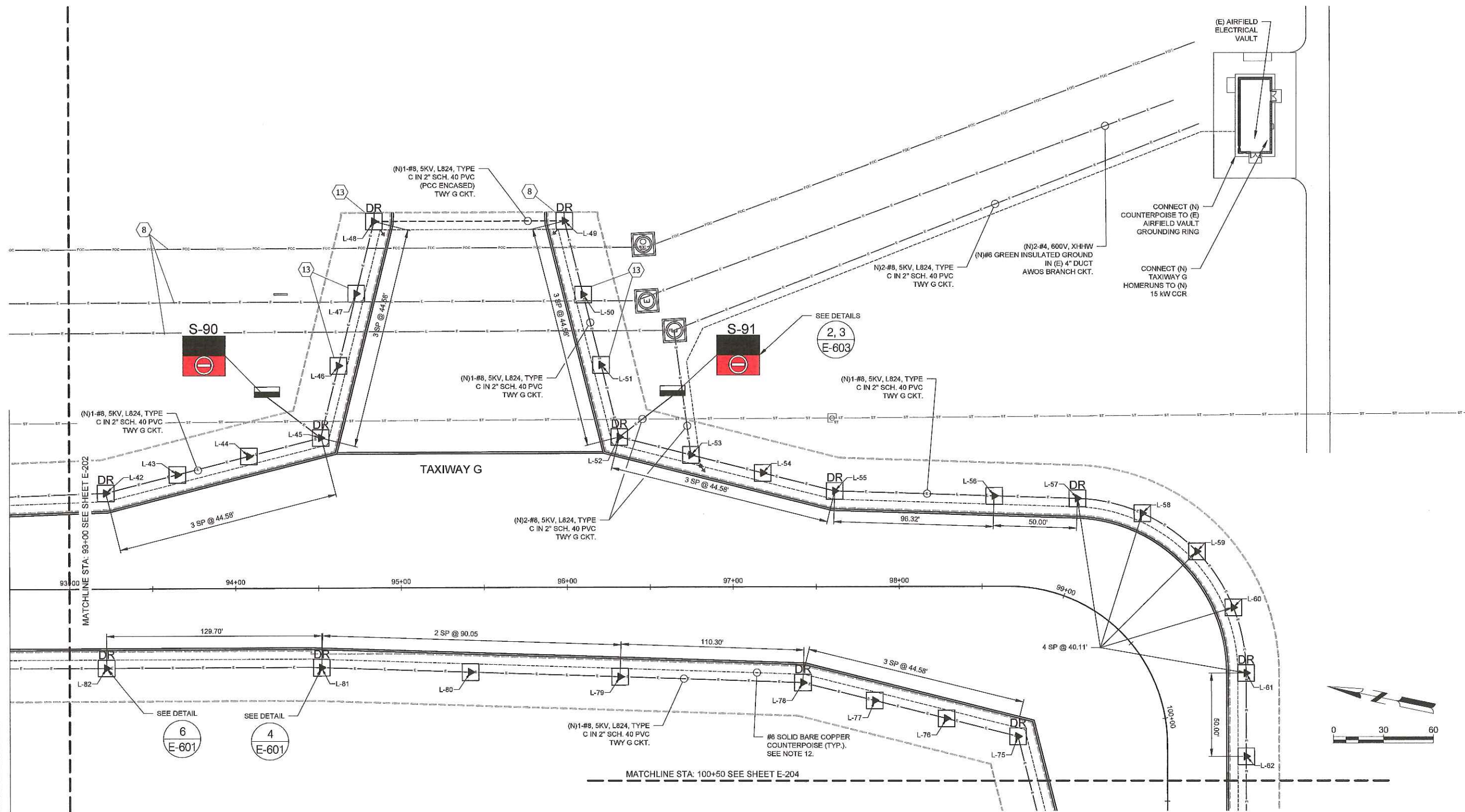
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SHEET CONTENTS  
ELECTRICAL LAYOUT

SHEET NO.

**E-203**



**ELECTRICAL LEGEND**

	(E) WIRE AND CONDUIT TO REMAIN		(N) L868D (L) TAXIWAY GUIDANCE SIGN ON PCC BASE.
	(N) #8 5KV, L-824C, IN (N) 2" SCH. 40 PVC		L868D J-CAN W/ 1-1/4" SOLID COVER (PULL THRU ONLY)
	(N) #6 SOLID BARE COPPER COUNTERPOISE		(N) 4'X4' AIRCRAFT RATED HANDHOLE WITH SPRING-ASSISTED LID
	(N) L-861T LED MITL, ON L-867 BASE		(N) 3/2"X10" COPPER CLAD GROUND ROD
	(N) L-861T LED MITL, ON L-867 BASE WITH DRAINAGE		S - 11 ← SIGN NUMBER, SEE SCHEDULE E-212
	(N) L852D(L) INSET, LED MRL ON L868 BASE		A - 101 ← ELEVATED LIGHT NUMBER, SEE SCHEDULE E-211

- ELECTRICAL KEY NOTES**
- 1 REMOVE (E) RUNWAY EDGE LIGHT FIXTURE BASE CAN, AND 2" PVC. SALVAGE FIXTURE TO THE AIRPORT
  - 2 INSTALL (N) L852D(L), (COLOR AS SHOWN) ON (N) L868 BASE CAN. SEE DETAIL ON SHEET E-601
  - 3 RUNWAY EDGE LIGHTS IMPACTED BY THE SHOULDER RECONSTRUCTION ON RUNWAY 8-26 AND NOT SCHEDULED FOR REPLACEMENT, SHALL BE REMOVED, STORED, AND REINSTALLED ON (N) L867 LIGHT BASE. SEE DETAIL ON SHEET E-601
  - 4 LOCATE (E) 4-4" DUCT BANK. EXPOSE CAREFULLY AND CONCRETE ENCASE UNDER PROPOSED PAVEMENT. A MINIMUM OF 3" COVER AROUND DUCT BANK IS REQUIRED. CONTRACTOR TO PROTECT EXISTING COUNTERPOISE.
  - 5 LOCATE (E) 2-2" DUCT BANK. EXPOSE CAREFULLY AND CONCRETE ENCASE UNDER PROPOSED PAVEMENT. A MINIMUM OF 3" COVER AROUND DUCT BANK IS REQUIRED. CONTRACTOR TO PROTECT EXISTING COUNTERPOISE.
  - 6 INSTALL (N) AIRCRAFT-RATED HANDHOLE 4'X4' (INSIDE DIMENSIONS) WITH SPRING ASSISTED COVER, ON TO (E) 4-4" DUCT BANK. INSTALLATION SHALL INCLUDE: LOCATING EXISTING DUCT; PULLING BACK (E)14-#5KV HOMERUN CABLES; RE-INSTALLING AND RECONNECTING (E) HOMERUNS USING L823 SUPERKIT OR COMPLETE KIT CONNECTORS.
  - 7 INSTALL TEMPORARY #8, 5KV JUMPER CABLES, AS NECESSARY TO KEEP RWY AND/OR TWY CIRCUIT ENERGIZED.
  - 8 PROTECT (E) UTILITIES AT ALL TIMES DURING THE PROJECT.
  - 9 CLEAN (E) CONDUIT PRIOR TO REUSING.
  - 10 IF CONFLICT BETWEEN THE ELECTRICAL DUCT RUN AND PAVEMENT EDGE DRAIN OR STORM LINE ARISES, CONTRACTOR SHALL ROUTE DUCT 6" (MIN) BELOW EDGE DRAIN OR STORM LINE INVERT.
  - 11 CORE (E) L867 LIGHT BASE AND INSTALL 2" PVC MALE ADAPTER. CONNECT (N) 2" PVC TO (E) LIGHT BASE
  - 12 INSTALL COUNTERPOISE USING THE EQUIPOTENTIAL METHOD WHERE THE COUNTERPOISE IS CENTERED OVER THE DUCT AND BONDED TO THE LIGHT BASE/GROUND ROD. ALL CONDUIT/DUCT RUNS REQUIRES COUNTERPOISE INSTALLATION.
  - 13 INSTALL 3/8" BLANK COVER. DELIVER LIGHT FIXTURE AND ISO TRANSFORMER TO THE AIRPORT FOR LATER USE.

- ELECTRICAL KEY NOTES**
- 1 REMOVE (E) RUNWAY EDGE LIGHT FIXTURE BASE CAN, AND 2" PVC. SALVAGE FIXTURE TO THE AIRPORT
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  - 13 INSTALL 3/8" BLANK COVER. DELIVER LIGHT FIXTURE AND ISO TRANSFORMER TO THE AIRPORT FOR LATER USE.



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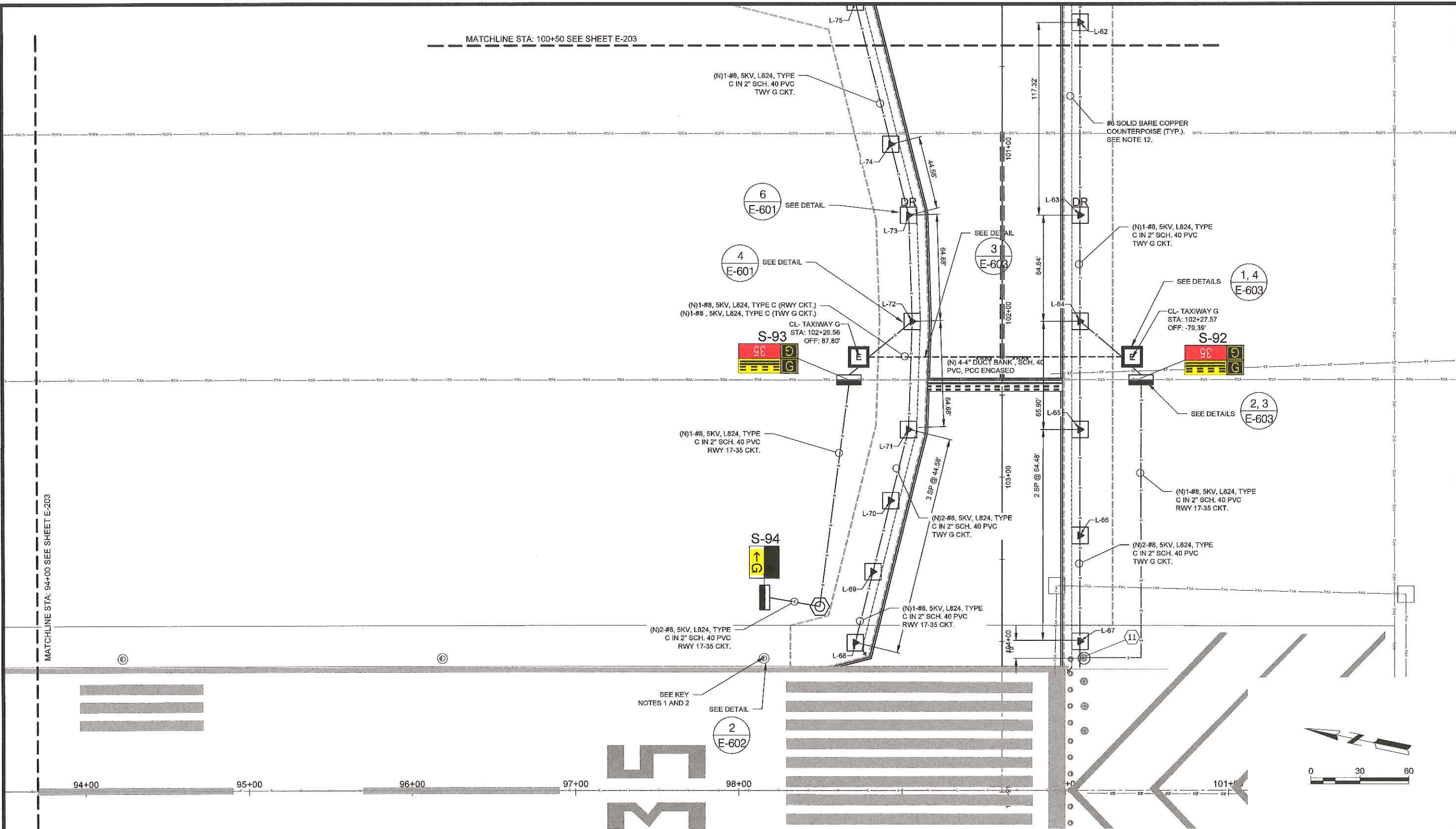
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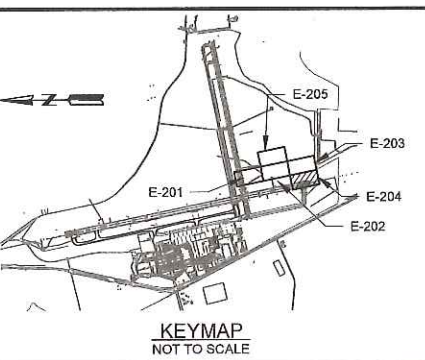
SHEET CONTENTS  
**ELECTRICAL LAYOUT**

SHEET NO.



MATCHLINE STA: 94+00 SEE SHEET E-203

MATCHLINE STA: 100+50 SEE SHEET E-203



- ELECTRICAL LEGEND**
- (E) WIRE AND CONDUIT TO REMAIN
  - (N) #8 5KV, L-824C, IN (N) 2" SCH. 40 PVC
  - (N) #6 SOLID BARE COPPER COUNTERPOISE
  - (N) L-861T LED MITL, ON L-867 BASE
  - (N) L-861T LED MITL, ON L-867 BASE WITH DRAINAGE
  - (N) L-852D(L) INSET, LED MIRL ON L868 BASE
  - (N) L858 (L) TAXIWAY GUIDANCE SIGN ON PCC BASE.
  - L868D J-CAN W/ 1-1/4" SOLID COVER (PULL THRU ONLY)
  - (N) 4"X4" AIRCRAFT RATED HANDHOLE WITH SPRING-ASSISTED LID
  - (N) 3/4"X10' COPPER CLAD GROUND ROD
  - S - 11 → SIGN NUMBER, SEE SCHEDULE E-212
  - A - 101 → ELEVATED LIGHT NUMBER, SEE SCHEDULE E-211

- ELECTRICAL KEY NOTES**
- 1 REMOVE (E) RUNWAY EDGE LIGHT FIXTURE BASE CAN, AND 2" PVC. SALVAGE FIXTURE TO THE AIRPORT
  - 2 INSTALL (N) L852D(L), (COLOR AS SHOWN) ON (N) L868 BASE CAN. SEE DETAIL ON SHEET E-601
  - 3 RUNWAY EDGE LIGHTS IMPACTED BY THE SHOULDER RECONSTRUCTION ON RUNWAY 8-28 AND NOT SCHEDULED FOR REPLACEMENT, SHALL BE REMOVED, STORED, AND REINSTALLED ON (N) L867 LIGHT BASE. SEE DETAIL ON SHEET E-801
  - 4 LOCATE (E) 4-4" DUCT BANK. EXPOSE CAREFULLY AND CONCRETE ENCASE UNDER PROPOSED PAVEMENT. A MINIMUM OF 3" COVER AROUND DUCT BANK IS REQUIRED. CONTRACTOR TO PROTECT EXISTING COUNTERPOISE.
  - 5 LOCATE (E) 2-2" DUCT BANK. EXPOSE CAREFULLY AND CONCRETE ENCASE UNDER PROPOSED PAVEMENT. A MINIMUM OF 3" COVER AROUND DUCT BANK IS REQUIRED. CONTRACTOR TO PROTECT EXISTING COUNTERPOISE.
  - 6 INSTALL (N) AIRCRAFT-RATED HANDHOLE 4"X4" (INSIDE DIMENSIONS) WITH SPRING ASSISTED COVER, ON TO (E) 4-4" DUCT BANK. INSTALLATION SHALL INCLUDE: LOCATING EXISTING DUCT; PULLING BACK (E) 14-#5KV HOMERUN CABLES; RE-INSTALLING AND RECONNECTING (E) HOMERUNS USING L823 SUPERKIT OR COMPLETE KIT CONNECTORS.
  - 7 INSTALL TEMPORARY #8, 5KV JUMPER CABLES, AS NECESSARY TO KEEP RWY AND/OR TWY CIRCUIT ENERGIZED.
  - 8 PROTECT (E) UTILITIES AT ALL TIMES DURING THE PROJECT.
  - 9 CLEAN (E) CONDUIT PRIOR TO REUSING.
  - 10 IF CONFLICT BETWEEN THE ELECTRICAL DUCT RUN AND PAVEMENT EDGE DRAIN OR STORM LINE ARISES, CONTRACTOR SHALL ROUTE DUCT 6" (MIN) BELOW EDGE DRAIN OR STORM LINE INVERT.
  - 11 CORE (E) L867 LIGHT BASE AND INSTALL 2" PVC MALE ADAPTER. CONNECT (N) 2" PVC TO (E) LIGHT BASE
  - 12 INSTALL COUNTERPOISE USING THE EQUIPOTENTIAL METHOD WHERE THE COUNTERPOISE IS CENTERED OVER THE DUCT AND BONDED TO THE LIGHT BASE/GROUND ROD. ALL CONDUIT/DUCT RUNS REQUIRES COUNTERPOISE INSTALLATION.
  - 13 INSTALL 3/8" BLANK COVER. DELIVER LIGHT FIXTURE AND ISO TRANSFORMER TO THE AIRPORT FOR LATER USE.



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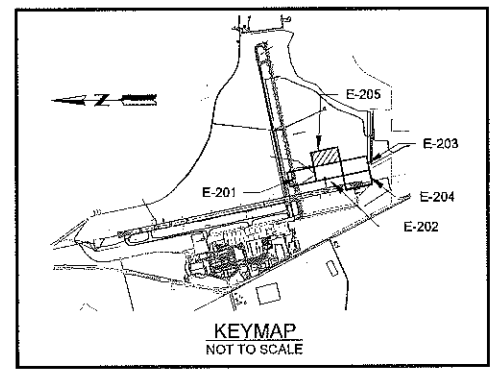
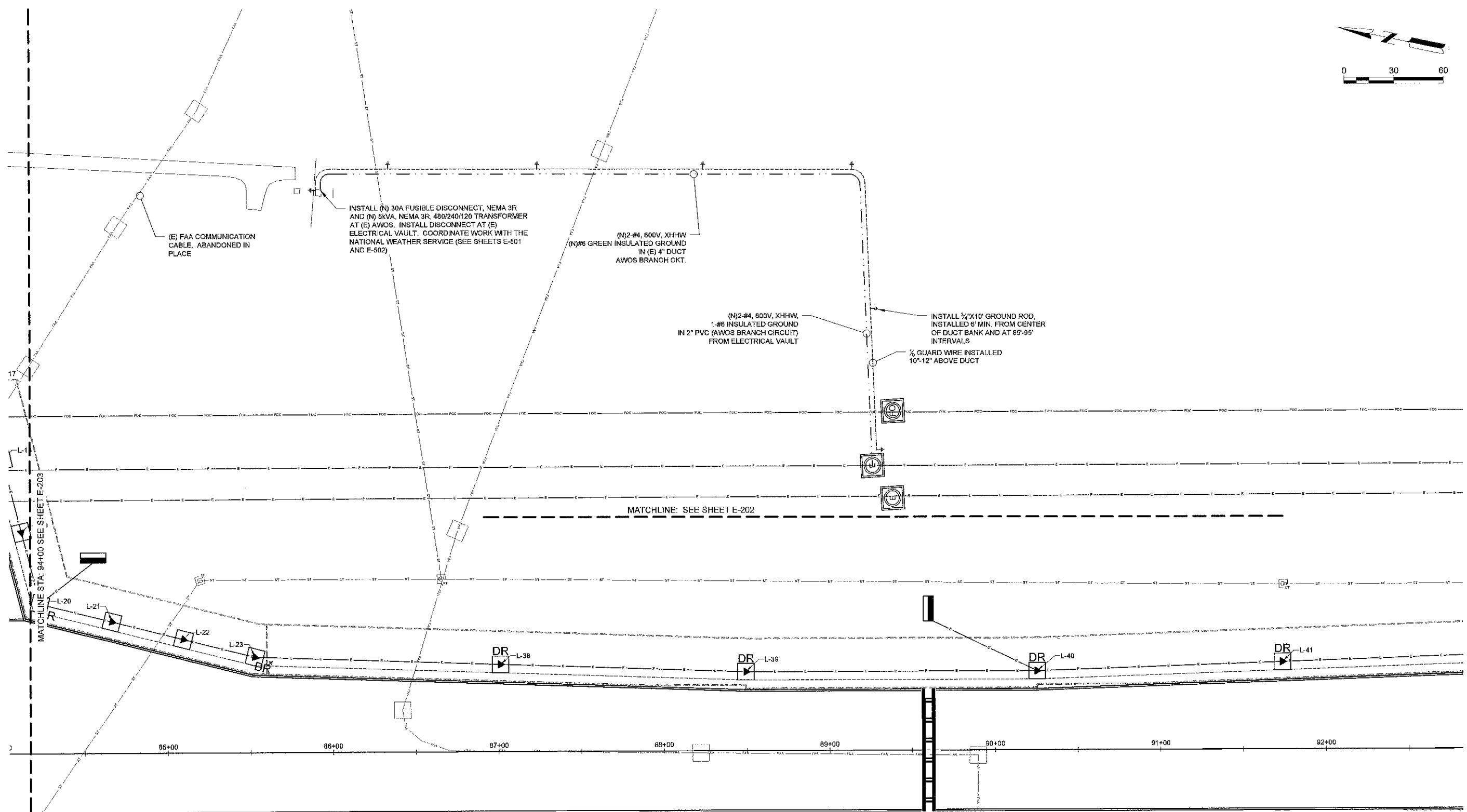
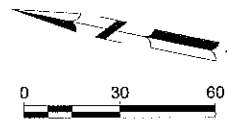
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 CHECKED BY: DES  
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SHEET CONTENTS  
**ELECTRICAL LAYOUT**

SHEET NO.

**E-205**



- ELECTRICAL LEGEND**
- (E) WIRE AND CONDUIT TO REMAIN
  - (N) #8 5KV, L-824C, IN (N) 2" SCH. 40 PVC
  - (N) #6 SOLID BARE COPPER COUNTERPOISE
  - (N) L861(L) ELEVATED, LED MITL ON L867 BASE
  - (N) L881(L) ELEVATED, LED MITL ON L867 BASE WITH DRAINAGE
  - (N) L850C, INSET HIRL ON L868 BASE
  - (N) L852D(L) INSET, LED MRL ON L868 BASE
  - (N) L858 (L) TAXIWAY GUIDANCE SIGN ON PCC BASE.
  - L868D J-CAN W/ 1-1/4" SOLID COVER (PULL THRU ONLY)
  - (N) 4"X4" AIRCRAFT RATED HANDHOLE WITH SPRING-ASSISTED LID
  - (N) 3/4"X1/8" COPPER CLAD GROUND ROD
  - S - 11 SIGN NUMBER, SEE SCHEDULE E-212
  - A - 101 ELEVATED LIGHT NUMBER, SEE SCHEDULE E-211

- ELECTRICAL KEY NOTES**
- 1 REMOVE (E) RUNWAY EDGE LIGHT FIXTURE BASE CAN, AND 2" PVC. SALVAGE FIXTURE TO THE AIRPORT
  - 2 INSTALL (N) L852D(L), (COLOR AS SHOWN) ON (N) L868 BASE CAN. SEE DETAIL ON SHEET E-601
  - 3 RUNWAY EDGE LIGHTS IMPACTED BY THE SHOULDER RECONSTRUCTION ON RUNWAY 8-28 AND NOT SCHEDULED FOR REPLACEMENT, SHALL BE REMOVED, STORED, AND REINSTALLED ON (N) L867 LIGHT BASE. SEE DETAIL ON SHEET E-601
  - 4 LOCATE (E) 4-4" DUCT BANK. EXPOSE CAREFULLY AND CONCRETE ENCASE UNDER PROPOSED PAVEMENT. A MINIMUM OF 3" COVER AROUND DUCT BANK IS REQUIRED. CONTRACTOR TO PROTECT EXISTING COUNTERPOISE.
  - 5 LOCATE (E) 2-2" DUCT BANK. EXPOSE CAREFULLY AND CONCRETE ENCASE UNDER PROPOSED PAVEMENT. A MINIMUM OF 3" COVER AROUND DUCT BANK IS REQUIRED. CONTRACTOR TO PROTECT EXISTING COUNTERPOISE.
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  - 11 CORE (E) L867 LIGHT BASE AND INSTALL 2" PVC MALE ADAPTER. CONNECT (N) 2" PVC TO (E) LIGHT BASE
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  - 13 INSTALL 3/4" BLANK COVER. DELIVER LIGHT FIXTURE AND ISO TRANSFORMER TO THE AIRPORT FOR LATER USE.



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SHEET CONTENTS  
 TAXIWAY LIGHT  
 FIXTURE SCHEDULE

SHEET NO.

TAXIWAY LIGHTING SCHEDULE (BASE BID)			
	LIGHT #	NORTHING	EASTING
1	L-01	1224692.502	717928.415
2	L-02	1224684.308	717911.660
3	L-03	1224676.113	717894.906
4	L-04	1224667.919	717878.152
5	L-05	1224659.724	717861.397
6	L-06	1224651.530	717844.643
7	L-07	1224643.335	717827.889
8	L-08	1224635.140	717811.135
9	L-09	1224626.945	717794.381
10	L-10	1224618.750	717777.627
11	L-11	1224610.555	717760.873
12	L-12	1224602.360	717744.119
13	L-13	1224594.165	717727.365
14	L-14	1224585.970	717710.611
15	L-15	1224577.775	717693.857

TAXIWAY LIGHTING SCHEDULE (BASE BID)			
	LIGHT #	NORTHING	EASTING
16	L-16	1224413.795	718008.728
17	L-17	1224301.467	718032.390
18	L-18	1224281.922	717992.325
19	L-19	1224262.376	717952.260
20	L-20	1224242.830	717912.194
21	L-21	1224198.284	717910.472
22	L-22	1224153.738	717908.751
23	L-23	1224109.192	717907.029
24	L-24	1224087.563	717804.359
25	L-25	1224215.861	717777.332
26	L-26	1224301.917	717756.083
27	L-27	1224389.342	717734.496
28	L-28	1224485.526	717710.747
29	L-29	1224525.702	717691.097
30	L-30	1224565.878	717671.447

TAXIWAY LIGHTING SCHEDULE (BASE BID)			
	LIGHT #	NORTHING	EASTING
31	L-31	1224606.054	717651.797
32	L-32	1224606.811	717631.677
33	L-33	1224607.513	717613.040
34	L-34	1224608.214	717594.402
35	L-35	1224608.916	717575.764
36	L-36	1224609.617	717557.126
37	L-37	1224610.319	717538.489
38	J-01	1224591.783	717590.308
39	J-02	1224631.994	717780.203
40	RL-01	1224622.827	717583.454
41	RL-02	1224662.735	717772.794

TAXIWAY LIGHTING SCHEDULE (ALT 1)			
	LIGHT #	NORTHING	EASTING
109	L-46	1223234.605	718168.768
101	L-38	1223963.041	717932.746
102	L-39	1223816.891	717958.463
103	L-40	1223644.735	717994.729
104	L-41	1223500.630	718030.157
105	L-42	1223356.525	718065.585
106	L-43	1223316.459	718085.131
107	L-44	1223276.393	718104.677
108	L-45	1223236.327	718124.223
110	L-47	1223232.884	718213.314
111	L-48	1223231.162	718257.860
112	L-49	1223118.835	718281.522
113	L-50	1223098.854	718240.564
114	L-51	1223079.743	718201.391
115	L-52	1223060.198	718161.326
116	L-53	1223015.651	718159.604
117	L-54	1222971.105	718157.882

TAXIWAY LIGHTING SCHEDULE (ALT 1)			
	LIGHT #	NORTHING	EASTING
118	L-55	1222926.559	718156.161
119	L-56	1222831.695	718172.853
120	L-57	1222782.451	718181.518
121	L-58	1222742.349	718180.797
122	L-59	1222705.444	718165.091
123	L-60	1222677.119	718136.694
124	L-61	1222661.509	718099.747
125	L-62	1222651.199	718050.822
126	L-63	1222627.008	717936.029
127	L-64	1222613.678	717872.774
128	L-65	1222600.090	717808.295
129	L-66	1222586.794	717745.200
130	L-67	1222573.497	717682.105
131	L-68	1222707.962	717653.022
132	L-69	1222706.238	717697.568
133	L-70	1222704.515	717742.114
134	L-71	1222702.792	717786.660

TAXIWAY LIGHTING SCHEDULE (ALT 1)			
	LIGHT #	NORTHING	EASTING
135	L-72	1222714.215	717851.595
136	L-73	1222729.657	717914.405
137	L-74	1222749.203	717954.471
138	L-75	1222788.295	718034.602
139	L-76	1222832.841	718036.324
158	L-77	1222877.387	718038.046
159	L-78	1222921.933	718039.767
160	L-79	1223030.564	718020.652
161	L-80	1223119.249	718005.047
162	L-81	1223207.934	717989.442
145	L-82	1223334.896	717962.915
146	L-83	1223480.024	717932.342
147	L-84	1223625.152	717901.770
148	L-85	1223797.308	717865.504
149	L-86	1223942.436	717834.931

TAXIWAY GUIDANCE SIGN SCHEDULE							
SIGN NUMBER	LEGEND	SIZE	STYLE	MODE	MODULE	NORTHING	EASTING
S-84		2	3	2	2	1224580.059	717536.851
S-85		2	3	2	4	1224504.107	717668.263
S-86		2	3	2	2	1224665.907	717943.954
S-87		2	2	2	1	1224448.244	717864.184
S-88		2	2	2	1	1224223.737	717941.478
S-89		2	2	2	3	1223711.9262	718011.2334
S-90		2	2	2	1	1223265.6115	718143.3158
S-91		2	2	2	1	1223041.1043	718190.6102
S-92		2	3	2	4	1222577.5977	717847.0483
S-93		2	3	2	4	1222738.0328	717813.2512
S-94		2	3	2	2	1222763.3945	717661.7546

NOTES:

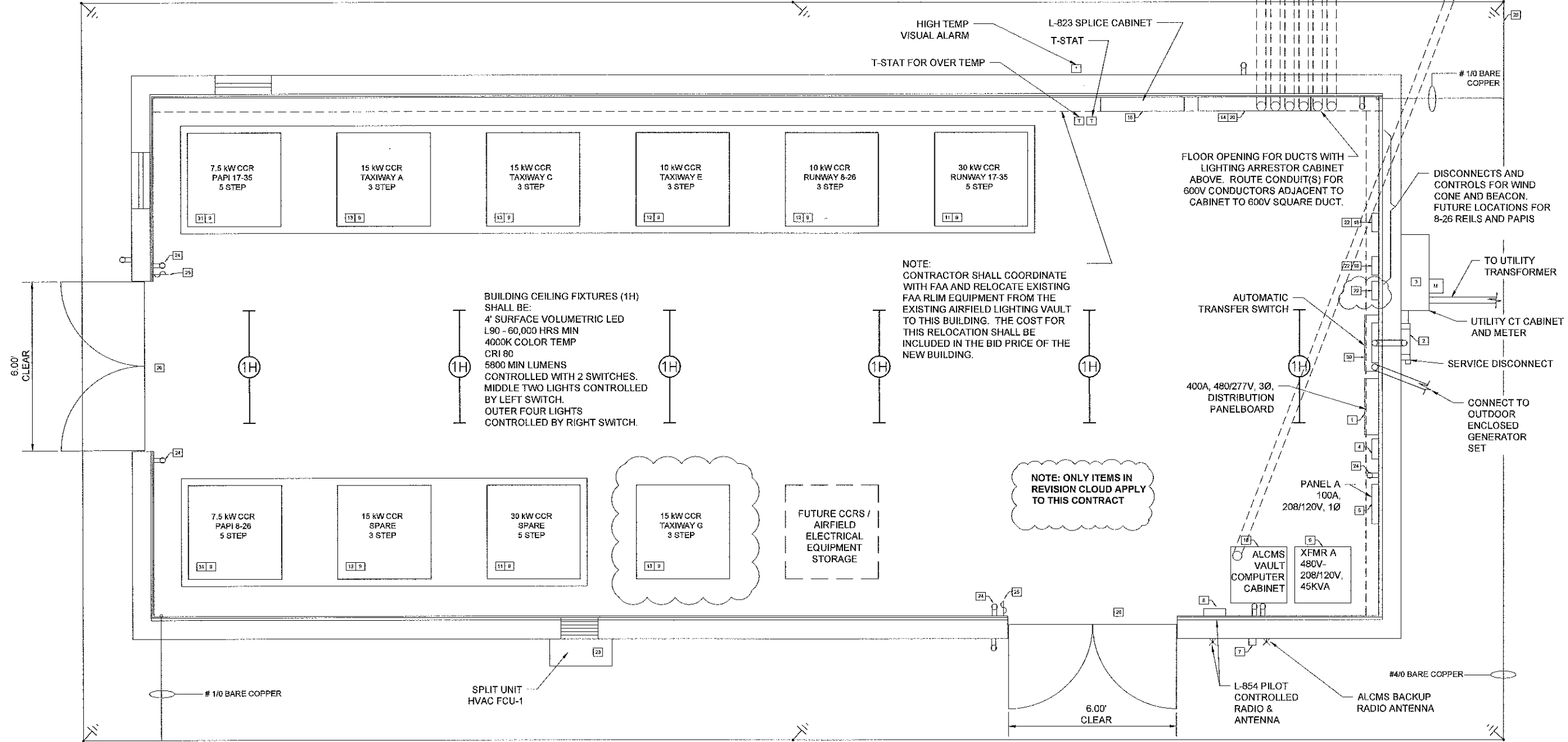
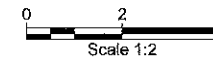
- ALL TAXIWAY EDGE LIGHTS, GUIDANCE SIGNS, AND ASSOCIATED CABLING TO BE INSTALLED AT A LATER DATE AND ARE NOT INCLUDED IN THIS PROJECT.
- TAXIWAY EDGE LIGHTING AND TAXIWAY GUIDANCE SIGN SCHEDULES PROVIDED AS REFERENCE FOR CONSTRUCTION AND INSTALLATION OF CANS AND SIGN PADS.

**EQUIPMENT GENERAL NOTES:**

- ALL WORK SHALL BE DONE IN A WORKMANLIKE MANNER.
- ALL WORK SHALL CONFORM TO THE 2014 EDITION OF THE NEC AND LOCAL CODES.
- ALL METAL CABINETS SHALL BE GROUNDED TO GROUND BUSS AS PER THE REQUIREMENTS OF THE N.E.C.
- AT A MINIMUM, THE LAST THREE FEET OF CONDUIT TO CCRs AND TRANSFORMERS SHALL BE FLEXIBLE METAL CONDUIT. INSTALL ALL CCRs ON SO THEY CAN BE MOVED OUT IF NECESSARY FOR MAINTENANCE OR REPAIR. LEAVE A MINIMUM OF 4' OF SLACK ON ALL FLEXIBLE CONNECTIONS TO ALLOW UNIT TO BE PULLED AWAY FROM THE WALL.

- ALL GROUND RODS ARE 3/4" X 10' COPPER CLAD.
- THIS DRAWINGS ARE DIAGRAMMATIC IN NATURE AND DEPICT THE GENERAL DESIGN CONCEPT. MINOR CHANGES TO THE LAYOUT OR DESIGN TO MAKE THE VARIOUS COMPONENTS FIT ARE ACCEPTABLE AND SHALL NOT BE CONSIDERED A JUSTIFIABLE CAUSE FOR ADDITIONAL COMPENSATION.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO APPRAISE THEMSELVES OF THE SITE CONDITIONS AND TO BRING TO THE ENGINEER'S ATTENTION ANY DISCREPANCIES BETWEEN THE SITE CONDITIONS AND THE CONTRACT DOCUMENTS PRIOR TO SUBMITTING A BID PRICE FOR THE WORK. NO ADDITIONAL COMPENSATION SHALL BE MADE FOR SMALL DIFFERENCES BETWEEN THE SITE CONDITIONS AND CONTRACT DOCUMENTS.

- FAN COIL (FCU-1): 2 TON, 800 CFM, WALL MTD, STAND ALONE TYPE, BUILT-IN THERMOSTATIC EXPANSION VALVE, FULLY CHARGED WITH R-410A, NOMINAL 8KW ELECTRIC HEATING COIL, 9 EER, 480V/3 PHASE, 20MCA (INCLUDES ELECTRIC HEATING COIL), DISCONNECT SWITCH AND MOTOR STARTER. FCU-1 SHALL BE EQUAL TO BARD, MODEL NUMBER W30A1. PROVIDE REQUIRED HARDWARE & ACCESSORIES FOR EXTERIOR WALL MOUNTING OF UNIT. AUTOMATICALLY CYCLE FCU-1 WITH PROGRAMMABLE THERMOSTAT & UNIT CONTROLLER TO MAINTAIN 80DEG F (ADJ) DURING COOLING MODE & 60DEG F (ADJ) DURING HEATING MODE. MOUNT BOTTOM OF UNIT APPROXIMATELY 2'-6" ABOVE FINISHED GRADE.
- PROGRAMMABLE T-STAT (T): SEVEN DAY PROGRAMMING, TOUCH SCREEN, TWO DEGREE F (ADJ) DEADBAND BOTH ABOVE & BELOW SETPOINT. PROGRAMMABLE T-STAT SHALL BE EQUAL TO HONEYWELL VISIONPRO 8000 SERIES.



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**ITEM NO. L-109.2 - FURNISH & INSTALL SPECIFIED ELECTRICAL BUILDING EQUIPMENT**

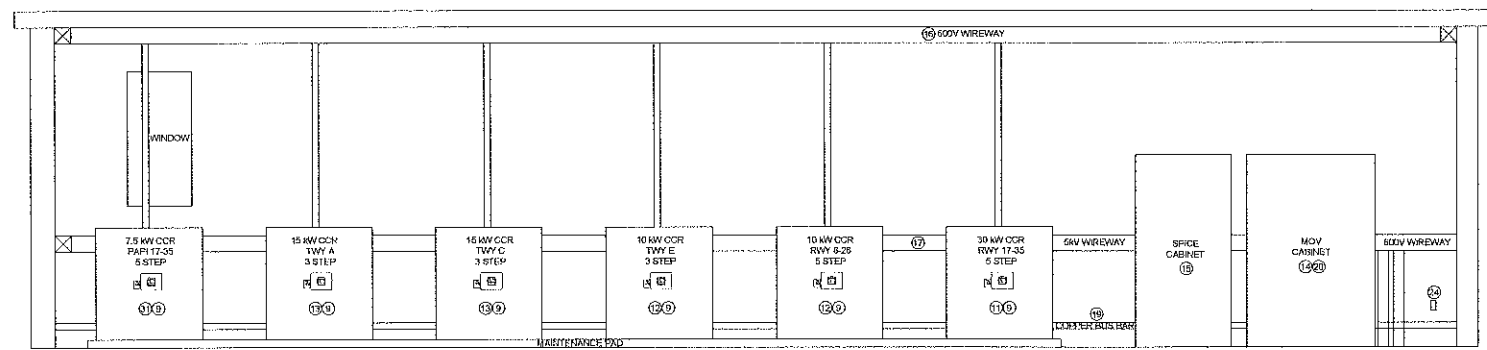
ITEM	DESCRIPTION	ITEM	DESCRIPTION	ITEM	DESCRIPTION
1	PANELBOARD, 277/480V, 3 PH, 4W, SN WITH 400A MAIN BREAKER, 42 POLE CAPACITY, NEMA 1 ENCLOSURE, CIRCUIT BREAKERS.	6	SINGLE PHASE TRANSFORMER, 480V-120/208V, 45 KVA.	19	1-1/4" X 1/4" COPPER GROUND BUSS INSTALLED ON 1" SPACERS AROUND THE ELECTRICAL VAULT INTERIOR 12" OFF THE FLOOR AND ROUTED AROUND THE DOORS.
2	HEAVY DUTY 400A FUSIBLE DISCONNECT.	7	PHOTOELECTRIC CONTROLLER.	20	METAL OXIDE LIGHTNING ARRESTORS, 5kV CIRCUIT RATING, SILICONE RUBBER HOUSING.
3	UTILITY APPROVED METER SOCKET, METER AND CT CABINET.	8	L-854 RADIO CONTROLLER - PROGRAMMABLE AND ADJUSTABLE SENSITIVITY.	21	STEP-DOWN TRANSFORMERS, 480V-120/208V, 1 KVA FOR NAVAIDS IN THE FIELD.
4	SURGE SUPPRESSION SYSTEM.	9	INTERNAL SERIES PLUG CUTOUT, "SCO" TYPE WITH THREE WORKING POSITIONS. (NORMAL, SHORT OR GROUNDED)	22	DISCONNECT, 30A, 600V, NEMA 1, OR NEMA 3R (AS APPLICABLE), FUSIBLE, FOR AWOS IN THE FIELD.
5	PANELBOARD, 120/208V, 1 PH, 3W, SN WITH 100A MAIN BREAKER, 42 POLE CAPACITY, NEMA 1 ENCLOSURE, AND CIRCUIT BREAKERS.	10	ALCMS 19" INDUSTRIAL RACK, NEMA 12, ENVIRONMENTALLY CONTROLLED, WITH ALCMS EQUIPMENT.	23	FCU-1.
		11	CONSTANT CURRENT REGULATOR, 480V INPUT, 30 kW, 6.6A OUTPUT, 5 BRIGHTNESS STEPS, L-829, CLASS 1, STYLE 2, FERRORESONANT.	24	GFCI ELECTRICAL OUTLET, INDUSTRIAL RATED, TYP.
		12	CONSTANT CURRENT REGULATOR, 480V INPUT, 10 kW, 6.6A OUTPUT, 3 BRIGHTNESS STEPS, L-829, CLASS 1, STYLE 1, FERRORESONANT.	25	LIGHT SWITCH, INDUSTRIAL RATED, TYP.
		13	CONSTANT CURRENT REGULATOR, 480V INPUT, 15 kW, 6.6A OUTPUT, 3 BRIGHTNESS STEPS, L-829, CLASS 1, STYLE 1, FERRORESONANT.	26	EMERGENCY EXIT/LIGHT UNIT
		14	LIGHTNING ARRESTOR CABINET, NEMA 1, 60"x60"x12".	27	INTERIOR LIGHT UNIT
		15	SPLICE CABINET, NEMA 1, 60"x36"x12".	28	EXTERIOR LIGHT UNIT
		16	METAL LAY-IN WIREWAY, 8"x8", NEMA 1, HINGED, 600V, UPPER DUCT, LENGTH AS REQUIRED	29	3/4" x 10" COPPERCLAD GROUND ROD WITH EXOTHERMIC WELD
		17	METAL LAY-IN WIREWAY, 8"x8", NEMA 1, HINGED, 5 kV, BOTTOM DUCT, LENGTH AS REQUIRED WITH 1 5/8" STAND OFF TO ALLOW CONDUIT RUNS BEHIND WIREWAY.	30	AUTOMATIC TRANSFER SWITCH, ATS, 3P-4W, 480V
		18	CONTACTOR, 2P, 30A, WITH 120V COIL AND ROTARY HOA SWITCH, IN NEMA 1 ENCLOSURE FOR VOLTAGE NAVAIDS.	31	CONSTANT CURRENT REGULATOR, 480V INPUT, 7.5 kW, 6.6A OUTPUT, 5 BRIGHTNESS STEPS, L-829, CLASS 1, STYLE 1, FERRORESONANT.
				32	TRANSFORMER, 480V-120/240V, 5 kVA, NEMA 3R INSTALLED AT THE AWOS.

MH NO: 0119700-170869.02  
 DATE: DECEMBER 6, 2022  
 DESIGNED BY: XXX  
 DRAWN BY: XXX  
 CHECKED BY: XXX  
DO NOT SCALE DRAWINGS

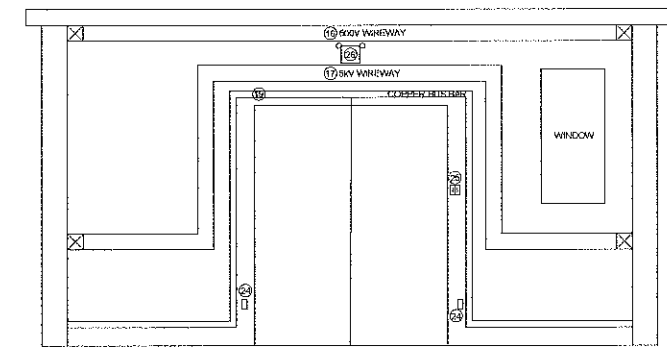
SHEET CONTENTS  
**ELECTRICAL BUILDING EQUIPMENT FLOOR PLAN**

SHEET NO.

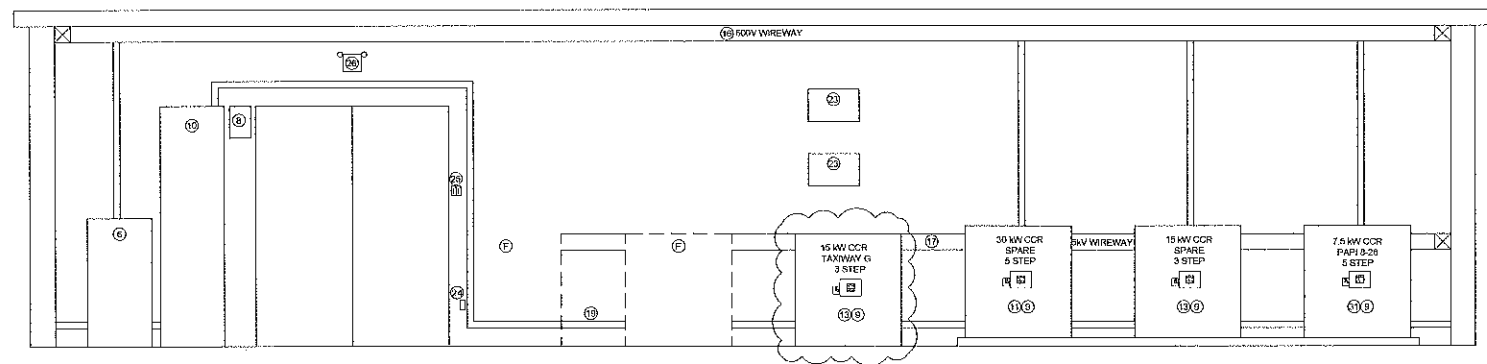




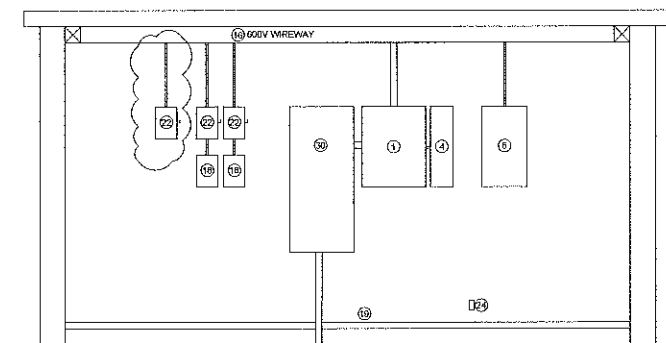
**1 NORTH INTERIOR ELEVATION**  
SCALE: 1/2" = 1'-0"



**2 WEST INTERIOR ELEVATION**  
SCALE: 1/2" = 1'-0"



**3 SOUTH INTERIOR ELEVATION**  
SCALE: 1/2" = 1'-0"



**4 EAST INTERIOR ELEVATION**  
SCALE: 1/2" = 1'-0"

**ITEM NO. L-109.2 - FURNISH & INSTALL SPECIFIED ELECTRICAL BUILDING EQUIPMENT**

ITEM	DESCRIPTION
1	PANELBOARD, 277/480V, 3 PH, 4W, SN WITH 400A MAIN BREAKER, 42 POLE CAPACITY, NEMA 1 ENCLOSURE, CIRCUIT BREAKERS.
2	HEAVY DUTY 400A FUSIBLE DISCONNECT.
3	UTILITY APPROVED METER SOCKET, METER AND CT CABINET.
4	SURGE SUPPRESSION SYSTEM.
5	PANELBOARD, 120/208V, 1 PH, 3W, SN WITH 100A MAIN BREAKER, 42 POLE CAPACITY, NEMA 1 ENCLOSURE, AND CIRCUIT BREAKERS.
6	SINGLE PHASE TRANSFORMER, 480V-120/208V, 45 KVA.
7	PHOTOELECTRIC CONTROLLER.
8	L-854 RADIO CONTROLLER - PROGRAMMABLE AND ADJUSTABLE SENSITIVITY.

9	INTERNAL SERIES PLUG CUTOUT, "SCO" TYPE WITH THREE WORKING POSITIONS. (NORMAL, SHORT OR GROUNDED)
10	ALCMS 19" INDUSTRIAL RACK, NEMA 12, ENVIRONMENTALLY CONTROLLED, WITH ALCMS EQUIPMENT.
11	CONSTANT CURRENT REGULATOR, 480V INPUT, 30 kW, 6.6A OUTPUT, 5 BRIGHTNESS STEPS, L-829, CLASS 1, STYLE 2, FERRORESONANT.
12	CONSTANT CURRENT REGULATOR, 480V INPUT, 10 kW, 6.6A OUTPUT, 3 BRIGHTNESS STEPS, L-829, CLASS 1, STYLE 1, FERRORESONANT.
13	CONSTANT CURRENT REGULATOR, 480V INPUT, 15 kW, 6.6A OUTPUT, 3 BRIGHTNESS STEPS, L-829, CLASS 1, STYLE 1, FERRORESONANT.
14	LIGHTNING ARRESTOR CABINET, NEMA 1, 60"x36"x12".
15	SPLICE CABINET, NEMA 1, 60"x36"x12".
16	METAL LAY-IN WIREWAY, 8"x8", NEMA 1, HINGED, 600V, UPPER DUCT, LENGTH AS REQUIRED

17	METAL LAY-IN WIREWAY, 8"x8", NEMA 1, HINGED, 5 KV, BOTTOM DUCT, LENGTH AS REQUIRED WITH 1 5/8" STAND OFF TO ALLOW CONDUIT RUNS BEHIND WIREWAY.
18	CONTACTOR, 2P, 30A, WITH 120V COIL AND ROTARY HOA SWITCH, IN NEMA 1 ENCLOSURE FOR VOLTAGE NAVAIDS.
19	1-1/4" X 1/4" COPPER GROUND BUSS INSTALLED ON 1" SPACERS AROUND THE ELECTRICAL VAULT INTERIOR 12" OFF THE FLOOR AND ROUTED AROUND THE DOORS.
20	METAL OXIDE LIGHTNING ARRESTORS, 5KV CIRCUIT RATING, SILICONE RUBBER HOUSING.
21	STEP-DOWN TRANSFORMERS, 480V-120/208V, 1 KVA FOR NAVAIDS IN THE FIELD.
22	DISCONNECT, 30A, 600V, NEMA 1, OR NEMA 3R (AS APPLICABLE), FUSIBLE, FOR AWOS IN THE FIELD.
23	HVAC SYSTEM.
24	GFCI ELECTRICAL OUTLET, INDUSTRIAL RATED, TYP.
25	LIGHT SWITCH, INDUSTRIAL RATED, TYP.
26	EMERGENCY EXIT/LIGHT UNIT

27	INTERIOR LIGHT UNIT
28	EXTERIOR LIGHT UNIT
29	3/4" x 10" COPPERCLAD GROUND ROD WITH EXOTHERMIC WELD
30	AUTOMATIC TRANSFER SWITCH, ATS, 3P-4W, 480V
31	CONSTANT CURRENT REGULATOR, 480V INPUT, 7.5 kW, 6.6A OUTPUT, 5 BRIGHTNESS STEPS, L-829, CLASS 1, STYLE 1, FERRORESONANT.
32	TRANSFORMER, 480V-120/240V, 5 KVA, NEMA 3R INSTALLED AT THE AWOS.
F	FUTURE CCRS.

NOTE: ONLY ITEMS IN REVISION CLOUD APPLY TO THIS CONTRACT

AUGUSTA REGIONAL AIRPORT  
CONSTRUCT TAXIWAY G

1501 AVIATION WAY  
AUGUSTA, GA 30906-9620

REVISION  
ISSUED FOR BID

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MSH NO: 0119700-170869.02  
DATE: DECEMBER 6, 2022  
DESIGNED BY: XXX  
DRAWN BY: XXX  
CHECKED BY: XXX  
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SHEET CONTENTS  
ELECTRICAL BUILDING  
EQUIPMENT  
ELEVATION PLAN

SHEET NO.

E-402

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**AUGUSTA REGIONAL AIRPORT  
 CONSTRUCT TAXIWAY G**

1501 AVIATION WAY  
 AUGUSTA, GA 30906-9620

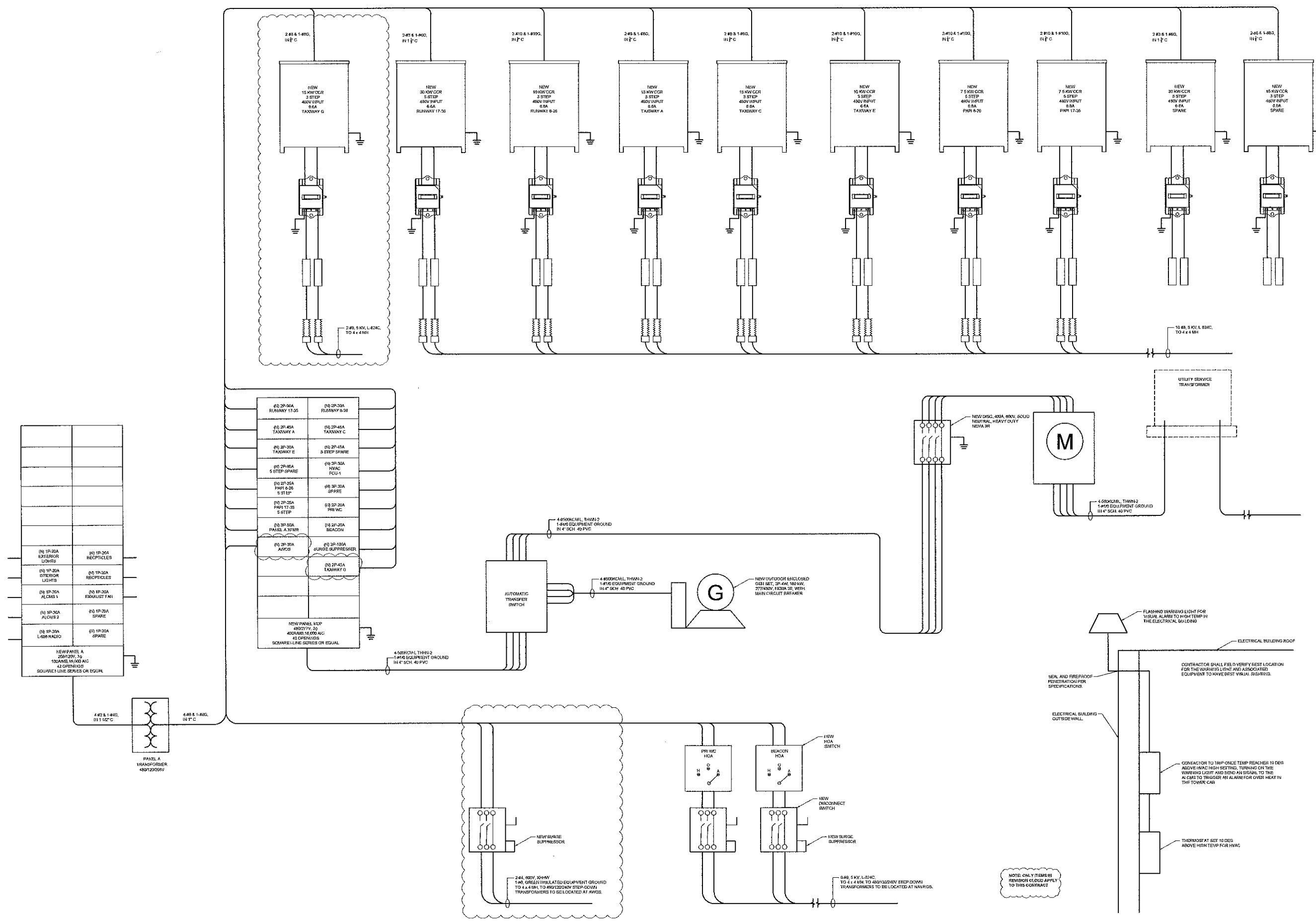
REVISION  
 ISSUED FOR BID

NOT FOR CONSTRUCTION

MEH NO: 0119700-170869.02  
 DATE: DECEMBER 6, 2022  
 DESIGNED BY: XXX  
 DRAWN BY: XXX  
 CHECKED BY: XXX  
 DO NOT SCALE DRAWINGS

SHEET CONTENTS  
 ELECTRICAL BUILDING  
 RISER DIAGRAM

SHEET NO.

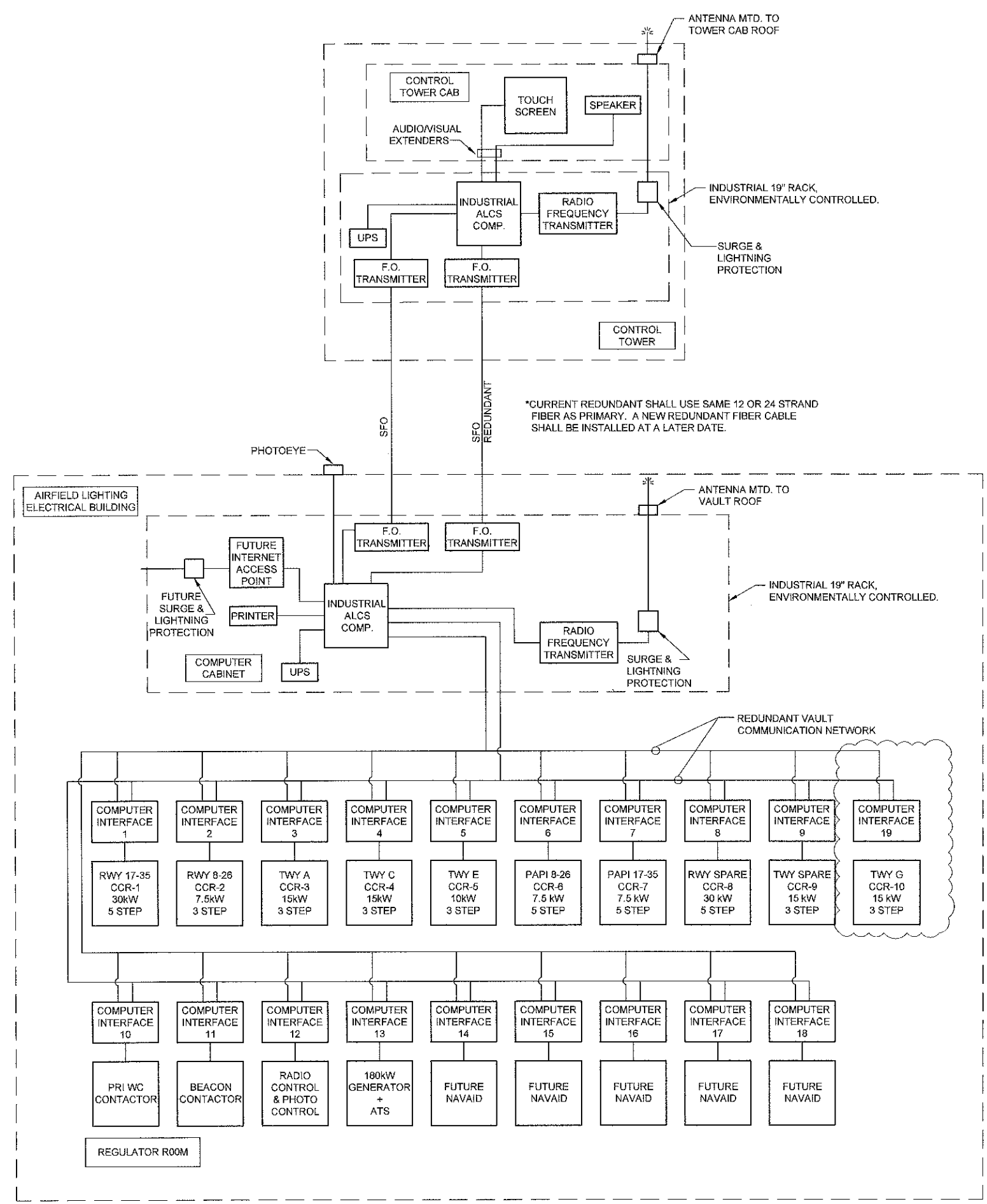




**L-854 RADIO CONTROL CHART**

	0-CLICK		3-CLICK		5-CLICK		7-CLICK	
	DAY	NIGHT	DAY	NIGHT	DAY	NIGHT	DAY	NIGHT
RWY 17-35	OFF	ON	LOW	LOW	MED	MED	HIGH	HIGH
RWY 8-26	OFF	OFF	LOW	LOW	MED	MED	HIGH	HIGH
TWY A	OFF	OFF	LOW	LOW	MED	MED	HIGH	HIGH
TWY C	OFF	OFF	LOW	LOW	MED	MED	HIGH	HIGH
TWY E	OFF	OFF	LOW	LOW	MED	MED	HIGH	HIGH
RWY SPARE	OFF	OFF	LOW	LOW	MED	MED	HIGH	HIGH
TWY SPARE	OFF	OFF	LOW	LOW	MED	MED	HIGH	HIGH
PAPI 17-35	HIGH	LOW	NA	NA	NA	NA	NA	NA
PAPI 8-26	HIGH	LOW	NA	NA	NA	NA	NA	NA
PRIMARY WINDCONE	OFF	ON	NA	NA	NA	NA	NA	NA
BEACON	OFF	ON	NA	NA	NA	NA	NA	NA
REIL 8	NA	NA	OFF	OFF	LOW	LOW	HIGH	HIGH
REIL 26	NA	NA	OFF	OFF	LOW	LOW	HIGH	HIGH
TWY G	OFF	OFF	LOW	LOW	MED	MED	HIGH	HIGH

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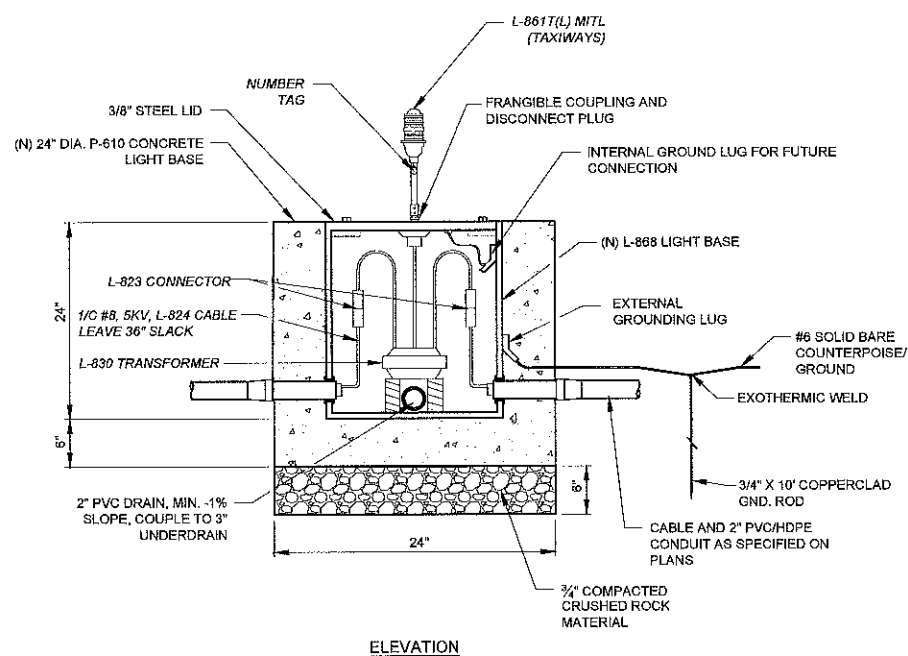


\*CURRENT REDUNDANT SHALL USE SAME 12 OR 24 STRAND FIBER AS PRIMARY. A NEW REDUNDANT FIBER CABLE SHALL BE INSTALLED AT A LATER DATE.

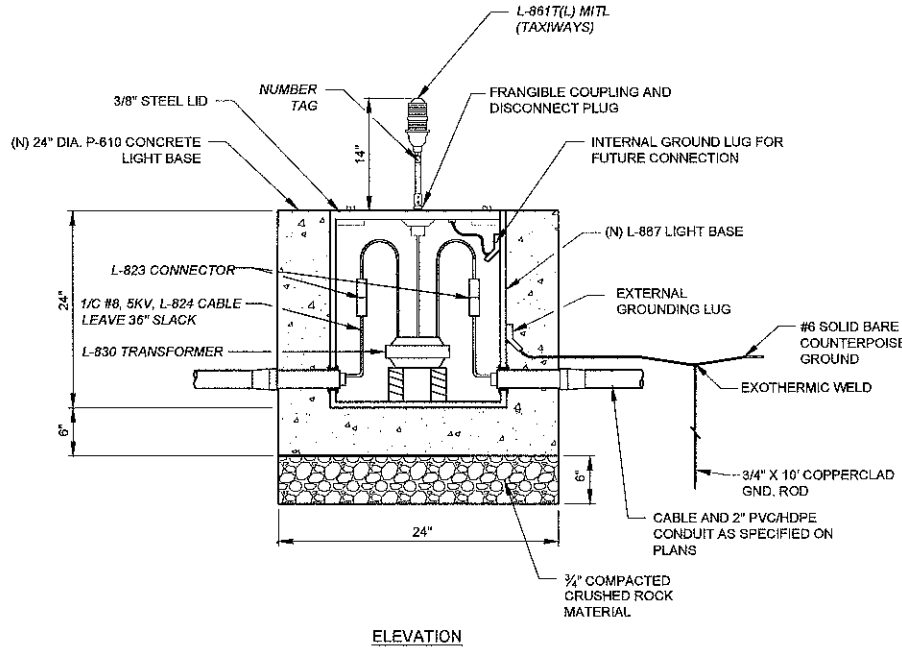
**LEGEND:**  
 SFO = INDICATES SINGLE MODE FIBER OPTIC CONNECTION.

- GENERAL NOTES:**
1. THIS RISER IS A CONCEPTUAL SCHEMATIC CONFIGURATION. THE SELECTED AIRFIELD LIGHTING CONTROL SYSTEM MANUFACTURER SHALL PROVIDE SPECIFIC SCHEMATIC CONFIGURATION.
  2. CONTRACTOR SHALL PROVIDE ALCMS SYSTEM. CONTRACTOR SHALL COORDINATE WITH ALCMS MANUFACTURER TO PROVIDE A COMPLETE AND OPERATING SYSTEM. THIS INCLUDES CONTRACTOR PROVIDING CABLES/WIRES, CCR'S, CONTACTORS, CONDUIT, CABLE TRAYS AND OTHER ITEMS NOT PROVIDED BY THE ALCMS MANUFACTURER.
  3. CONTRACTOR SHALL VERIFY ALL EXISTING CONDITIONS IN THE TOWER, INCLUDING THE NEW ELECTRICAL BUILDING. THE CONTRACTOR SHALL PROVIDE A COMPLETE AND WORKING ALCMS SYSTEM, WHICH MAY REQUIRE MORE THAN WHAT IS SHOWN IN THIS DIAGRAM.
  4. THE ALCMS SYSTEM SHALL BE A COMPUTER BASE SYSTEM THAT IS MOUNTED IN AN INDUSTRIAL 19" RACK THAT IS ENVIRONMENTALLY CONTROLLED TO PROTECT THE EQUIPMENT.
  5. CONTRACTOR SHALL COORDINATE WITH TOWER PERSONAL ON EQUIPMENT LOCATIONS INSIDE THE TOWER.
  6. L-854 RADIO CONTROL CHART FOR WHEN THE TOWER IS CLOSED. DURING TOWER HOURS, USE PRESETS FROM THE SPECIFICATIONS AND APPROVED BY THE AIRPORT.
  7. REILS POWERED FROM SEPARATE POWER SOURCE AND CONTROLLED FROM RWY CIRCUIT.

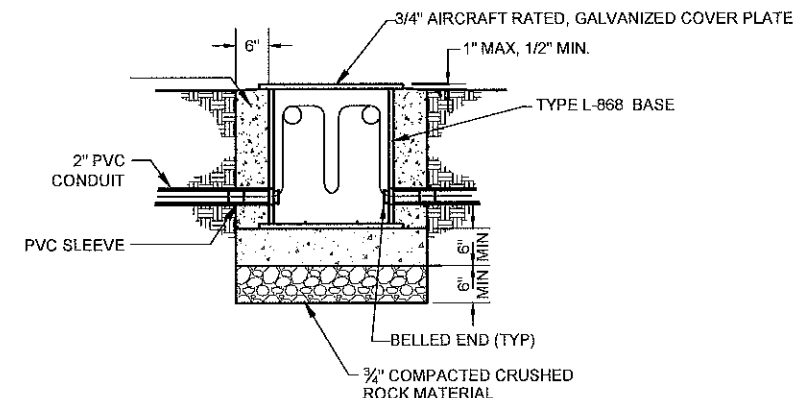
**NOTE: ONLY ITEMS IN REVISION CLOUD APPLY TO THIS CONTRACT**



ELEVATION



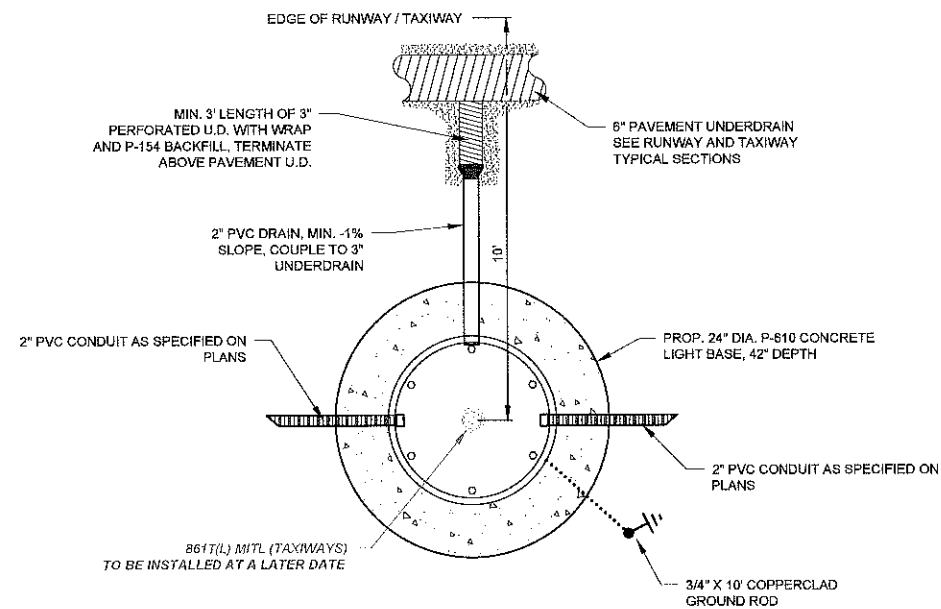
ELEVATION



NOTES:  
 1. INSTALL GROUND ROD AND GROUND WIRE SIMILAR TO ELEVATED TAXIWAY EDGE LIGHT DETAIL

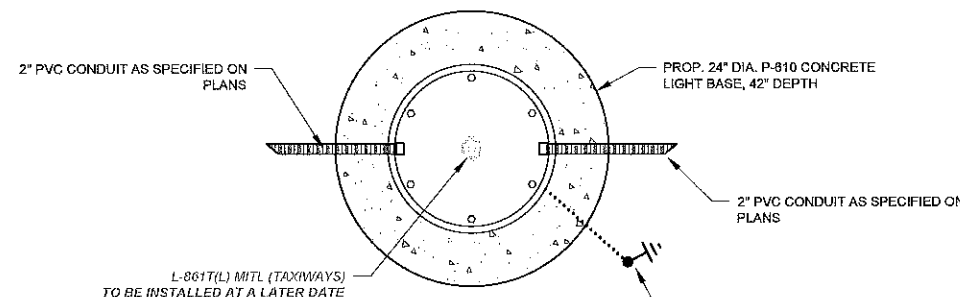
JUNCTION CAN DETAIL  
 SCALE: NTS

1  
 E-601



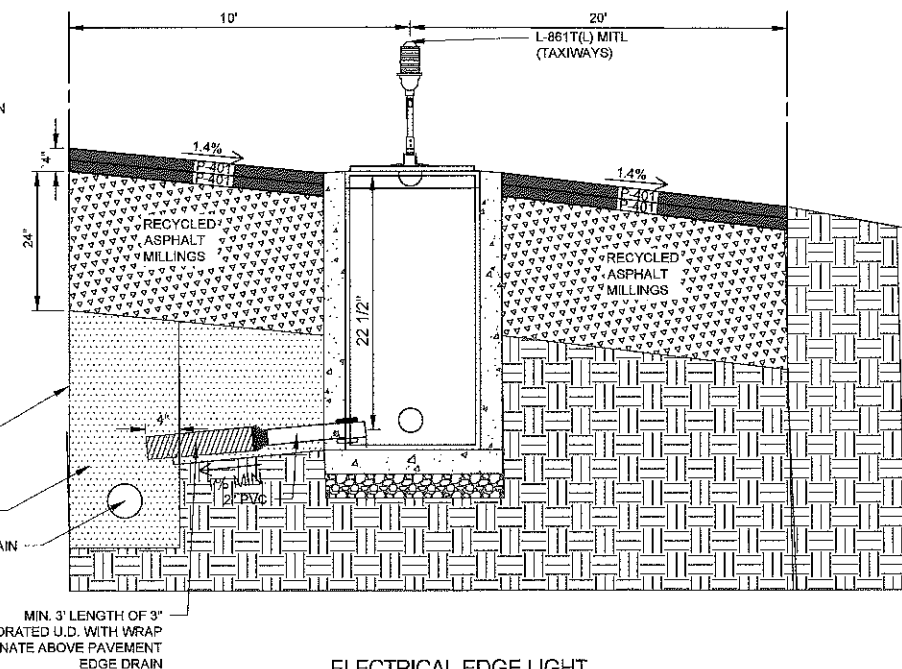
L861T (L) TAXIWAY EDGE LIGHT  
 INSTALLATION DETAIL WITH DRAINAGE  
 SCALE: NTS

5  
 E-601



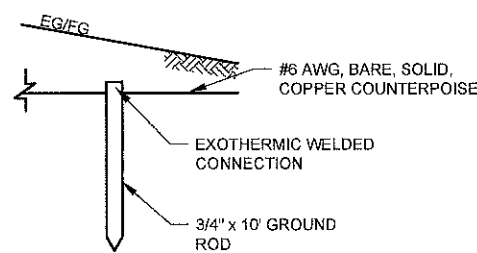
L861T (L), L861, L862 EDGE LIGHT  
 INSTALLATION DETAIL  
 SCALE: NTS

3  
 E-601



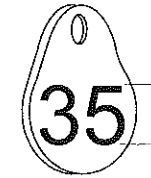
ELECTRICAL EDGE LIGHT  
 DRAIN DETAIL  
 SCALE: NTS

2  
 E-601



GROUND ROD INSTALLATION DETAIL  
 SCALE: NTS

4  
 E-601



NOTE:  
 AFFIX NON-CORROSIVE NUMBERING TAG TO FIXTURE FACING RUNWAY WITH SET SCREW, WIRE TIE, OR METAL BAND, NUMBERS SHALL BE ENGRAVED FOR PERMANENT READABILITY.

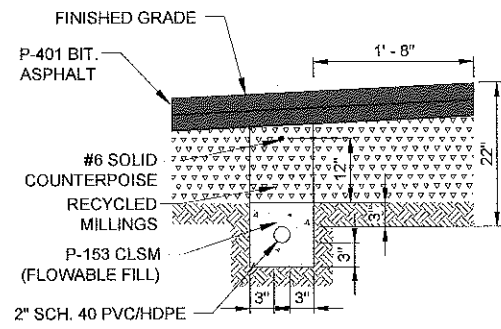
TAG NUMBERING DETAIL  
 SCALE: NTS

2  
 E-602

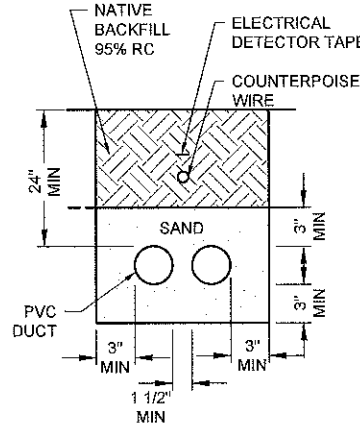
TYPICAL NOTES:

- SEE LAYOUT SHEET E-201 FOR EDGE LIGHT CAN LOCATIONS.
- ALL TAXIWAY EDGE LIGHTS AND ASSOCIATED CABLING TO BE INSTALLED AT A LATER DATE AND ARE NOT INCLUDED IN THIS PROJECT.
- ALL BOLTS SHALL BE STAINLESS STEEL.
- APPLY ANTI-SEIZE COMPOUND TO ALL MALE THREADS.
- PROVIDE FORMS AS NECESSARY FOR CONCRETE PLACEMENT. COST INCIDENTAL TO THE INSTALLATION.
- ALL NEW BASE CANS SHALL HAVE INTERNAL AND EXTERNAL GROUND LUGS.
- NON-METALLIC COMPONENTS IN THE LIGHT FIXTURE STEMS AND BREAKABLE COUPLINGS ARE NOT PERMITTED. L-867 PLASTIC CANS ARE NOT ACCEPTABLE.
- LIGHT AND BASES SHALL BE SET PLUMB.
- UNDER PAVEMENT, INSTALL COUNTERPOISE USING THE EQUIPOTENTIAL METHOD, 12" ABOVE CABLE/DUCT, CONNECTED TO OUTSIDE OF LIGHT BASE. IN GREEN SPACES, INSTALL COUNTERPOISE 12" ABOVE DUCT AND INSTALL GROUND RODS AT THE END OF RUNS AND AT 500' INTERVALS.
- ALL CONNECTIONS TO GROUND RODS SHALL BE MADE USING EXOTHERMIC WELDS.

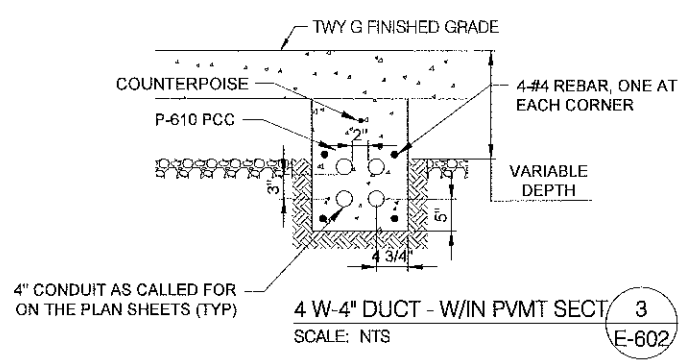




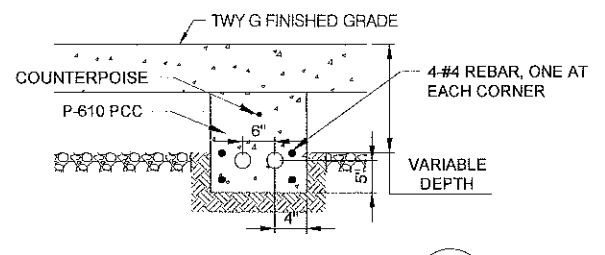
**SHOULDER AREAS**  
1 W-2" DUCT  
SCALE: NTS  
7  
E-602



**DUCT INSTALLATION UNPAVED AREAS**  
SCALE: NTS  
4  
E-602



3  
E-602

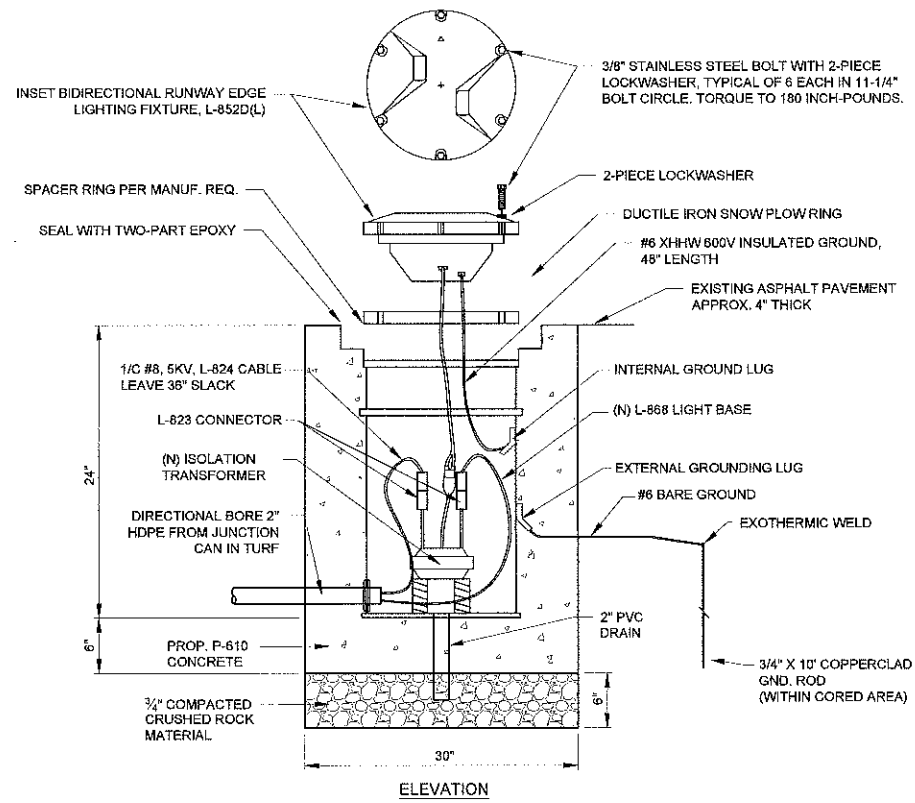


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E-602

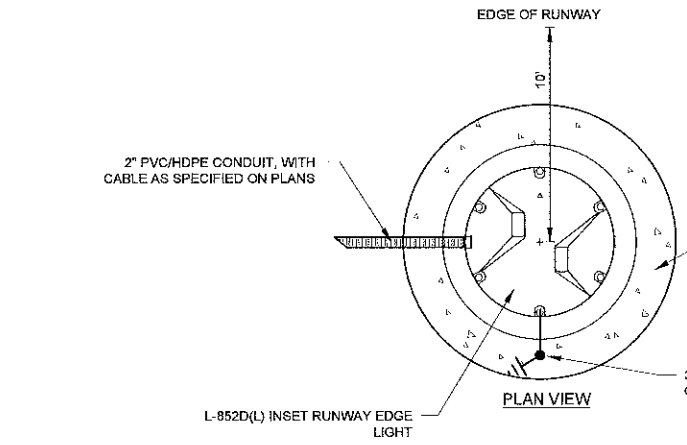
DUCT 2-3" TYPICAL. TEXT AND ARROW DISPLAY TO BE ADJUSTED TO ACTUAL FIELD CONDITION.

- NOTES:**
1. MARKERS SHALL BE 24 INCHES SQUARE BY 4 INCHES THICK PLACED IN LOCATIONS WHERE CHANGE IN DIRECTION OCCURS AND IN ACCORDANCE WITH THE SPECIFICATIONS.
  2. COST OF CONCRETE MARKERS IS INCIDENTAL TO THE ASSOCIATED ITEMS OF DUCT OR CABLE.
  3. LETTER AND NUMBER HEIGHT SHALL BE 4 INCHES DEPRESSED 1/4" INTO THE CONCRETE.

**DUCT, CABLE, AND SPLICE MARKER DETAIL**  
SCALE: NTS  
8  
E-602

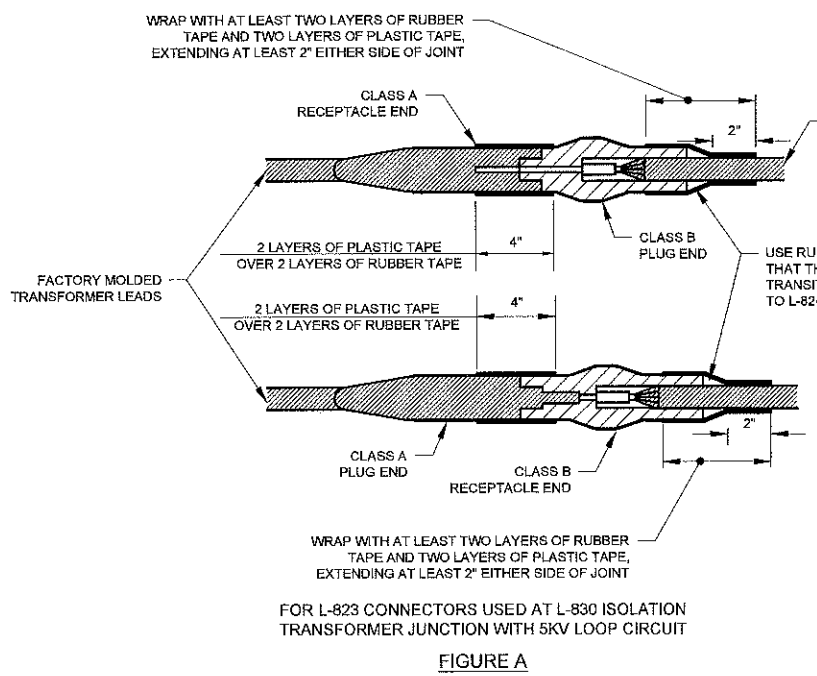


**L-852D(L) INSET RUNWAY LIGHT INSTALLATION**  
SCALE: NTS  
2  
E-602



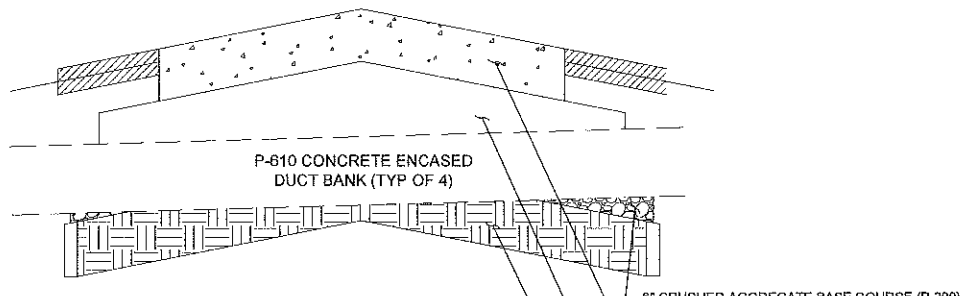
**L-852D(L) INSET RUNWAY LIGHT INSTALLATION**  
SCALE: NTS  
2  
E-602

- RUNWAY LIGHT NOTES:**
1. CONTRACTOR SHALL VERIFY LIGHT ORIENTATION WHEN INSTALLING THE BASE CAN TO MAKE SURE THE LIGHT BEAMS SHINE IN THE CORRECT DIRECTION. THE ONLY REPAIR IS TO REPLACE THE ENTIRE BASE CAN AT THE CONTRACTORS EXPENSE.



**CABLE SPLICE WATER PROOFING DETAIL**  
SCALE: NTS  
6  
E-602

- SPLICING NOTES:**
1. ALL SPLICES IN MANHOLES, IF ALLOWED, SHALL BE PER FIGURE B.
  2. CONNECTION OF CONDUCTORS MUST BE MADE BY USING CRIMP CONNECTORS AND A CRIMPING TOOL APPROVED BY THE CONNECTOR/LUG MANUFACTURER. THE TOOL MUST PRODUCE A COMPLETE CRIMP BEFORE IT CAN BE REMOVED. THE CRIMPING TOOL USED MUST BE LISTED BY THE L-823 KIT MANUFACTURER. MAKE THE NUMBER AND TYPE OF CRIMPS PER THE KIT MANUFACTURER'S INSTRUCTIONS.
  3. L-823 SPLICE KITS SHALL BE EITHER AMERAGE 54 SUPER KIT, OR INTEGRO COMPLETE KIT.
  4. RUBBER TAPE SHALL BE 3M SCOTCH 130C-RUBBER SPLICING, LINERLESS TAPE, 1-1/2"W OR APPROVED EQUAL.
  5. PLASTIC TAPE SHALL BE 3M SUPER 88 SCOTCH VINYL TAPE, 1-1/2"W OR APPROVED EQUAL.



**TAXIWAY G DUCTBANK CROSSING**  
SCALE: NTS  
5  
E-602

- DUCT BANK NOTES:**
1. SLOPE DUCT BANK TO FOLLOW GRADE. MAINTAIN 1-FOOT COVER WHEN JOINING AN EXISTING & PROPOSED DUCT BANK TOGETHER. THE EXISTING CONCRETE ENCASEMENT SHALL BE REMOVED TO PERMIT A TIGHT, SEAMLESS FIT.
  2. ALL CONNECTIONS SHALL BE APPROVED BY THE PROJECT ENGINEER.
  3. SEE TRENCHING PAY ITEM DETAILS FOR BACKFILL BENEATH ROADWAYS. SPOIL BACKFILL IN OPEN FIELD LOCATIONS SHALL BE COMPACTED TO SPECIFIED DENSITY IN 8-INCH LIFTS.
  4. ALL EXISTING MANHOLE CONNECTIONS SHALL BE MADE BY CORING THROUGH THE MANHOLE (JACK HAMMERING OF MANHOLES IS PROHIBITED).
  5. FOR #6 COUNTERPOISE, REQUIRED GROUND RODS SHALL BE SPACED NO MORE THAN 500 FEET APART, PLUS AT BEGINNING AND END OF RUNS.

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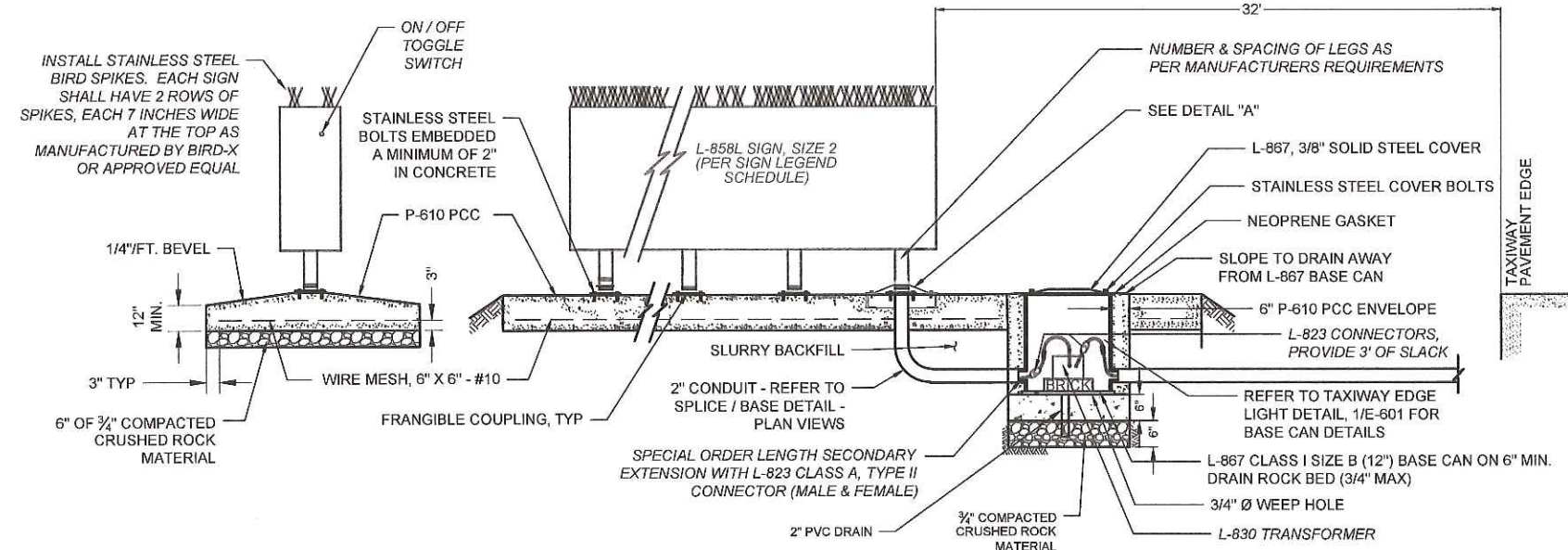
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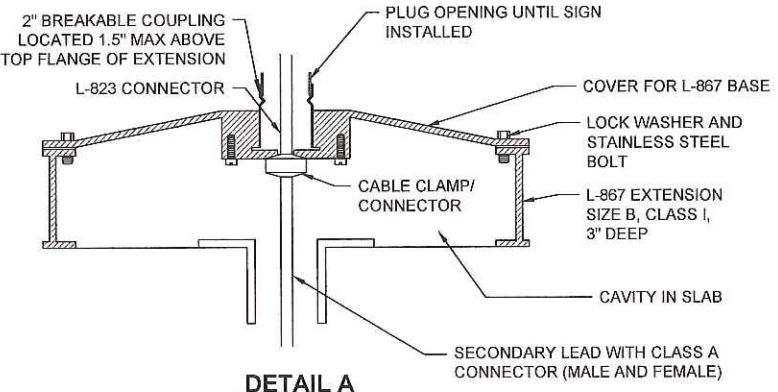
SHEET CONTENTS  
ELECTRICAL DETAILS

SHEET NO.



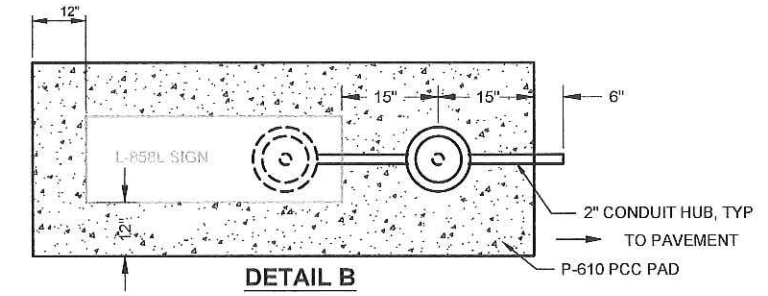
**TYPICAL LIGHTED AIRFIELD GUIDANCE AND HOLD SIGN**  
SCALE: NTS

3  
E-603



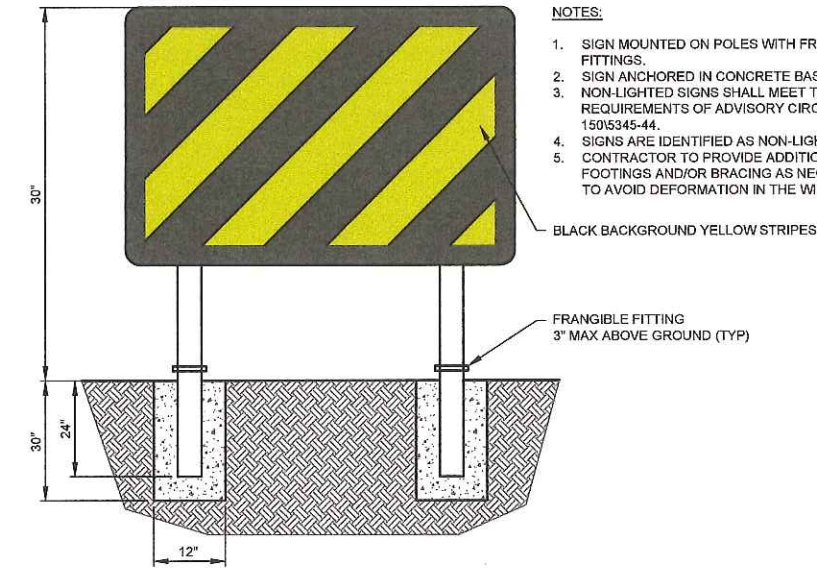
**DETAIL A**

- GENERAL SIGN NOTES:**
1. ALL GUIDANCE SIGNS AND ASSOCIATING CABLING TO BE INSTALLED AT A LATER DATE AND ARE NOT INCLUDED IN THIS PROJECT.
  2. COUPLING WEEP HOLES SHALL NOT DRAIN TO THE OUTSIDE OF THE L-867 BASE CAN OR LID.
  3. PROVIDE ONE TETHER ON EACH END OF SIGN.
  4. INSTALL GROUND WIRE AND GROUND ROD TO EXTERIOR LUG ON L-867 BASE CAN.
  5. BOND SIGN METAL SURFACES TO LIGHT BASE GROUNDING LUG, USING #6 BARE COPPER WIRE.
  6. DIMENSION SIGN PAD PER DETAIL "A"
  7. PLUG ALL OPENINGS FOR FUTURE SIGN INSTALLATION.



**SIGN BASE DETAILS-PLAN VIEWS**  
SCALE: NTS

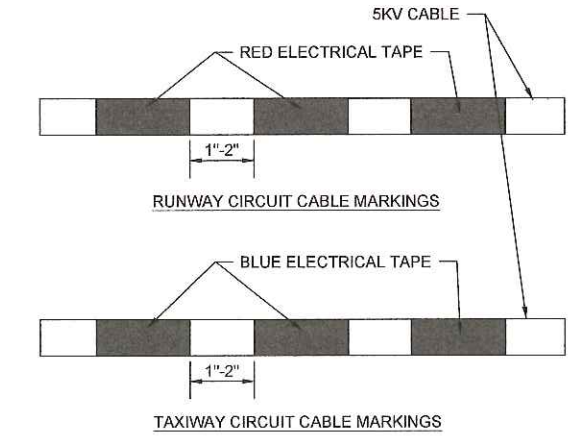
2  
E-603



**NON-LIGHTED SIGN, SIZE 1, STYLE 4**  
SCALE: NTS

5  
E-603

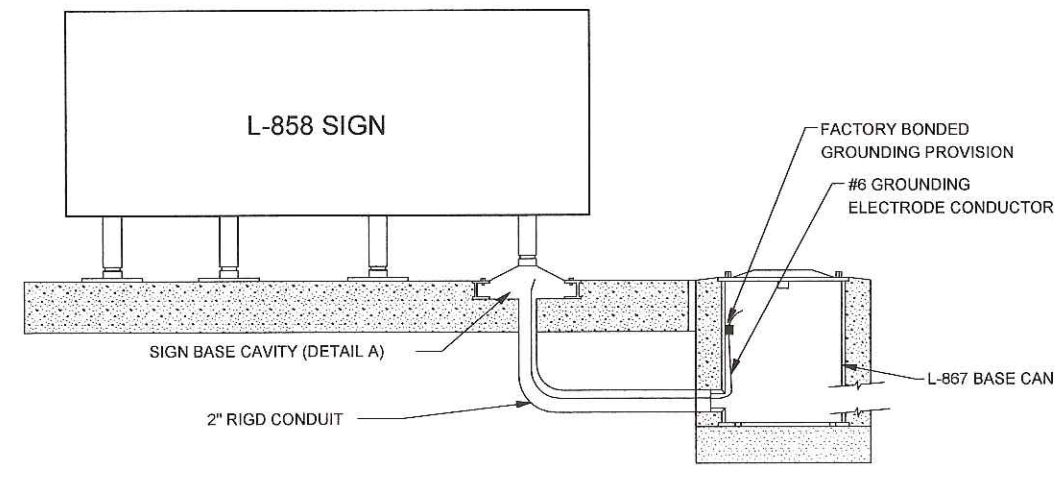
- NOTES:**
1. SIGN MOUNTED ON POLES WITH FRANGIBLE FITTINGS.
  2. SIGN ANCHORED IN CONCRETE BASE.
  3. NON-LIGHTED SIGNS SHALL MEET THE REQUIREMENTS OF ADVISORY CIRCULAR 150/5345-44.
  4. SIGNS ARE IDENTIFIED AS NON-LIGHTED SIGNS.
  5. CONTRACTOR TO PROVIDE ADDITIONAL FOOTINGS AND/OR BRACING AS NECESSARY TO AVOID DEFORMATION IN THE WIND.



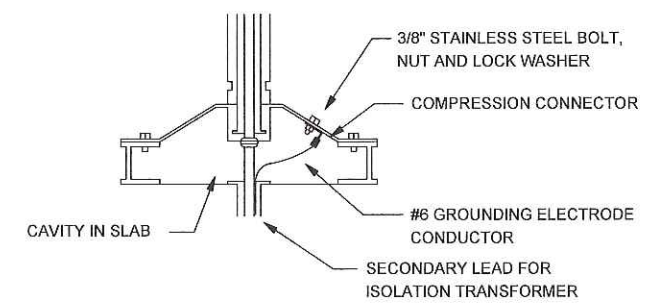
**NOTE: APPLY ELECTRICAL TAPE MARKINGS ON EITHER SIDE OF A SPLICE WITHIN A JUNCTION STRUCTURE AND AT A MINIMUM OF 2 LOCATIONS WITHIN A JUNCTION STRUCTURE (PULLCAN AND PULL BOXES).**

**CABLE MARKING AND IDENTIFICATION DETAIL**  
SCALE: NTS

4  
E-603



**MULTIPLE SIGN PEDESTAL GROUNDING DETAIL**  
NOT TO SCALE



**SIGN BASE CAVITY GROUNDING DETAIL**  
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SHEET CONTENTS  
GUIDANCE SIGN  
DETAILS

SHEET NO:



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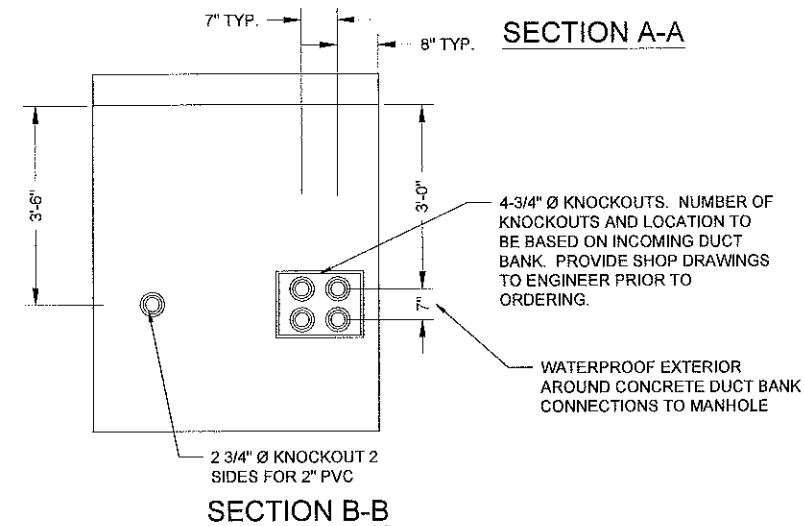
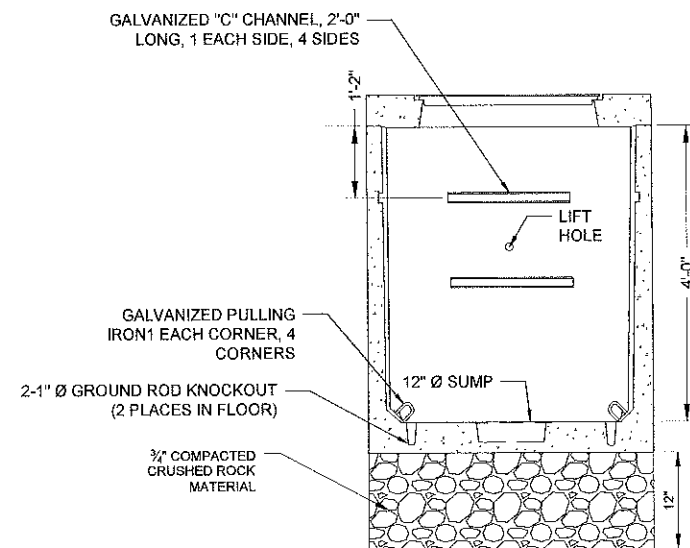
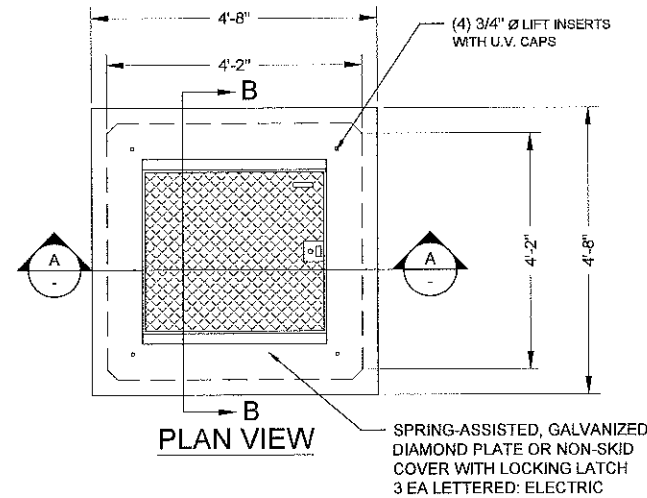
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SHEET CONTENTS  
PULL-BOX DETAILS

SHEET NO.

**E-604**



**ELECTRICAL PULLBOX**

SCALE: NTS

1  
E-603

**NOTES:**

- PULL BOXES SHALL BE RATED FOR FAA DESIGNATED 150,000-POUND, DUAL WHEEL AIRCRAFT LOADING.
- PULL BOXES SHALL BE SET 1" ABOVE ADJACENT GRADE SHAPE GRADE AT PULL BOXES AS NECESSARY TO DRAIN AWAY.
- THE HEAVY DUTY ELECTRICAL PULL BOX COVERS SHALL BE SPRING LOADED.
- PULL BOXES SHALL BE GROUNDED IN ACCORDANCE WITH THE SPECIFICATIONS.
- PROVIDE GALVANIZED OR NONMETALLIC CABLE RACKS INSIDE ELECTRICAL PULL BOXES
- GROUND MANHOLE PER L-115 SPECIFICATION.